



Gannett Fleming

*Excellence Delivered **As Promised***

Date: November 5, 2020

To: Steve Norcini, PE Township Engineer

From: Roger Phillips, PE

cc: Kevin W. Kochanski, RLA, CZO – Director of Community Development
Mary Eberle, Esq. – Grim, Biehn, and Thatcher
Damon Drummond, PE – Gilmore & Associates, Inc.
Patricia Sherwin – Radnor Township Engineering Department

RE: Hamilton Estate Strafford Ave and Forrest Lane – Conditional Use Review

Gannett Fleming, Inc. has completed our review of the Conditional Use Application for 208 & 228 Strafford Avenue and 18 Forrest Lane Conditional Use Plans for compliance with the Radnor Township Code.

The applicant has filed a conditional use application as outlined in Article XXIII – Conditional Uses, of the Township Code, and is requesting conditional use under §280-90 (Density Modification) of the Township Zoning Code to allow the development of the property to be consolidated from 3 lots to 1 lot with the construction of 41 townhomes. This plan was reviewed only for items relating to the Density Modification portion of the Township Code. A full land development review will be completed following the Conditional Use Hearing should the Density Modification application be approved by the Board of Commissioners.

The tract presently consists of several parcels with a combined lot area of approximately 7.5 acres, containing a total of six dwelling units and related improvements. The tract has frontage on Eagle Road and Strafford Avenue, with the rear boundary adjacent to the Eagle Village Shops.

The applicant intends to develop the 7.5 acres into 41 townhomes under the Pennsylvania Uniform Planned Community Act. The plan is in conformance with the required open space under §280-91. The Plan proposed 53,685 square feet of open space (15.9%) of the total adjusted tract area in accordance with the Density Modification Provisions that will be available for the use of the residents of the new homes.

We have the following general comments regarding the plans as submitted. A detailed review of the subdivision and land development plans will be completed should the Conditional Use Application be approved by the Board of Commissioners.

Gannett Fleming, Inc.

Valley Forge Corporate Center • 1010 Adams Avenue • Audubon, PA 19403-2402
t: 610.650.8101 • f: 610.650.8190
www.gannettfleming.com

1. Information regarding the ownership of the proposed roadways must be provided. Should the applicant wish to dedicate the roadways to the Township a 60-foot right of way must be provided.
2. The applicant must provide information regarding trash removal, snow removal, etc.
3. §255-38.H – All shade trees provided must be in conformance with the approved trees listing. Upon approval of the Board of Commissioners, other species may be utilized.

If you have any questions or require any additional information, please contact me.

Very truly yours,

GANNETT FLEMING, INC.

A handwritten signature in blue ink, appearing to be 'R. Phillips', with a large, stylized initial 'R' and a horizontal line extending to the right.

Roger A. Phillips, P.E.
Senior Project Manager



GILMORE & ASSOCIATES, INC.
ENGINEERING & CONSULTING SERVICES

MEMORANDUM

Date: November 16, 2020

To: Steve Norcini, P.E.

From: Damon Drummond, P.E., PTOE

cc: Leslie Salsbury, P.E.

Reference: 208 & 228 Strafford Avenue and 18 Forrest Lane
Conditional Use
Radnor Township, Delaware County
Transportation Review 1

G&A #20-11003

Our transportation department has completed a review for the Conditional Use plans prepared for above reference project. We offer the following comments for your consideration:

A. PROJECT DESCRIPTION

The Applicant is proposing to construct 41 townhomes on the southwest corner of Stafford Avenue and Eagle Road.

B. DOCUMENTS REVIEWED

1. Conditional Use and Preliminary Subdivision and Land Development Plans prepared by Site Engineering Concepts, LLC, prepared for Hamilton Estate, consisting of 15 sheets and dated August 27, 2020.
2. Traffic Impact Assessment prepared by F. Tavani and Associates, Inc., dated August 25, 2020.
3. Cover Letter prepared for Radnor Township, prepared by Kaplin Stewart Meloff Reiter & Stein, PC, dated September 24, 2020.

C. ZONING ORDINANCE COMMENTS

1. §280-135 F. (1) In addition to demonstrating compliance with all standards applicable to the conditional use being requested, the generalized site plan shall show the applicant's intentions with regard to site access, interior circulation and parking.
 - a. In accordance with SADLO §255-27A.(5) Dead-end streets shall be prohibited, except as stubs to permit future street extension into adjoining tracts or when designed as cul-de-sacs. In accordance with SADLO §255-27A.(6) Stub streets greater in length than one lot width shall be provided with a turnaround designed

to meet the standards required for a cul-de-sac and shall be provided with sufficient rights-of-way to permit the future extension of the street into the adjacent property.

Unit1 and Unit 2 access a dead-end section of Road A. Access to these two units should be further evaluated.

- b. SADLO §255-27C(1) - Sidewalks should be provided on both sides of Road A and Road B for pedestrian circulation.
2. §280-135 G. (1) – Traffic Impact Study Comments
- a. The site trips to/and from the site accesses are to be based on 41 units. We understand there are existing units and the applicant is assuming credit for these trips in the existing network; however, the traffic volumes at the access should reflect the proposed site trips based on 41 units to evaluate its operation.
 - b. Revise the Trip Generation Table to reflect site trips for 41 units. An additional line should be added indicating credit for existing site trips.
 - c. Provide a figure indicating the existing site trips.
 - d. Update the analysis worksheets accordingly.
 - e. Attach the turning movement traffic counts in the appendix of the Traffic Impact Assessment for verification.

George W. Broseman
Direct Dial: (610) 941-2459
Direct Fax: (610) 684-2005
Email: gbroseman@kaplaw.com
www.kaplaw.com

September 24, 2020

VIA HAND DELIVERY

Stephen Norcini
Township Engineer
Township of Radnor
301 Iven Avenue
Wayne, PA 19087

**RE: 208 & 228 Strafford Ave, 18 Forrest Lane – Wayne, Radnor Township
Conditional Use and Preliminary Land Development Applications
Our Reference: 15709.1**

Dear Mr. Norcini:

I represent the Trustees of the Dorrance Hamilton 3/15/1996 Revocable Agreement of Trust (“**Applicant**”), the owner of the properties at 208 & 228 Strafford Avenue and 18 Forrest Lane in Radnor Township (collectively “**Property**”) located in the R-4 Residence District. The Applicant is proposing to construct forty-one townhomes and related improvements on the Property as generally shown on the plans included with this application (“**Project**”). On behalf of the Applicant, I am filing the following:

I. Conditional Use Application

1. Ten (10) copies of the Conditional Use Application seeking a conditional use under Radnor Township Code (“**Code**”) Section 280-29.B.(1) for Density Modification Development in the R-4 Residence District;
2. Ten (10) copies of the plan set entitled “208 & 228 Strafford Avenue 18 Forrest Lane Preliminary Land Development Plan Set,” containing 15 Sheets dated August 27, 2020 (sheets 1-11 prepared by SITE Engineering Concepts, LLC, sheets 12-15 prepared by Glackin Thomas Panzak);
3. Ten (10) copies of a stormwater management report titled “Post Construction Stormwater Management Analysis,” prepared by SITE Engineering Concepts, LLC, dated August 27, 2020;

4. Ten (10) copies of the narrative for the planning module for land development dated August 24, 2020, prepared by HILBEC Engineering & Geosciences, LLC;
5. Ten (10) copies of a traffic impact study prepared by F. Tavani & Associates, dated August 25, 2020;
6. Ten (10) copies of a Fiscal Impact Analysis prepared by Erik W. Hetzel, AICP/PP, dated September 17, 2020;
7. Ten (10) copies of a development impact study compiled by Kaplin Stewart Meloff Reiter & Stein, dated September 24, 2020;
8. Ten (10) copies of the Conceptual Architectural Elevations;
9. Ten (10) copies of a draft Planned Community Declaration;
10. A \$1,500.00 check made payable to the Township of Radnor, the applicable Conditional Use application fee;
11. An electronic copy of all submitted materials;

II. Preliminary Land Development Application

1. Eighteen (18) copies of the Preliminary Land Development application;
2. Eighteen (18) copies of the Delaware County Planning Commission review application;
3. Eighteen (18) copies of the plan set entitled "208 & 228 Strafford Avenue 18 Forrest Lane Preliminary Land Development Plan Set," containing 15 Sheets dated August 27, 2020 (sheets 1-11 prepared by SITE Engineering Concepts, LLC, sheets 12-15 prepared by Glackin Thomas Panzak);
4. Six (6) copies of a stormwater management report titled "Post Construction Stormwater Management Analysis," prepared by SITE Engineering Concepts, LLC, dated August 27, 2020;
5. Eighteen (18) copies of a title report prepared by First American Title Insurance Company and identified as PAFA19-0709D/M that includes the portions of the Properties currently known as Delaware Folio Numbers: 36-01-00538-00; 36-01-00539-00; 36-01-00540-00, together with attachments. It is noted that this title report also pertains to other lands owned by Applicant that are not part of this Application. The Properties subject to this these applications are identified in this

title report as "Premises A" and "Premises B". This title report also contains the deed for these parcels;

6. Eighteen (18) copies of a title report prepared by First American Title Insurance Company and identified as Commitment Number PAFA20-1391D/M that includes the portion of the Properties currently known as 18 Forrest Lane, Delaware County Folio Number: 36-01-00236-02, together with attachments. It is noted that the title report also pertains to other lands owned by Applicant that are not part of this Application. The Property is identified as "Premises C" in the title report. The title report contains the deed for this portion of this parcel;
7. Eighteen (18) copies of a traffic impact study prepared by F. Tavani & Associates, dated August 25, 2020;
8. Eighteen (18) copies of a letter from Aqua Pennsylvania to SITE Engineering Concepts, LLC regarding the availability of public water for the Project, dated March 25, 2020;
9. An electronic copy of all submitted materials;
10. Two (2) checks as follows:
 - a. A \$50.00 check made payable to the Township of Radnor, the applicable Preliminary Land Development application fee;
 - b. A \$16,950.00 check made payable to the Township of Radnor, the applicable Professional Services Account fee.

Please submit the applications and plans to the appropriate Township staff, consultants and bodies for review, public meetings, and a public hearing for the Conditional Use application. Please provide us with copies of all reviews, communications, notices and other documentation relating to these applications, or related to the Property, as soon as they are generated and/or received.

If you have any questions or require any further information, please contact me. Thank you for your attention to this matter.

Sincerely,



George W. Broseman

GWB:DPR

Stephen Norcini
September 24, 2020
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Enclosures

cc via email: Haverford Properties, Inc.
SITE Engineering Concepts, LLC
Glackin Thomas Panzak

George W. Broseman, Esquire
Identification No.: 62649
Daniel P. Rowley, Esquire
Identification No.: 322680
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www.kaplaw.com

Attorneys for Applicant

**BEFORE THE BOARD OF COMMISSIONERS
OF RADNOR TOWNSHIP, DELAWARE COUNTY, PENNSYLVANIA**

In the Matter of:

**Trustees of the Dorrance Hamilton
3/15/1996 Revocable Agreement of Trust,
Applicant**

Premises:

**208 & 228 Strafford Avenue
18 Forrest Lane
Wayne, PA 19087**

CONDITIONAL USE APPLICATION

The Trustees of the Dorrance Hamilton 3/15/1996 Revocable Agreement of Trust, through its attorneys, George W. Broseman and Daniel P. Rowley, hereby files this Conditional Use application seeking approval to redevelop the properties at 208 Strafford Avenue, 228 Strafford Avenue, and 18 Forrest Lane in Wayne under the Density Modification Development provisions of the Township Zoning Ordinance and in support thereof states as follows:

1. **NAME/ADDRESS OF APPLICANT.**

The Applicant is the Trustees of the Dorrance Hamilton 3/15/1996 Revocable Agreement of Trust (“**Applicant**”), c/o Mr. D. Charles Houder, Haverford Properties, Inc., 551 W. Lancaster Avenue, Suite 307, Haverford, PA 19041. Applicant is the owner of the parcels currently known as 208 Strafford Avenue, 228 Strafford Avenue, and 18 Forrest Lane (collectively the “**Tract**”). The Tract consists of Delaware County Folio Numbers: 36-01-00538-00; 36-01-00539-00; 36-

01-00540-00; 36-01-00236-02. A copy of the deeds to the parcels that comprise the Tract are attached as **Exhibit A-1** and **Exhibit A-2**. **Exhibit A-1** is a copy of the deed that includes the parcels known as 208 Strafford Avenue and 228 Strafford Avenue, also known as Delaware County Folio Numbers: 36-01-00538-00; 36-01-00539-00; 36-01-00540-00. **Exhibit A-2** is a copy of the deed for the parcel known as 18 Forrest Lane, also known as Delaware County Folio Number 36-01-00236-02. It is noted that both deeds also include other lands owned by the Applicant that are not part of this Application.

2. **DESCRIPTION OF PROPERTY.**

The Tract presently consists of several parcels with a combined lot area of approximately 7.5 acres that contain a total of six dwelling units and related improvements. The Tract has frontage on Eagle Road and Strafford Avenue, and has an extensive boundary along the rear of the Eagle Village Shops, a large, multi-use shopping center that fronts on Lancaster Avenue and Eagle Road¹. The Tract is served by four driveways, three of which are interconnected. Three driveways connect to Strafford Avenue and one connects to Eagle Road². There are no sidewalks along the frontages of the Tract. The Tract and the general area around the Tract are developed suburban areas and are served by both public water and sanitary sewer service. Like many of the other properties in the area, including those in close proximity to the Tract that were developed before today's strict stormwater management requirements, the parcels within the Tract have no stormwater management facilities.

The existing conditions of the Tract are shown on the plan sheet entitled "Existing Conditions Plan," sheet 2 of 15 of the plan set entitled "208 & 228 Strafford Avenue, 18 Forrest

¹ One parcel, 18 Forrest Lane also abuts the Eagle Village Shops and presently contains a single-family detached dwelling that fronts on Forrest Lane.

² The parcel at 18 Forrest Lane presently has its own driveway to Forrest Lane.

Lane Preliminary Land Development Plan Set” containing 15 sheets dated August 27, 2020 (sheets 1-11 prepared by SITE Engineering Concepts, LLC, sheets 12-15 prepared by Glackin Thomas Panzak) (collectively referred to as “**Plans**”). The Plans are attached as **Exhibit “B.”**.

Pursuant to the Radnor Township Code (“**Code**”), the Tract is located in the R-4 Residence District (“**R-4 District**”), and is one property removed from the Township’s primary commercial artery, Lancaster Avenue/Route 30. The Tract serves as a transitional area from the commercial area along Lancaster Avenue to the residential areas to the north and northeast. The Tract either borders or is in close proximity to a number of commercial zoning districts, including: the C-2 General Commercial District, the C-3 Service Commercial District, the CO Commercial-Office District, and the PB Planned Business District. Some of the commercial uses that border or are in close proximity to the Tract include the Eagle Village Shops (described above), the Strafford Shopping Center with frontage on Lancaster Avenue and Eagle Road, and the Executive Commons of the Main Line office park with extensive frontage on both Eagle Road and Strafford Avenue. Only one property line, and the side lot lines of the 18 Forrest Lane parcel abut residentially zoned lots.

3. **CONDITIONAL USE FOR DENSITY MODIFICATION DEVELOPMENT**

A. **Density Modification Development – Townhomes a Permitted Use & Minimum Tract Size**

The Applicant requests conditional use approval from the Township Board of Commissioners under Code § 280-29.B.(1) for Density Modification Development in the R-4 District to redevelop the Tract with forty-one townhomes, common open space, significant stormwater management facilities, and related improvements (collectively “**Project**”). (See, *Code § 280-93 permitting townhomes in the R-4 District*). Density Modification Development is

permitted in many of the Township's residential zoning districts (*i.e.* AC, R-1, R-1A, R-2, R-3 and R-4). *Code § 280-92.A.* In the R-4 District, Density Modification Development is permitted on tracts of at least five acres in size. *Code § 280-92.A.(4).* The Tract, with over 7.5 acres exceeds this requirement. The Project will include common open space along the existing road frontages, in a centralized green to be created, and in other areas.

The Project is shown on the plan sheet entitled "Record Plan," sheet 4 of 15 of the Plans. As described below, the Plans comply with applicable provisions of the Zoning Ordinance for Density Modification Development in the R-4 District. The Applicant has also filed with the Township a preliminary land development application for the Project which will be reviewed by the Township engineering consultants, the Radnor Township Planning Commission, and the Board of Commissioners.

B. Density Modification Development Requirements.

1. Townhome regulations.

Article XIX of Chapter 280 of the Code contains the requirements for Density Modification Development. *Code § 280-93* sets forth the specific regulations applicable to townhomes in Density Modification Developments. As required, the townhomes will be owned and operated under the applicable provisions of the Pennsylvania Uniform Planned Community Act. *See, Code § 280-93.A.* The proposed townhomes are arranged in groups of four, five or six units respectively, in compliance with *Code § 280-93.B*, which allow up to eight townhomes to be attached in any one group. Each townhouse group is separated by at least 20 feet. *Code § 280-93.B.* The townhomes also meet the requirements for wall plane design. *Id.*

2. Permitted Density, Dimensional Standards etc.

The Project also complies with the other applicable standards for Density Modification Development. As noted above, the area of the Tract is over 7.5 acres, which exceeds the

minimum requirement of five acres in the R-4 District under Code §280-92.A(4). The proposed 41 townhome units on the Tract are in accordance with the density requirements of 5.5 units per acre. *Id.* No buildings are within 40 feet of an existing street right-of-way line or within 25 feet from any adjacent property line. *Code § 280-92.A(4).* The Plans also comply with the height, building coverage, and impervious surface requirements of Code 280-94, as shown in the Zoning Table on sheet 4 of 15 of the Plans.

Code § 280-97.D provides for yard setback requirements for townhomes developed under the Planned Community Act in lieu of the yard requirements of Code § 280-97.B. In the R-4 District, Code § 280-97.D requires a distance of at least 30 feet between the sides of groups of townhomes. The Plans satisfy these requirements. No accessory buildings or structures are proposed at this time, so Code § 280-97.E is not at issue. The Plans provide for ample parking in the proposed garages, the driveways, and in designated areas along the internal roadways that exceed Code parking requirements. *Code §§ 280-98.C; 280-103.B(1).* Pedestrian connectivity will be enhanced by the addition of sidewalks along the Tract frontage on Strafford Avenue and Eagle Road, and along the internal roadways. *Id.* As noted above, the Tract and general area are presently served by public sanitary sewer and public water, and the new dwelling units will likewise be served by these public facilities. *Code §280-96.*

3. Common Open Space & Buffers.

Code § 280-91 provides standards for common open space and for buffers where the Tract adjoins residentially zoned districts. As required, the Plans designate at least 15% of the Tract area as common open space that will be available for use by or enjoyment of the residents of the new homes. *Code §§ 280-91.A & F.* The proposed common open space is appropriate and suitable for recreation and open space purposes. *Code § 280-91.B.* The proposed common

open space is part of the development and is not separated from the Tract by existing roads. *Code § 280-91.C.* The proposed common open space includes a centralized green area that will be easily accessible to the proposed dwellings and help to, along with other portions of the common open space, preserve the character of the streetscape along Strafford Avenue and Eagle Road. *Code § 280-91.D.* No structures, parking areas, streets, or public facilities are proposed within the common open space. *Code §§ 280-91. D & F.* The common open space will be restricted as required by the Code. *Code § 280-91.E.* Along the Tract's property lines that border other lots that are residentially zoned, the required 25 feet wide buffer area will be provided. *Code § 280-91.G.*

The ownership and maintenance of the proposed common open space will be provided by a homeowners association that will be formed in compliance with the requirements of Code §280-99, and the applicable provisions of the Planned Community Act.

4. Application Requirements for a Density Modification Development

Code § 280-100.B sets forth specific information to be provided as part of the Conditional Use application. This information has been provided and is summarized as follows:

(1) The nature of the landowners' interest in the land to be developed.

As noted above, Applicant is the owner of the Tract.

(2) The density of land use to be allocated to the site to be developed.

In accordance with Code § 280-92.A(4), the permitted density is 5.5 units/per acre. The proposed density of forty-one units on 7.523 acres (+/-) satisfies that requirement.

(3) Location and size of common open space and the form of the organization proposed to own and maintain the common open space.

The common open space is shown on the Plans. The Tract will be subjected to the Pennsylvania Uniform Planned Community Act through a Declaration of Planned Community, which will be executed and recorded at the appropriate time. The

homeowners' association will own and maintain the common open space on behalf of the unit owners within the planned community. A draft of the Planned Community Declaration has been submitted to the Township and may be refined at a later stage.

(4) The use and the approximate height, bulk and location of dwellings and other structures.

The Plans depict the bulk and location of the proposed townhomes. Compliance with these provisions is described above and on the Plans. The height of the buildings will not exceed 35 feet per applicable Code requirements for Density Modification Development. It is noted that the permissible height of 35 feet is the same as allowed for conventional development in the R-4 District, and in many of the Township's other residential zoning districts.

(5) The feasibility of proposals for the disposition of sanitary waste and stormwater and provision of public water supply.

Public water and sanitary sewer are proposed. The Tract and the surrounding area are already served by public water and sanitary sewer.

(6) The substance of covenants, grants of easements or other restrictions proposed to be imposed upon the use of the land, buildings and structures, including proposed easements or grants for public utilities.

As noted above, the Tract will be subject to the Pennsylvania Uniform Planned Community Act. A draft of the Planned Community Declaration has been submitted to the Township as part of the filing of the Conditional Use Application. The Planned Community Declaration will be finalized at a later stage.

(7) A provision for parking of vehicles and the location and width of proposed streets and public ways.

The Plans depict the parking areas and internal drives. No public streets or ways are proposed. The Plans show four parking spaces for each dwelling unit, consisting of two garage parking spaces and two driveway parking spaces. There are also eleven additional parking spaces located on the internal drives. The proposed level of parking exceeds the two parking spaces per dwelling unit required by Code §280-103.B(1) and the one additional parking space per four dwelling units required by Code §255-29.A(20).

(8) A statement which will show the ecological and economic impact of the development on the Township and especially as to the surrounding areas.

The Tract and the surrounding area is within a fully-developed suburban area. The Tract will be redeveloped and will include common open space and stormwater management facilities, neither of which presently exist at the Tract. The Project will comply with applicable stormwater management requirements, and the requirements associated with the required outside agency permits, including a National Pollutant Discharge Elimination System (“NPDES”) permit which includes a further review of during-construction and post-construction stormwater management. The substantial investment in the redevelopment of the Tract will generate revenue for both the Township and the Radnor Township School District through increased property values and through the realty transfer taxes on the sale of each townhouse unit. A Fiscal Impact Study and a Development Impact Study further addressing these issues are included with this Conditional Use application.

(9) The results of traffic studies taken on the surrounding and nearby roads.

The Conditional Use Application submission includes a Traffic Impact Study dated August 25, 2020, prepared by F. Tavani and Associates, Inc.

4. NATURE OF A CONDITIONAL USE.

A conditional use is not an exception to a zoning ordinance, but rather, is a use to which an applicant is entitled unless objectors demonstrate, according to standards set forth in the zoning ordinance and the law, that the proposed use would adversely affect the community. *Blancett Maddock v. City of Pittsburgh Zoning Board of Adjustment*, 640 A.2d 498 (Pa. Cmwlth. 1994). A conditional use is nothing more than a special exception which falls within the jurisdiction of the municipal governing body as opposed to the zoning hearing board. *In re Thompson*, 896 A.2d 659, 670 (Pa. Cmwlth. 2006). The existence of a conditional use provision in a zoning ordinance indicates a legislative determination that the use is consistent with the municipality's zoning plan, and is a use which is presumptively consistent with the public health, safety and welfare. *Borough of Perkasié v. Moulton Builders*, 850 A.2d 778 (Pa. Cmwlth. 2004); *Ruddy v. Lower Southampton Township Zoning Hearing Board*, 669 A.2d 1051 (Pa. Cmwlth. 1995).

An applicant for conditional use approval has the burden of proving that the proposed use complies with the specific objective requirements of the zoning ordinance that are applicable to the conditional use. *Appeal of Neill*, 634 A.2d 749 (Pa. Cmwlth. 1993). Once an applicant for conditional use approval proves that the proposed use complies with the specific objective requirements for conditional use set forth in the zoning ordinance, the burden shifts to the objectors to demonstrate through substantial evidence that the proposed use would detrimentally affect the public health, safety and welfare, to a greater degree than the typical use in question. *Mann v. Lower Makefield Township*, 634 A.2d 768 (Pa. Cmwlth. 1993); *Dotterer v. Zoning Hearing Board of Upper Pottsgrove Township*, 588 A.2d 1023 (Pa. Cmwlth. 1991).

The Commonwealth Court enunciated the general rule governing special exceptions and conditional uses in *Appeal of Brickstone Realty Corp.*, 789 A.2d 333 (2001):

A special exception [or conditional use] is a conditionally permitted use, allowed by the Legislature if specifically listed standards are met. A special exception [or conditional use] is thus not an "exception" to the zoning ordinance, but a use permitted conditionally, the application for which is to be granted or denied by the zoning hearing board pursuant to express standards and criteria. Where a particular use is permitted in a zone by special exception [or conditional use], it is presumed that the local legislature has already considered that such use satisfies local concerns for the general health, safety, and welfare and that such use comports with the intent of the zoning ordinance. Thus, once the applicant for a special exception [or conditional use] shows compliance with the specific requirements of the ordinance, it is presumed that the use is consistent with the promotion of health, safety, and general welfare. The burden then shifts to objectors to prove that the proposed use is not, in fact, consistent with the promotion of health, safety and general welfare. (citations omitted)

The objectors' burden of demonstrating that a proposed use does not comply with the general health, safety and welfare criteria imposed upon conditional uses is not satisfied by showing that a proposed use will have effects that are no different than those which normally result from the construction of the permitted use. *Moyer's Landfill, Inc. v. Zoning Hearing*

Board of Lower Providence Township, 450 A.2d 273 (Pa. Cmwlth. 1982). Rather, a conditional use may only be denied if opponents demonstrate that the impact of the proposed use on the public welfare is greater than that which might be expected from such a use in normal circumstances. *Ruddy v. Lower Southampton Township Zoning Hearing Board*, *supra.*; *New Bethlehem Borough Council v. McVay*, 467 A.2d 395 (Pa. Cmwlth. 1983). Moreover, those who object to an application for a conditional use cannot meet their burden of showing that the proposed use would violate the health, safety and welfare of the community by merely speculating as to possible harm; rather, objectors must show a high degree of probability that the proposed use will substantially affect the health and safety of the community. *Manor Healthcare Corp. v. Lower Moreland Township Zoning Hearing Board*, 590 A.2d 65 (Pa. Cmwlth. 1991).

5. **CONCLUSION.**

As noted above, the Plans comply with the applicable provisions of the Township Zoning Ordinance for Density Modification Development in the R-4 District. The Plans will also provide for common open space, limited new curb cuts to the adjoining roadways, enhanced pedestrian connectivity, and provide for significant stormwater management where none exists. Accordingly, the Conditional Use should be granted.

Respectfully Submitted

A handwritten signature in blue ink, appearing to read "George W. Broseman", is written over a horizontal line.

GEORGE W. BROSEMAN, ESQUIRE
DANIEL P. ROWLEY, ESQUIRE
Attorneys for Applicant

Date: September 24, 2020

EXHIBIT A-1

REDACTED DEED FOR 208 & 228 STRAFFORD AVENUE

PREPARED BY AND RETURN TO:

Chicago Title Insurance Company

Suite 1325

1515 Market Street

Philadelphia, PA 19102

8109-25100(9)

RD BK04569-0060

2009038189 06/23/2009 01:57:53 PM:1

RCD FEE: \$100.00



36-RADNOR \$0.00

THOMAS J. JUDGE SR. ROD

DT-DEED

DELAWARE
COUNTY

This Indenture Made this 24th day of April , 2009

Between DORRANCE H. HAMILTON, a/k/a DORRANCE HILL HAMILTON,
of Boca Grande, Florida
(hereinafter called the Grantor)

AND

DORRANCE H. HAMILTON, MARGARET H. DUPREY, NATHANIEL PETER
HAMILTON, JR., S. MATTHEWS V. HAMILTON, JR., and BARBARA R. COBB,
TRUSTEES of the DORRANCE H. HAMILTON MARCH 15, 1996 REVOCABLE
AGREEMENT OF TRUST
(hereinafter called the Grantees)

Witnesseth that the said Grantor for and in consideration of the sum of
[REDACTED] lawful money of the United States of America, unto her well and
truly paid by the said Grantees at or before the sealing and delivery hereof, the receipt
whereof is hereby acknowledged, has granted, bargained and sold, aliened, enfeoffed
released and confirmed, and by these presents does grant, bargain and sell, alien,
enfeoff, release and confirm unto the said Grantees, their successors and assigns;

PREMISES "A"

ALL THAT CERTAIN lot or tract of land with the buildings and improvements thereon
erected, Situate in the Township of Radnor, County of Delaware, Commonwealth of
Pennsylvania, described according to a survey thereof, made by John F. Kauffman,
Civil Engineer on September 22, 1886, as follows, to wit:

BEGINNING at a point in the center line of a public road leading from the Philadelphia and Lancaster Turnpike to the Gulf Road and known as the Eagle Road, where the center line of a new Avenue leaving to Eagle Station meets the said center line of the said Eagle Road; thence along the center line of the said new Avenue North twenty five degrees, two minutes East three hundred twenty and twelve one-hundredths feet; thence by a line parallel with the said Eagle Road and along the line of land conveyed by Henry D. Hughes and wife to Richard T. McCarter, Jr., South sixty four degrees, fifty eight minutes West two hundred seventy two and fifteen one-hundredths feet to a point in line of land conveyed by William Robb to the said Clara T. Shaw; thence along the said last mentioned land by a line parallel with the aforesaid avenue South twenty five degrees, two minutes East three hundred twenty and twelve one-hundredths feet to the aforesaid center line of said Eagle Road; thence along the center line of said Eagle Road North sixty four degrees, fifty eight minutes East two hundred seventy two and fifteen one-hundredths feet to the first mentioned point and place of beginning.

ALSO ALL THAT CERTAIN tract of land, situate in the Township and County aforesaid,

BEGINNING at a stone in the center line of a public road called the Eagle Road the distance of two hundred thirty eight feet and one tenths of a foot measured along the center line of the said road Northeastwardly from the corner of land now or late of John B. Thayer; thence along the said center line of the said Road North sixty four degrees, fifty eight minutes East two hundred five and seven tenths feet to a stone set for a corner in the center line of an Avenue thirty three feet wide (lately vacated); thence along the center line of the said vacated Avenue being also a line of land conveyed by Harry C. Banks and wife to the said Clara T. Shaw North twenty five degrees, two minutes West three hundred twenty and twelve one-hundredths feet to a corner of land conveyed by William Robb to Richard T. McCarter Jr.; thence along the said land of the said Richard T. McCarter Jr., South sixty four degrees, fifty eight minutes West two hundred five and seven tenths feet to a point in the middle of land of Ellwood Wilson; thence along the said land South twenty five degrees, two minutes East three hundred twenty and twelve one-hundredths feet to the first mentioned point and place of beginning.

BEING known as 204 Strafford Avenue.

BEING designated as Uniform Parcel Identifier No. 36-01-00538-00.

PREMISES "B"

ALL THAT CERTAIN lot or piece of ground with the buildings and improvements thereon erected, situate in the Township of Radnor, County of Delaware, State of Pennsylvania, described according to a survey thereof made by Wilson R. and John B. Yerkes, Civil Engineers and Surveyors of Byrn Mawr, Pennsylvania, dated June 6, 1962, as follows:

BEGINNING at a point in the middle of Strafford Avenue (50 feet wide) said point is at the distance of three Hundred twenty and forty-five one hundredths feet (320.45') measured North twenty-five degrees two minutes West (N 25°02' W.) along the middle of Strafford Avenue from its intersection with the middle of Eagle Road; thence leaving Strafford Avenue by land of L. Weinrich South sixty-four degrees fifty-eight minutes West (S. 64°58' W.) four hundred seventy-seven and eighty-five one hundredths feet (477.85') to an iron pin in line of land of Lecian Von Bermuth; thence partly by the same and partly by land of various owners North twenty-five degrees two minutes West (N. 25°02' W.) four hundred twenty-one and fifty one hundredths feet (481.50') to a stone; thence by land of Elisabeth J. Grant North sixty-five degrees five minutes East (N. 65°05' E.) four hundred seventy-seven and eighty-five one hundredths feet (477.85') to a point in the middle of Strafford Avenue aforesaid; thence along the middle of same South twenty-five degrees two minutes East (S. 25°02' E.) four hundred twenty and forty-five one hundredths feet (420.45') to the place of beginning.

BEING known as 228 Strafford Avenue.

BEING designated as Uniform Parcel Identifier No. 36-01-00539-00 and 36-01-00540-00.

PREMISES "C"

ALL THAT CERTAIN lot or piece of ground with the buildings and improvements thereon erected, situate in the Township of Radnor, County of Delaware and Commonwealth of Pennsylvania, bounded and described according to a Map of Property of Wallace and Warner Corporation, made by M.R. and J.B. Yerkes, Civil Engineers, Bryn Mawr, Pennsylvania, dated October 24, 1946 and revised November 8, 1946, (new lot numbers) September 25, 1947, as follows, to wit:

N/A

BEGINNING at a spike marking the intersection of the middle lines of Fairfield Lane and Forrest Lane (both forty feet wide); thence along the middle of Forrest Lane, the two following courses and distances: (1) South thirty four degrees, thirty seven minutes East, nine and eighty four one-hundredths feet to a point; and (2) South twenty three degrees, forty seven minutes East, one hundred three feet to a point; thence leaving Forrest Lane, South sixty five degrees, seven minutes West, one hundred thirty four and eighty four one-hundredths feet to a stone; thence by other land of Wallace and Warner of which this is a part, North twenty four degrees West, one hundred seventeen and twenty nine one-hundredths feet to a point in the middle of Fairfield Lane; thence along the middle of same, North sixty seven degrees, two minutes East, one hundred thirty three and forty two one-hundredths feet to the place of beginning.

BEING Lot No. 12 on the above mentioned Plan.

BEING known as 142 Fairfield Lane.

BEING designated as Uniform Parcel Identifier No. 36-01-00200-00.

PREMISES "D"

ALL THAT CERTAIN lot or piece of ground with the buildings and improvements thereon to be erected, situate in the Township of Radnor, County of Delaware and Commonwealth of Pennsylvania and described according to a Map of Property of Edwin T. Myskowski, et ux made by Yerkee Engineering Co., Surveyors, Bryn Mawr, Pennsylvania dated January 3, 1969 and last revised February 3, 1969 as follows:

BEGINNING at a point in the middle line of Forrest Lane (proposed to be opened to the width of 30 feet), which said point is measured, the two following courses and distances along said middle line from its intersection with the extended middle line of Fairfield Lane (forty feet wide): (1) from said point of intersection South thirty four degrees, thirty seven minutes East, nine and eighty four one hundredths feet and (2) South twenty three degrees, forty seven minutes East, forty three and forty six one-hundredths feet to the point and place of beginning; thence extending from said beginning point leaving Forest Lane by other land of Edwin T. Myskowski of which this was a part, North sixty five degrees seven minutes East, one hundred twenty three feet to a point in line of land now or late of J.B. Parker; thence extending along said land South twenty three degrees, forty seven minutes East, fifty nine and fifty four one-hundredths feet to a point a corner of land now or late of Lecian Von Bermuth; thence extending along said land South sixty five degrees, seven minutes West, one hundred twenty three feet to a point in the middle line of Forest Lane aforesaid; thence extending along the middle line of same North twenty three degrees, forty seven minutes West, fifty nine and fifty four one-hundredths feet to the place of beginning.

BEING Lot No. 2 as shown on said Plan.

BEING known as 22 Forrest Lane.

BEING designated as Uniform Parcel Identifier No. 36-01-00236-03.

BEING AS TO PREMISES "A" the same premises which H. Everett Shore, III, Straw Party by deed dated 6/3/1977 and recorded 6/10/1977 in the Office of the Recorder of Deeds in and for the County of Delaware in Deed Book 2609 page 716, granted and conveyed unto Dorrance H. Hamilton, in fee.

BEING AS TO PREMISES "B" the same premises which Thomas Hart and S. Leonard Kent, Jr., Executors under the Will of J. Brooks B. Parker, deceased by Deed dated 6/27/1952 and recorded 7/2/1952 in the Office of the Recorder of Deeds in and for the County of Delaware in Deed Book 1532 page 347, granted and conveyed unto Dorrance Hill Hamilton, in fee.

BEING AS TO PREMISES "C" the same premises which Gerard V. Cavanaugh and Danielle Cavanaugh, his wife by Deed dated 7/1/1985 and recorded 7/15/1985 in the Office of the Recorder of Deeds in and for the County of Delaware in Volume 251 page 1525, granted and conveyed unto Dorrance H. Hamilton, in fee.

BEING AS TO PREMISES "D" the same premises which H. Everett Shore, III, Straw Party by Deed dated 1/19/1984 and recorded 2/23/1984 in the Office of the Recorder of Deeds in and for the County of Delaware in Volume 139 page 983, granted and conveyed unto Dorrance H. Hamilton, in fee.

Together with all and singular the buildings, improvements, ways, streets, alleys, driveways, passages, waters, water-courses, rights, liberties, privileges, hereditaments and appurtenances, whatsoever unto the hereby granted premises belonging, or in any wise appertaining, and the reversions and remainders, rents, issues, and profits thereof; and all the estate, right, title, interest, property, claim and demand whatsoever of the said Grantor, as well at law as in equity, of, in, and to the same.

UNDER AND SUBJECT, nevertheless to matters of record, to the extent valid and enforceable, subsisting and in fact affect title to the property or any part thereof.

To have and to hold the said lot or piece of ground above described with the buildings and improvements thereon erected; with the hereditaments and premises hereby granted, or mentioned, and intended so to be, with the appurtenances, unto the said Grantees, their successors and assigns to and for the only proper use and behoof of the said Grantees, their successors and assigns, forever. Under and subject, as aforesaid.

And the said Grantor, for herself, her heirs, executors and administrators does by these presents, grant, covenant, promise and agree, to and with the said Grantees, their successors and assigns, that she the said Grantor, her heirs, all and singular the hereditaments and premises herein above described and granted, or mentioned and intended so to be with the appurtenances unto the said Grantees, their successors and assigns, against her the said Grantor, her heirs and against all and every person or persons whomsoever lawfully claiming or to claim the same or any part thereof, by, from, or under him, her, them or any of them shall and will, **UNDER AND SUBJECT** as aforesaid, **WARRANT** and forever **DEFEND**.

In Witness Whereof, the said Grantor has caused these presents to be duly executed and dated the day and year first above written.

Sealed and Delivered
IN THE PRESENCE OF US

Dorrance H. Hamilton (SEAL)
Dorrance H. Hamilton

Dorrance H. Hamilton (SEAL)
Dorrance Hill Hamilton

In Witness Whereof, I hereunto set my hand and official seal.

NOTARIAL SEAL
ROSEMARIE COLYAS
Notary Public
RADNOR TWP, DELAWARE COUNTY
My Commission Expires Oct 16, 2011

Rosemarie Polyzos
Notary Public

My Commission Expires: 10/16/2011

REV-183 EX (11-04)



COMMONWEALTH OF PENNSYLVANIA
DEPARTMENT OF REVENUE
BUREAU OF INDIVIDUAL TAXES
PO BOX 280603
HARRISBURG PA 17128-0603

REALTY TRANSFER TAX STATEMENT OF VALUE

See Reverse for Instructions

RECORDER'S USE ONLY

State Tax Paid 0
Book Number 4569
Page Number 60
Date Recorded 6/23/09

Complete each section and file in duplicate with Recorder of Deeds when (1) the full value/consideration is not set forth in the deed, (2) when the deed is without consideration, or by gift, or (3) a tax exemption is claimed. A Statement of Value is not required if the transfer is wholly exempt from tax based on: (1) family relationship or (2) public utility easement. If more space is needed, attach additional sheet(s).

A. CORRESPONDENT - All inquiries may be directed to the following person:

Name Anthony T. Fratianno, Esquire, Morgan, Lewis & Bockius, LLP Telephone Number: _____
Street Address 1701 Market Street City Philadelphia State PA Zip Code 19103

B. TRANSFER DATA

Date of Acceptance of Document

Grantor(s)/Lessor(s) Dorrance H. Hamilton, a/k/a Dorrance Hill Hamilton Grantee(s)/Lessee(s) Trustees of the Dorrance H. Hamilton 3/15/96 Rev. Agr. of Tr
Street Address _____ Street Address Suite 316, 200 Eagle Road
City Boca Grande State FL Zip Code _____ City Wayne State PA Zip Code 19087

C. PROPERTY LOCATION

Street Address See Exhibit "A" City, Township, Borough Radnor Township
County Delaware School District _____ Tax Parcel Number See Exhibit "A"

D. VALUATION DATA

1. Actual Cash Consideration 2. Other Consideration 3. Total Consideration =
4. County Assessed Value 5. Common Level Ratio Factor 6. Fair Market Value = See Exhibit "A"

E. EXEMPTION DATA

1a. Amount of Exemption Claimed 1b. Percentage of Interest Conveyed

2. Check Appropriate Box Below for Exemption Claimed

- ☐ Will or intestate succession _____ (Name of Decedent) _____ (Estate File Number)
- ☐ Transfer to Industrial Development Agency.
- ☐ Transfer to a trust. (Attach complete copy of trust agreement identifying all beneficiaries.)
- ☐ Transfer between principal and agent. (Attach complete copy of agency/straw party agreement.)
- ☐ Transfers to the Commonwealth, the United States and Instrumentalities by gift, dedication, condemnation or in lieu of condemnation. (If condemnation or in lieu of condemnation, attach copy of resolution.)
- ☐ Transfer from mortgagor to a holder of a mortgage in default. Mortgage Book Number _____, Page Number _____
- ☐ Corrective or confirmatory deed. (Attach complete copy of the prior deed being corrected or confirmed.)
- ☐ Statutory corporate consolidation, merger or division. (Attach copy of articles.)
- ☒ Other (Please explain exemption claimed, if other than listed above.) 72 P.S. 8102 C.3 (8.1) A transfer for no or nominal actual consideration to a trustee of a living trust from the settlor of the living trust.

Under penalties of law, I declare that I have examined this Statement, including accompanying information, and to the best of my knowledge and belief, it is true, correct and complete.

Signature of Correspondent/Responsible Party [Signature]

Date 4/24/09

FAILURE TO COMPLETE THIS FORM PROPERLY OR ATTACH APPLICABLE DOCUMENTATION MAY RESULT IN THE RECORDER'S REFUSAL TO RECORD THE DEED.

EXHIBIT "A"

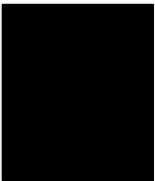

Section C: Street Address

204 Strafford Avenue (Premises "A")
228 Strafford Avenue (Premises "B")
142 Fairfield Lane (Premises "C")
22 Forrest Lane (Premises "B")



Section C: Tax Parcel Number

Uniform Parcel Identifier No. 36-01-00538-00 (Premises "A")
Uniform Parcel Identifier No. 36-01-00539-00 and
No. 36-01-00540-00 (Premises "B")
Uniform Parcel Identifier No. 36-01-00200-00 (Premises "C")
Uniform Parcel Identifier No. 36-01-00236-03 (Premises "D")

Section D-4: County Assessed Value

	(Premises "A")
	(Premises "B")
	(Premises "B")
	(Premises "C")
	(Premises "D")
TOTAL 	

Section D-6: Fair Market Value

	(Premises "A")
	(Premises "B")
	(Premises "B")
	(Premises "C")
	(Premises "D")
TOTAL 	

CTIC File #08-PHI-1046

DEED

Grantor: DORRANCE H. HAMILTON, a/k/a DORRANCE HILL HAMILTON

TO

Grantees: DORRANCE H. HAMILTON, MARGARET H. DUPREY,
NATHANIEL PETER HAMILTON, JR.,
S. MATTHEWS V. HAMILTON, JR., and
BARBARA R. COBB, TRUSTEES of the DORRANCE H. HAMILTON
MARCH 15, 1996 REVOCABLE AGREEMENT OF TRUST

Premises: 204 Strafford Avenue (Premises "A")
228 Strafford Avenue (Premises "B")
142 Fairfield Lane (Premises "C")
22 Forrest Lane (Premises "D")
Township of Radnor
County of Delaware
Commonwealth of Pennsylvania

Uniform Parcel Identifier No. 36-01-00538-00 (Premises "A")
Uniform Parcel Identifier No. 36-01-00539-00 and
36-01-00540-00 (Premises "B")
Uniform Parcel Identifier No. 36-01-00200-00 (Premises "C")
Uniform Parcel Identifier No. 36-01-00236-03 (Premises "D")

The address of the above named Grantees is:

218 Enterprises
Suite 316
200 Eagle Road
Wayne, PA 19087

Certified by: 

EXHIBIT A-2

REDACTED DEED FOR 18 FORREST LANE

6
RETURN TO:
Chicago Title Insurance Co
1601 Market Street, Ste. #2550
Philadelphia, PA 19103
8103-00452 (36)

This Indenture

Made this 17th day of October, 2003

Between James W. Lament and Elizabeth A. Ashdale-Lament
(hereinafter called the Grantors)

AND

Trustees Under Agreement of Trust dated March 15, 1996 as Amended,
Dorrance H. Hamilton, Settlor
(hereinafter called the Grantees)

Witnesseth That the said Grantor for and in consideration of the sum of [REDACTED] lawful money of the United States of America, unto them well and truly paid by the said Grantees at or before the sealing and delivery hereof, the receipt whereof is hereby acknowledged, has granted, bargained and sold, released and confirmed and by these presents does grant, bargain and sell, release and confirm unto the said Grantees, their successors and assigns.

ALL THAT CERTAIN lot or piece of ground with the buildings and improvements thereon erected, Situate in the Township of Radnor, County of Delaware, Commonwealth of Pennsylvania and described according to a Map or Property of Edwin I. Myskowski, et ux, made by Yerkes Engineering Co., Surveyors, Bryn Mawr, PA, dated 1/3/1969 as follows, to wit:

BEGINNING at a point on the middle line of Forrest Lane of variable widths at the distance of 674.37 feet measured Southeastwardly along said middle line of Forrest Lane from the center line of Old Eagle School Road; thence extending from said point of beginning North 65 degrees 07 minutes 00 seconds East, along the Strafford Realty Company Subdivision Plan 129.79 feet to a point; thence extending South 23 degrees 47 minutes 00 seconds East along land now or late of J. B. Parker, 79.06 feet to a point; thence extending South 65 degrees 07 minutes 00 seconds West along Lot No. 2 on said plan 123 feet to a point on the middle line of Forrest Lane, aforesaid; thence extending along the same, the two following courses and distances: (1) North 23 degrees 47 minutes 00 seconds West 43.46 feet to a point; and (2) North 34 degrees 37 minutes 00 seconds West 36.11 feet to the first mentioned point and place of beginning.

BEING Lot No. 1 on said Plan.

BEING Folio Number 36-01-00236-02

RD BK02991-1480

2003144115 10/28/2003 10:52:44 AM

REC FILE \$54.00 PA \$1.00 TAX \$4.50C 00 \$1.00 TAX \$3.200000



DELAWARE COUNTY

THOMAS J. JUDGE SR. REC

Power of Attorney

REALTY TRANSFER TAX STATEMENT OF VALUE

See Reverse for Instructions

State Tax No.	\$ 3,300.00
Book Number	2771
Page No.	170
Date Recorded	10/17/03

Complete each section and file in duplicate with Recorder of Deeds when (1) the full value/consideration is not set forth in the deed, (2) when the deed is without consideration, or by gift, or (3) a tax exemption is claimed. A Statement of Value is not required if the transfer is wholly exempt from tax based on: (1) family relationship or (2) public utility easement. If more space is needed, attach additional sheet(s)

A - CORRESPONDENT - All inquiries may be directed to the following person:

Name	Chicago Title Insurance Company	Telephone Number	
Street Address	1601 Market Street	Area Code	215
City	Philadelphia	State	PA
		Zip Code	19103

B - TRANSFER DATA

Grantor(s)/Lessor(s)	James W. Lament & Elizabeth Lament	Date of Acceptance of Document	
Street Address	18 Forrest Road	Grantee(s)/Lessee(s)	Trustee under Agreement of Trust dated 3/15/96 as amended
City	Wayne	State	PA
State	PA	Zip Code	19087

C - PROPERTY LOCATION

Street Address	18 Forrest Road	City, Township, Borough	Radnor
County	Delaware	School District	Radnor
		Tax Parcel Number	36-01-00236-02

D - VALUATION DATA

1. Actual Cash Consideration		2. Other Consideration		3. Total Consideration	
4. County Assessed Value		5. Common Level Ratio Factor		6. Fair Market Value	

E - EXEMPTION DATA

1a. Amount of Exemption Claimed		1b. Percentage of Interest Conveyed	

2. Check Appropriate Box Below for Exemption Claimed

- ☐ Will or intestate succession (Name of Decedent) _____ (Estate File Number) _____
- ☐ Transfer to Industrial Development Agency
- ☐ Transfer to a trust. (Attach complete copy of trust agreement identifying all beneficiaries.)
- ☐ Transfer between principal and agent. (Attach complete copy of agency/straw party agreement.)
- ☐ Transfers to the Commonwealth, the United States and instrumentalities by gift, dedication, condemnation or in lieu of condemnation. (If condemnation or in lieu of condemnation, attach copy of resolution.)
- ☐ Transfer from mortgagor to a holder of a mortgage in default. Mortgage Book Number _____, Page Number _____
- ☐ Corrective or confirmatory deed. (Attach complete copy of the prior deed being corrected or confirmed.)
- ☐ Statutory corporate consolidation, merger or division. (Attach copy of articles.)
- ☐ Other (Please explain exemption claimed, if other than listed above.) _____

Under penalties of law, I declare that I have examined this Statement, including accompanying information, and to the best of my knowledge and belief, it is true, correct and complete.

Signature of Correspondent or Responsible Party	Date
<i>[Signature]</i>	10/17/03

FAILURE TO COMPLETE THIS FORM PROPERLY OR ATTACH APPLICABLE DOCUMENTATION MAY RESULT IN THE RECORDER'S REFUSAL TO RECORD THE DEED.

BEING the same premises which Madeline L. Fisher, by Indenture bearing date April 29, 2003 and recorded MAY 5, 2003 in the Office of the Recorder of Deeds, in and for the County of Delaware in Volume 2761 page 2189 etc., granted and conveyed unto James W. Lament and Elizabeth A. Ashdale-Lament, Husband and Wife, in fee

(Remainder of page intentionally left blank)

Together with all and singular the improvements, ways, streets, alleys, driveways, passages, waters, water-courses, rights, liberties, privileges, hereditaments and appurtenances, whatsoever unto the hereby granted premises belonging, or in any wise appertaining, and the reversions and remainders, rents, issues, and profits thereof; and all the estate right, title, interest, property, claim and demand whatsoever of the said Grantors, as well at law as in equity, of, in, and to the same.

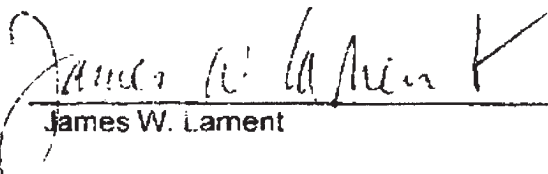
To have and to hold the said lot or piece of ground above described with the buildings and improvements thereon erected; with the hereditaments and premises hereby granted, or mentioned, and intended so to be, with the appurtenances, unto the said Grantees, their successors and assigns to and for the only proper use and behoof of the said Grantees, their successors and assigns, forever

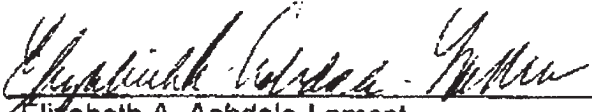
And the said Grantors, for themselves, their heirs, executors, administrators do by these presents, covenant, grant and agree, to and with the said Grantees, their successors, and assigns, that they the said Grantors, their heirs, all and singular the Hereditaments and premises herein above described and granted, or mentioned and intended so to be with the Appurtenances unto the said Grantees, their successors and assigns, against them the said Grantors, their heirs and against all and every Person or Persons whomsoever lawfully claiming or to claim the same or any part thereof, by from, or under him, her, them or any of them shall and will WARRANT and forever DEFEND.

In Witness Whereof, the said Grantors have caused these presents to be duly executed dated the day and year first above written.

Sealed and Delivered

IN THE PRESENCE OF US


James W. Lament (SEAL)


Elizabeth A. Ashdale-Lament (SEAL)

7007144115 Page: 1484.00

DEED

Grantor: James W. Lament and Elizabeth A. Ashdale-Lament

TO

Grantee: Trustees Under Agreement of Trust dated March 15,
1996, as Amended, Dorrance H. Hamilton, Settlor

PREMISES: 18 Forrest Lane
Folio Number 36-01-00236-02
Radnor Township
Delaware County

The address of the above named Grantees is:

Suite 316
200 Eagle Road
Wayne, PA 19087

Certified by:

Gail H. Hamilton

EXHIBIT “B”

PRELIMINARY LAND DEVELOPMENT PLAN

CONDITIONAL USE AND PRELIMINARY LAND DEVELOPMENT PLAN SET

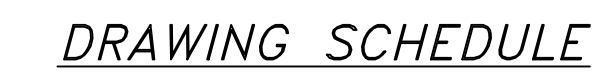


- CIVIL ENGINEER:
SITE ENGINEERING CONCEPTS, LLC

LANDSCAPE ARCHITECT:
GLACKIN THOMAS PANZAK, INC.

APPLICANT/OWNER:
THE TRUSTEES OF THE DORRANCE HAMILTON
3/15/1996 REVOCABLE AGREEMENT OF TRUST

ATTN: CHARLES HOUDER
551 W LANCASTER AVE, SUITE 307
HAVERFORD, PA 19041
P: (610) 389-0305
E: DCH@HAVERFORDPROPERTIES.COM



1. COVER SHEET
2. EXISTING CONDITIONS PLAN
3. DEMOLITION PLAN
4. RECORD PLAN
5. POST CONSTRUCTION STORMWATER PLAN
6. DURING CONSTRUCTION E&S
7. PCSM DETAILS
8. CONSTRUCTION DETAILS
9. E&S DETAILS
10. PROFILES
11. PROFILES
12. LP-1 LANDSCAPE PLAN
13. LP-2 LANDSCAPE DETAILS
14. LI-1 LIGHTING PLANS
15. LI-2 LIGHTING DETAILS

NUM.	DATE	REVISION
PLAN PREPARED BY:		

SITE ENGINEERING CONCEPTS, LLC

P.O. BOX 1992

SOUTHEASTERN, PA 19399

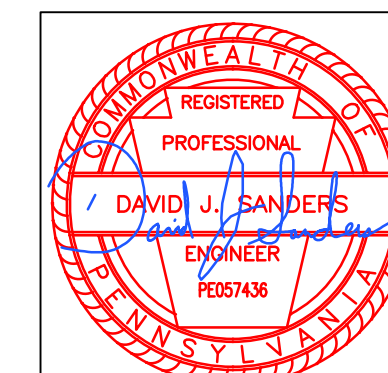
P: 610-240-0450	F: 610-240-0451	E: INFO@SITE-ENGINEERS.COM
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PLAN PREPARED FOR:		
<div style="text-align: center;"><i>HAMILTON ESTATE</i></div> <div style="margin-top: 20px;"> <p>208 & 228 STRAFFORD AVE AND 18 FORREST LANE</p> <p>WAYNE, PA 19087</p> </div>		

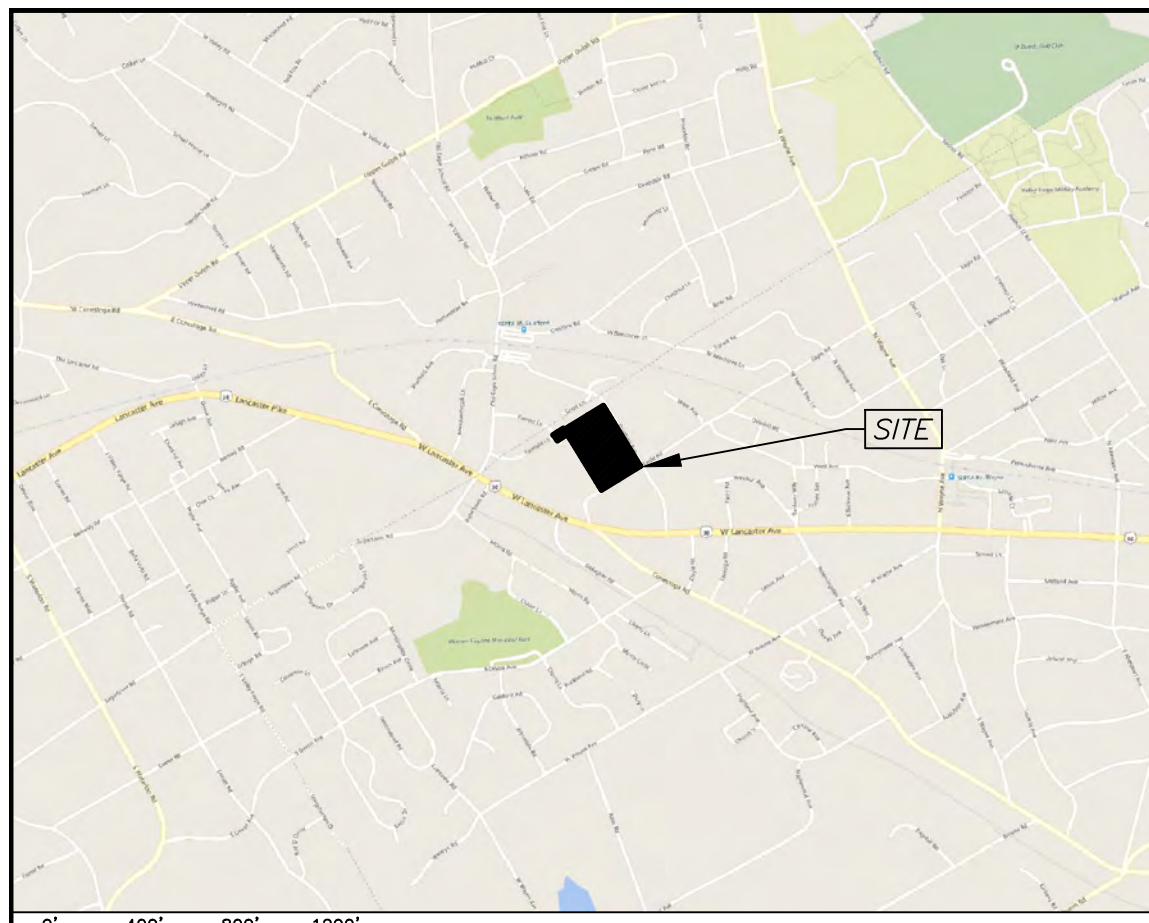
RADNOR TOWNSHIP	DELAWARE COUNTY	PENNSYLVANIA
-----------------	-----------------	--------------

COVER SHEET	SHEET 1 of 15
-------------	----------------------

SCALE: 1" = 200'

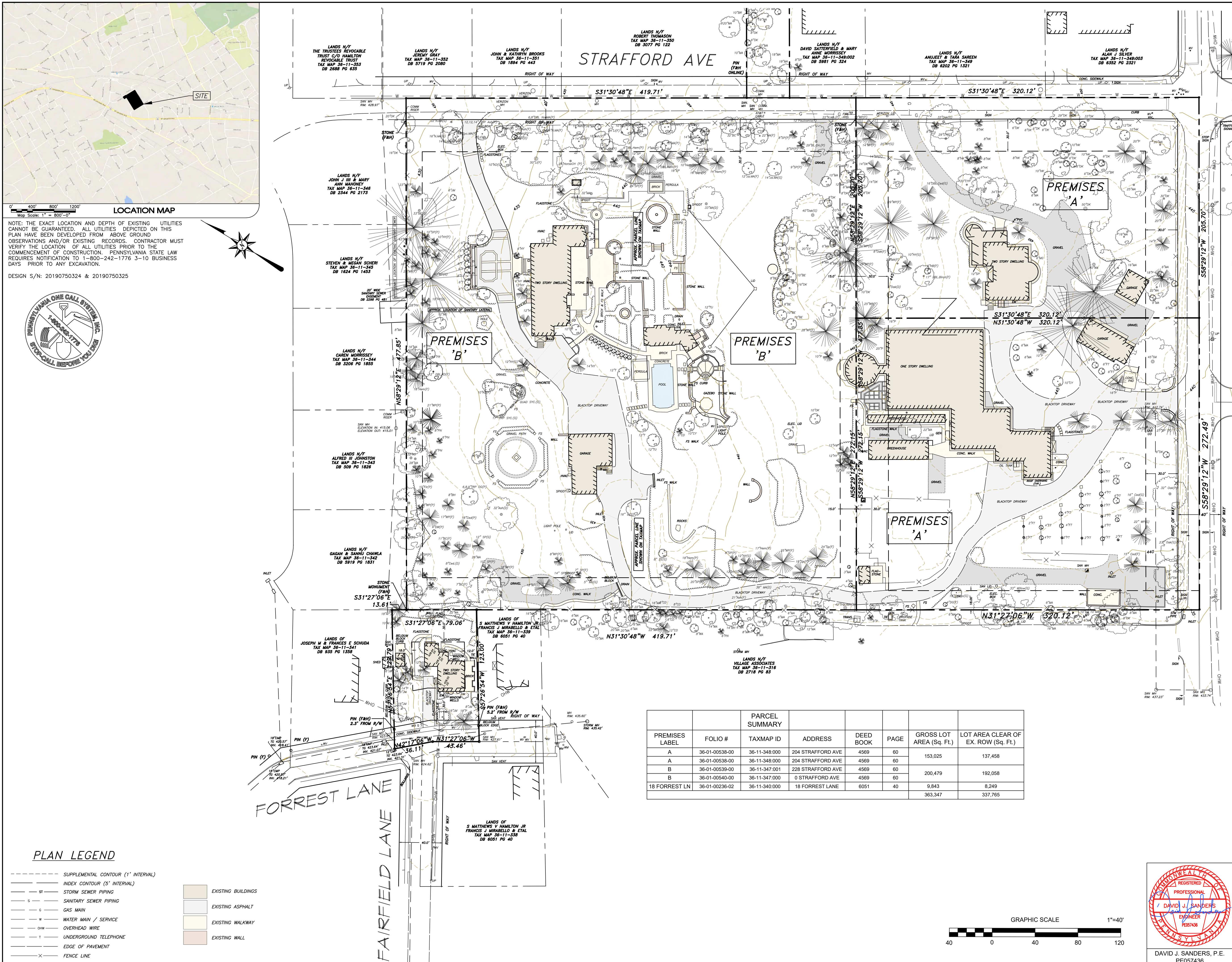


DAVID J. SANDERS, P.E.
PE057436



NOTE: THE EXACT LOCATION AND DEPTH OF EXISTING UTILITIES CANNOT BE GUARANTEED. ALL UTILITIES DEPICTED ON THIS PLAN HAVE BEEN DEVELOPED FROM ABOVE GROUND OBSERVATIONS AND/OR EXISTING RECORDS. CONTRACTOR MUST VERIFY THE LOCATION OF ALL UTILITIES PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. PENNSYLVANIA STATE LAW REQUIRES NOTIFICATION TO 1-800-242-1776 3-10 BUSINESS DAYS PRIOR TO ANY EXCAVATION.

DESIGN S/N: 20190750324 & 20190750325



GENERAL NOTES

- BEARINGS SHOWN ARE BASED ON A MERIDIAN ESTABLISHED USING PA STATE PLANE COORDINATES. MERIDIAN DIFFERS FROM THE RECORDED DEED BY 6°35'48".
- CONTOURS PLOTTED FROM FIELD RUN SURVEY. GPS CONTROL PERFORMED ON SITE TO ESTABLISH BENCHMARK. DATUM NAD83/NAVOD88.
- THIS SURVEY WAS PERFORMED WITH THE BENEFIT OF A CURRENT TITLE REPORT.
- THIS SURVEY IS NOT TO BE USED FOR TREE REMOVAL ALONG PROPERTY LINES. TREE LOCATIONS ARE APPROXIMATE. FOR TREE REMOVAL PURPOSES, PROPERTY LINES MUST BE FIELD MARKED WITH STAKES SET TO ESTABLISH EXACT TREE OWNERSHIP.
- THERE IS NO IDENTIFIABLE FEMA FLOOD PLAIN AREAS WITHIN THE PROJECT SITE AS ILLUSTRATED ON COMMUNITY PANEL NUMBER 420428 0017F OF THE FLOOD INSURANCE RATE MAP AS PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY FOR THE TOWNSHIP OF RADNOR, PENNSYLVANIA.
- EXISTING SUBSURFACE UTILITY INFORMATION INDICATED IS BASED UPON VISUAL FIELD INSPECTION BY JEFFERY P. TURNER, PLS. SUCH INFORMATION CONCERNING THE SIZE, LOCATION, DEPTH, QUANTITY, ETC. OF SUBSURFACE UTILITIES IS APPROXIMATE IN NATURE AND HAS BEEN OBTAINED AS A SUPPLEMENT TO THE SITE SURVEY. THE INFORMATION PROVIDED IS REPRESENTATIVE OF SUBSURFACE CONDITIONS ONLY AT LOCATIONS AND DEPTHS WHERE SUCH INFORMATION WAS OBTAINED. THERE IS NO EXPRESSED OR IMPLIED AGREEMENT THAT UTILITY SIZE, LOCATION, DEPTH, QUANTITY, ETC. AS SHOWN EXISTS BETWEEN EXPLORED LOCATIONS. ACCORDINGLY, UTILITY INFORMATION SHOWN SHOULD NOT BE RELIED UPON FOR CONSTRUCTION PURPOSES. IT IS INCUMBENT UPON THE CONTRACTOR TO VERIFY THE SIZE, LOCATION, DEPTH, QUANTITY, ETC. OF ALL UTILITIES BEFORE EXCAVATION. THE EXISTENCE AND/OR LOCATION OF ALL SUBSURFACE UTILITIES SHALL BE CONSIDERED APPROXIMATE AND MUST BE FIELD VERIFIED BY ALL CONTRACTORS PRIOR TO ANY CONSTRUCTION. NO GUARANTEE IS MADE THAT UNDERGROUND UTILITIES ARE ACCURATELY OR COMPLETELY SHOWN HEREON.
- PLANS REFERENCED: LOT LINE CHANGE PLAN PREPARED BY YERKES ASSOCIATES, INC. FOR MRS. S.M.V HAMILTON DATED JULY 21, 2001.

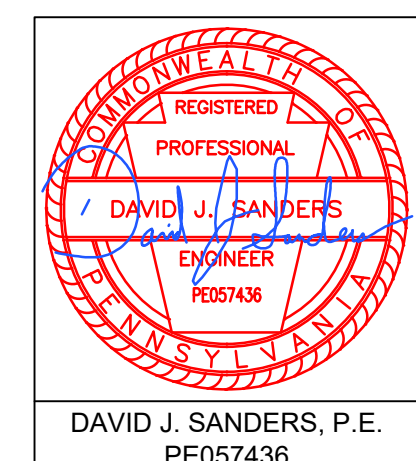
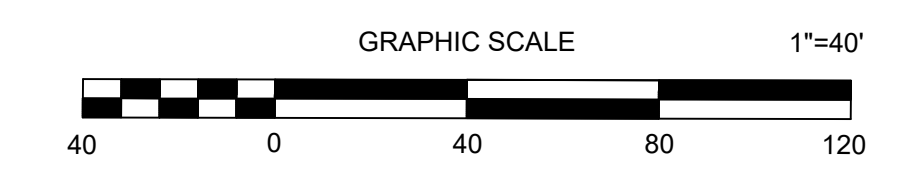
ZONING SUMMARY

R-4 - RESIDENTIAL DISTRICT	
ORDINANCE ITEM	REQUIREMENT
MIN. LOT AREA	7,000 SF
MIN. LOT WIDTH @ BLDG	55 FT
MIN. SETBACKS	
FRONT	30 FT
SIDE (INDIVIDUAL/AGGREGATE)	12 FT / 30 FT
REAR	30 FT
MAX. BUILDING HEIGHT	35 FT
MAX. BUILDING COVERAGE	30%
MAX. IMPERVIOUS COVERAGE	40%
RIPARIAN BUFFER SETBACK	25 FT

PARCEL SUMMARY							
PREMISES LABEL	FOLIO #	TAXMAP ID	ADDRESS	DEED BOOK	PAGE	GROSS LOT AREA (Sq. Ft.)	LOT AREA CLEAR OF EX. ROW (Sq. Ft.)
A	36-01-00538-00	36-11-348-000	204 STRAFFORD AVE	4569	60	153,025	137,458
A	36-01-00538-00	36-11-348-000	204 STRAFFORD AVE	4569	60		
B	36-01-00539-00	36-11-347-001	228 STRAFFORD AVE	4569	60	200,479	192,058
B	36-01-00540-00	36-11-347-000	0 STRAFFORD AVE	4569	60	9,843	8,249
18 FORREST LN	36-01-00236-02	36-11-340-000	18 FORREST LANE	6051	40	363,347	337,765

PLAN LEGEND

- SUPPLEMENTAL CONTOUR (1' INTERVAL)
- INDEX CONTOUR (5' INTERVAL)
- ST --- STORM SEWER PIPING
- S --- SANITARY SEWER PIPING
- G --- GAS MAIN
- W --- WATER MAIN / SERVICE
- OW --- OVERHEAD WIRE
- T --- UNDERGROUND TELEPHONE
- --- EDGE OF PAVEMENT
- X --- FENCE LINE
- EXISTING BUILDINGS
- EXISTING ASPHALT
- EXISTING WALKWAY
- EXISTING WALL

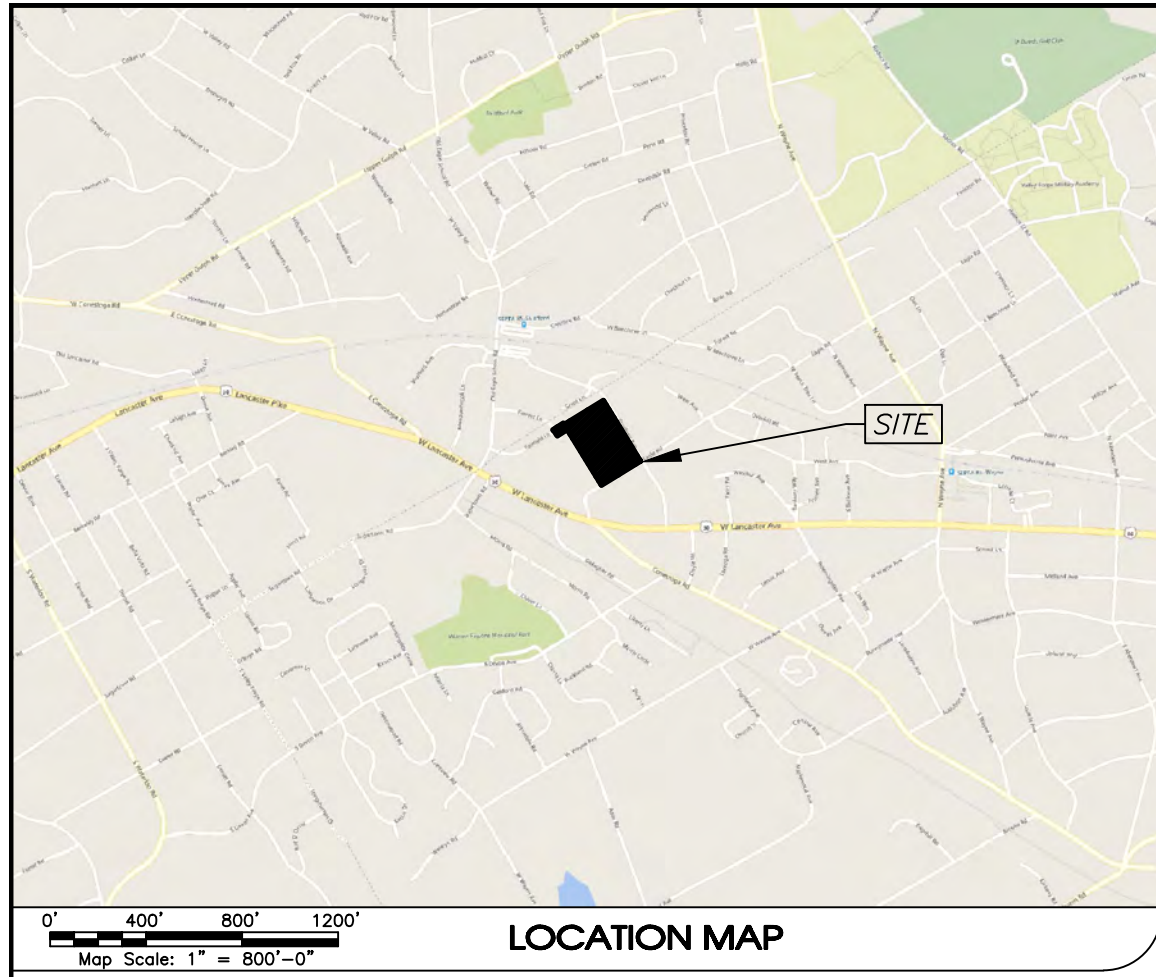


NUM. DATE REVISION

PLAN PREPARED BY:
SITE ENGINEERING CONCEPTS, LLC
P.O. BOX 1992
SOUTHEASTERN, PA 19399
P: 610-240-0450 F: 610-240-0451 E: INFO@SITE-ENGINEERS.COM
PLAN PREPARED FOR:
HAMILTON ESTATE
208 & 228 STRAFFORD AVE AND 18 FORREST LANE
WAYNE, PA 19087
RADNOR TOWNSHIP DELAWARE COUNTY PENNSYLVANIA
DATE: AUGUST 27, 2020

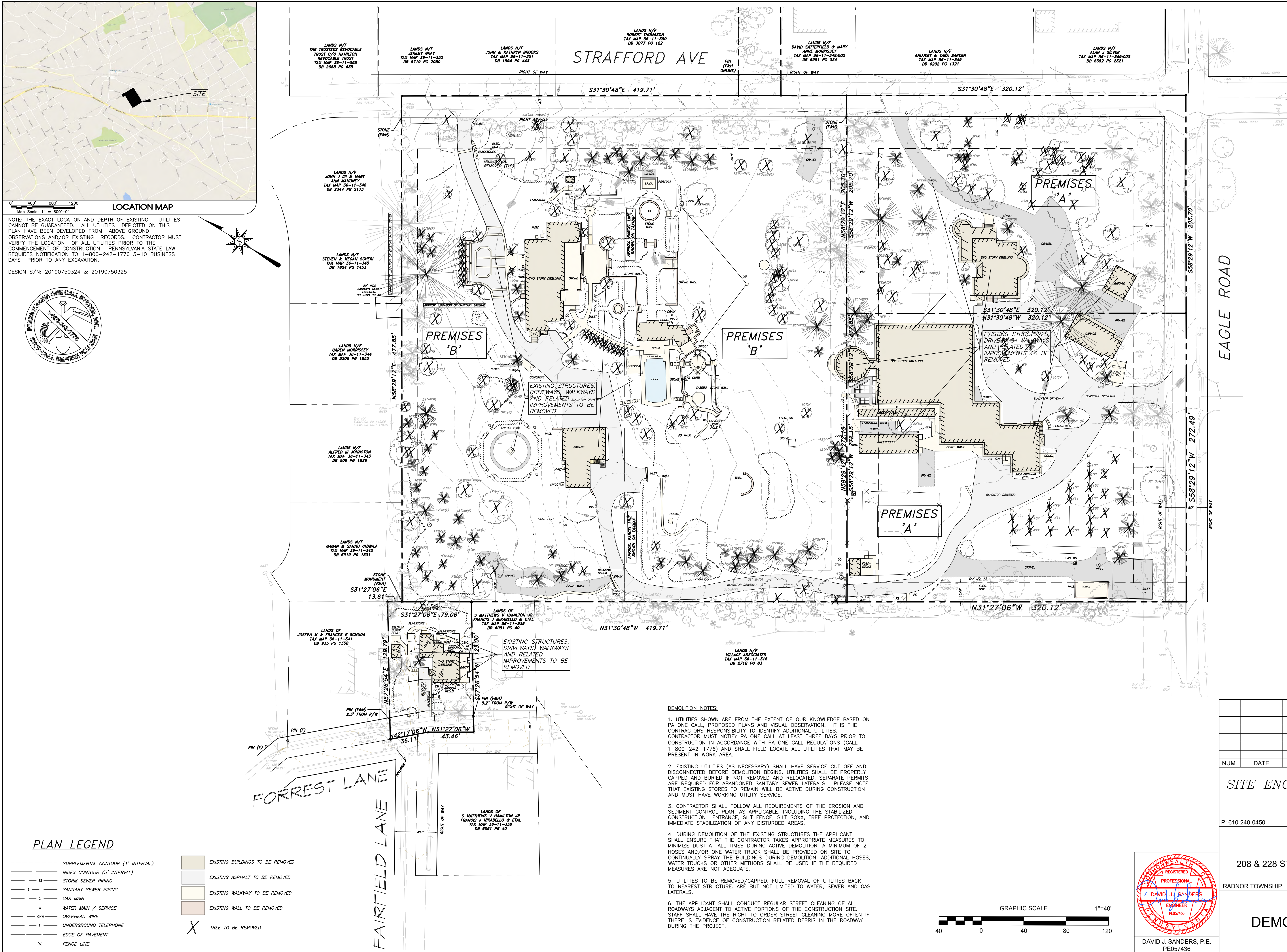
EXISTING CONDITIONS PLAN

SHEET 2 of 15
SCALE: 1" = 40'



NOTE: THE EXACT LOCATION AND DEPTH OF EXISTING UTILITIES CANNOT BE GUARANTEED. ALL UTILITIES DEPICTED ON THIS PLAN HAVE BEEN DEVELOPED FROM ABOVE GROUND OBSERVATIONS AND/OR EXISTING RECORDS. CONTRACTOR MUST VERIFY THE LOCATION OF ALL UTILITIES PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. PENNSYLVANIA STATE LAW REQUIRES NOTIFICATION TO 1-800-242-1776 3-10 BUSINESS DAYS PRIOR TO ANY EXCAVATION.

DESIGN S/N: 20190750324 & 20190750325



DEMOLITION NOTES:

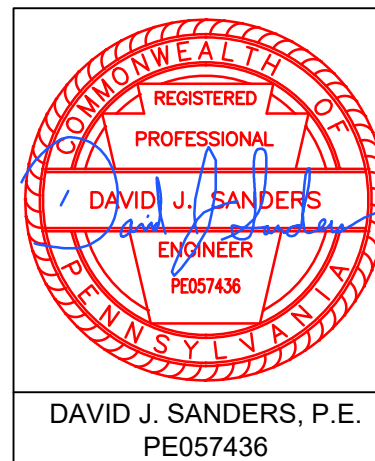
- UTILITIES SHOWN ARE FROM THE EXTENT OF OUR KNOWLEDGE BASED ON PA ONE CALL, PROPOSED PLANS AND VISUAL OBSERVATION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO IDENTIFY ADDITIONAL UTILITIES. CONTRACTOR MUST NOTIFY PA ONE CALL AT LEAST THREE DAYS PRIOR TO CONSTRUCTION IN ACCORDANCE WITH PA ONE CALL REGULATIONS (CALL 1-800-242-1776) AND SHALL FIELD LOCATE ALL UTILITIES THAT MAY BE PRESENT IN WORK AREA.
- EXISTING UTILITIES (AS NECESSARY) SHALL HAVE SERVICE CUT OFF AND DISCONNECTED BEFORE DEMOLITION BEGINS. UTILITIES SHALL BE PROPERLY CAPPED AND BURIED IF NOT REMOVED AND RELOCATED. SEPARATE PERMITS ARE REQUIRED FOR ABANDONED SANITARY SEWER LATERALS. PLEASE NOTE THAT EXISTING STORES TO REMAIN WILL BE ACTIVE DURING CONSTRUCTION AND MUST HAVE WORKING UTILITY SERVICE.
- CONTRACTOR SHALL FOLLOW ALL REQUIREMENTS OF THE EROSION AND SEDIMENT CONTROL PLAN, AS APPLICABLE, INCLUDING THE STABILIZED CONSTRUCTION ENTRANCE, SILT FENCE, SILT SOXX, TREE PROTECTION, AND IMMEDIATE STABILIZATION OF ANY DISTURBED AREAS.
- DURING DEMOLITION OF THE EXISTING STRUCTURES THE APPLICANT SHALL ENSURE THAT THE CONTRACTOR TAKES APPROPRIATE MEASURES TO MINIMIZE DUST AT ALL TIMES DURING ACTIVE DEMOLITION. A MINIMUM OF 2 HOSES AND/OR ONE WATER TRUCK SHALL BE PROVIDED ON SITE TO CONTINUALLY SPRAY THE BUILDINGS DURING DEMOLITION. ADDITIONAL HOSES, WATER TRUCKS OR OTHER METHODS SHALL BE USED IF THE REQUIRED MEASURES ARE NOT ADEQUATE.
- UTILITIES TO BE REMOVED/CAPPED, FULL REMOVAL OF UTILITIES BACK TO NEAREST STRUCTURE, ARE BUT NOT LIMITED TO WATER, SEWER AND GAS LATERALS.
- THE APPLICANT SHALL CONDUCT REGULAR STREET CLEANING OF ALL ROADWAYS ADJACENT TO ACTIVE PORTIONS OF THE CONSTRUCTION SITE. STAFF SHALL HAVE THE RIGHT TO ORDER STREET CLEANING MORE OFTEN IF THERE IS EVIDENCE OF CONSTRUCTION RELATED DEBRIS IN THE ROADWAY DURING THE PROJECT.

PLAN LEGEND

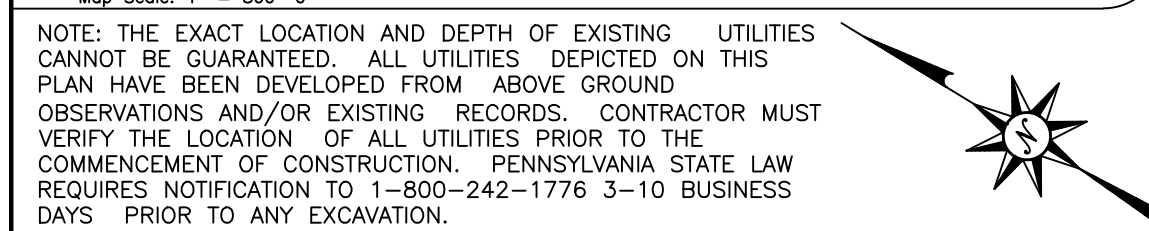
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- T --- UNDERGROUND TELEPHONE
- --- EDGE OF PAVEMENT
- X --- FENCE LINE

- EXISTING BUILDINGS TO BE REMOVED
- EXISTING ASPHALT TO BE REMOVED
- EXISTING WALKWAY TO BE REMOVED
- EXISTING WALL TO BE REMOVED
- TREE TO BE REMOVED

NUM.	DATE	REVISION
PLAN PREPARED BY:		
SITE ENGINEERING CONCEPTS, LLC		
P.O. BOX 1992		
SOUTHEASTERN, PA 19399		
P: 610-240-0450 F: 610-240-0451 E: INFO@SITE-ENGINEERS.COM		
PLAN PREPARED FOR:		
HAMILTON ESTATE		
208 & 228 STRAFFORD AVE AND 18 FORREST LANE		
WAYNE, PA 19087		
RADNOR TOWNSHIP	DELAWARE COUNTY	PENNSYLVANIA
DATE: AUGUST 27, 2020		
DEMOLITION PLAN		
SHEET		
3 of 15		
SCALE: 1" = 40'		



DAVID J. SANDERS, P.E.
PE057436



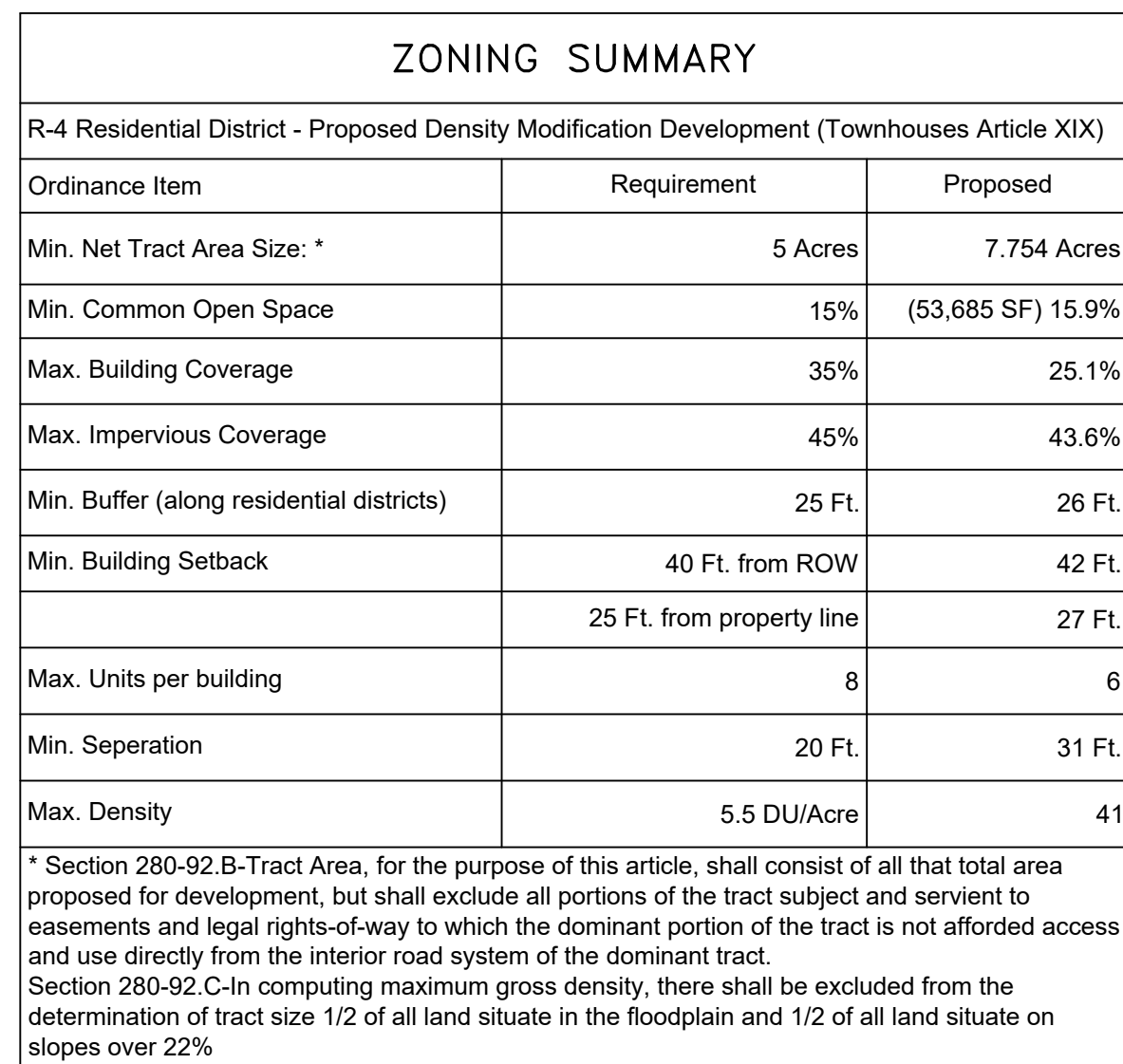
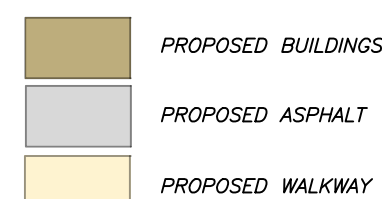
ZONING SUMMARY				
R-4 RESIDENTIAL DISTRICT - PROPOSED DENSITY MODIFICATION (DEVELOPMENT) (TOWNHOUSES)				
ORDINANCE ITEM	REQUIREMENT	EXISTING	PROPOSED	
MIN NET TRACT AREA SIZE	-	N/A	7,754 ACRES	
MIN RESIDENTIAL BUFFER	-	N/A	26 FT	
MIN. SETBACKS				
FROM ROW	40 FT	N/A	46 FT	
FROM PROPERTY LINE	25 FT	N/A	42 FT	
MAX UNITS PER BUILDING	8		6	
MIN SIDE TO SIDE DISTANCE	20 FT		21 FT	
MAX DENSITY	5.5 DU/ACRE	N/A	41	
MAX. BUILDING COVERAGE	35%	7.6%	25.1%	
MAX. IMPERVIOUS COVERAGE	45%	23.9%	43.6%	
MIN. OPEN SPACE	15%	15%	15%	

IMPERVIOUS SUMMARY (SQ. FT.)				
NET LOT AREA	EXISTING	TO BE REMOVED	TO BE ADDED	PROPOSED
BUILDINGS	25,600	25,600	84,870	84,870
ASPHALT	27,676	27,676	47,427	47,427
WALKS/CONC.	9,634	9,634	14,845	14,845
OTHER IMPERVIOUS	17,877	17,877	0	0
TOTAL BUILDING COVERAGE	25,600	25,600	84,870	84,870
TOTAL BUILDING PERCENTAGE	7.6%			25.1%
TOTAL IMPERVIOUS COVERAGE	80,787	80,787	147,142	147,142
TOTAL IMPERVIOUS PERCENTAGE	23.9%			43.6%
IMPERVIOUS REMAINING FOR FUTURE DEVELOPMENT				4,652

BOLD LINES: PROPOSED
FADED LINES: EXISTING

— — — — — SUPPLEMENTAL CONTOUR (1' INTERVAL)
————— INDEX CONTOUR (5' INTERVAL)

— ST — STORM SEWER PIPING
— S — SANITARY SEWER PIPING
— G — GAS MAIN
— W — WATER MAIN / SERVICE
— OHW — OVERHEAD WIRE
— T — UNDERGROUND TELEPHONE
— — — — — EDGE OF PAVEMENT
— X — FENCE LINE



- ## GENERAL NOTES
1. THE TOWNHOUSE DEVELOPMENT SHALL BE CONSTRUCTED OWNED AND OPERATED UNDER SINGLE OWNERSHIP, OR UNDER THE PENNSYLVANIA UNIT PROPERTY ACT, OR SIMILAR STATE SUCH AS THE PLANNED COMMUNITY ACT.
 2. THE DEVELOPMENT WILL BE SERVED BY SANITARY SEWERS AND PUBLIC WATER SUPPLY.
 3. EACH TOWNHOUSE SHALL HAVE AT LEAST TWO FLOORS IN COMPLIANCE WITH APPLICABLE HEIGHT LIMITS AND SHALL BE OCCUPIED AS A SINGLE-FAMILY DWELLING.
 4. NO MORE THAN TWO CONTIGUOUS TOWNHOUSES IN ANY GROUP MAYBE CONSTRUCTED IN LINE AND EACH TOWNHOUSE SHALL HAVE AT LEAST ONE PLAN ELEMENT ON ANY FLOOR WHICH PROJECTS BEYOND HEIGHTS WITHIN THE WALL PLANE OF THE FACADE A MINIMUM DIMENSION OF TWO FEET.
 5. THE DEVELOPMENT WILL BE SERVICED BY PECO ELECTRIC.

PROPERTY OWNER

COMMONWEALTH OF PENNSYLVANIA

COUNTY OF DELAWARE

ON THE _____ DAY OF _____ 20____, BEFORE ME, THE
UNDERSIGNED NOTARY PUBLIC OF THE COMMONWEALTH OF PENNSYLVANIA, THE
UNDERSIGNED OFFICER PERSONALLY APPEARED, _____ OF
_____, WHO ACKNOWLEDGED HIM/HERSELF TO BE THE _____ OF
_____, OWNER OF THE PROPERTIES PROPOSED TO BE
DEVELOPED IN THE SUBJECT PLAN, BEING AUTHORIZED TO DO SO. ACKNOWLEDGED
THIS PLAN TO BE THE OFFICIAL PLAN OF THE PROPERTIES SHOWN THEREON,
SITUATED IN THE TOWNSHIP OF RADNOR, DELAWARE COUNTY, PENNSYLVANIA, AND
DESCRIBED THAT THIS PLAN IS MADE WITH THE OWNER'S FREE CONSENT AND
INTENDED TO BE RECORDED ACCORDING TO LAW.

WITNESS MY HAND AND NOTARIAL SEAL THE DAY AND YEAR AFORESAID.

NOTARY _____

OWNER _____

RADNOR TOWNSHIP:

THIS IS TO CERTIFY THE BOARD OF SUPERVISORS HAS APPROVED THIS LAND DEVELOPMENT PLAN FOR STRAFFORD AVENUE AND EAGLE ROAD, ON THE ____ DAY OF _____, 20____, FOR RECORDING AT THE RECORDER OF DEEDS OF DELAWARE COUNTY, MEDIA, PENNSYLVANIA. OFFERS OF DEDICATION HAVE BEEN ACCEPTED, DIRECTLY TO BONDS, FUNDS OR SECURITIES IN ESCROW HAVE BEEN FILED WITH THE TOWNSHIP AND ACCEPTED TO COVER ALL IMPROVEMENTS SHOWN ON THE PLAN AND REQUIRED UNDER THE SUBDIVISION/LAND DEVELOPMENT AGREEMENT.

APPROVED THE ____ DAY OF _____, 20____

BOARD OF SUPERVISORS SIGNED THIS ____ DAY OF _____, 20____.

CHAIRMAN

MEMBER

TOWNSHIP ENGINEER

ATTEST: _____
MANAGER

DELAWARE COUNTY PLANNING COMMISSION:

REVIEWED BY THE DELAWARE COUNTY PLANNING COMMISSION THIS __ DAY OF _____, 20__.

SECRETARY

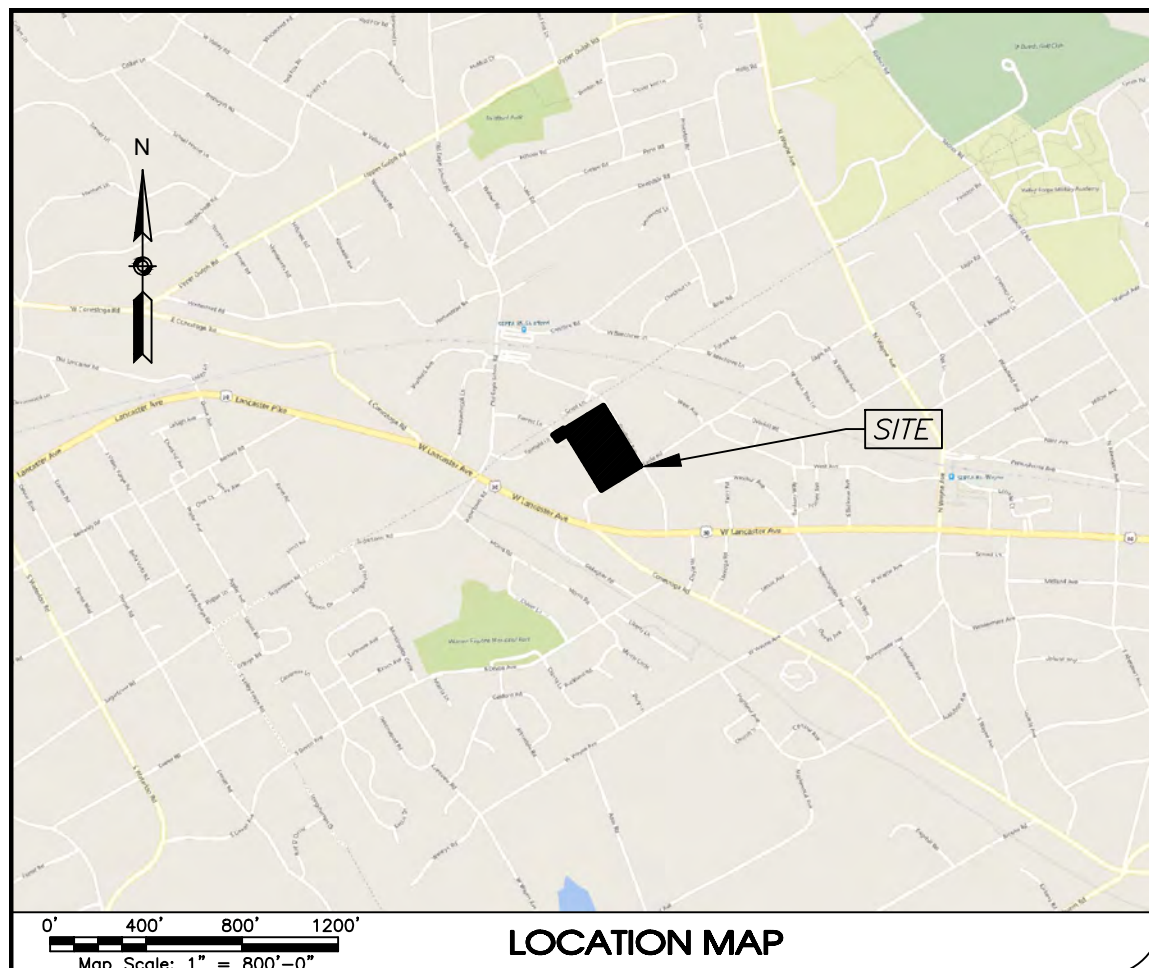
RECORDED OF DEEDS

RECORDED IN THE OFFICE OF THE RECORDER OF DEEDS OF DELAWARE COUNTY AT
MEDIA CHESTER, PENNSYLVANIA IN THE

PLAN BOOK _____ PAGE _____ ON THE ____ DAY OF _____,
20_____

RECORDED OF DEEDS

NUM.	DATE	REVISION
PLAN PREPARED BY:		
<i>SITE ENGINEERING CONCEPTS, LLC</i>		
P.O. BOX 1992		
SOUTHEASTERN, PA 19399		
P: 610-240-0450	F: 610-240-0451	E: INFO@SITE-ENGINEERS.COM
PLAN PREPARED FOR:		
<i>HAMILTON ESTATE</i>		
208 & 228 STRAFFORD AVE AND 18 FORREST LANE WAYNE, PA 19087		
RADNOR TOWNSHIP	DELAWARE COUNTY	PENNSYLVANIA
		DATE: AUGUST 27, 2020
RECORD PLAN		SHEET 4 of 15 SCALE: 1" = 40'



NOTE: THE EXACT LOCATION AND DEPTH OF EXISTING UTILITIES CANNOT BE GUARANTEED. ALL UTILITIES DEPICTED ON THIS PLAN HAVE BEEN DEVELOPED FROM ABOVE GROUND OBSERVATIONS AND/OR EXISTING RECORDS. CONTRACTOR MUST VERIFY THE LOCATION OF ALL UTILITIES PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. PENNSYLVANIA STATE LAW REQUIRES NOTIFICATION TO 1-800-242-1776 3-10 BUSINESS DAYS PRIOR TO ANY EXCAVATION.

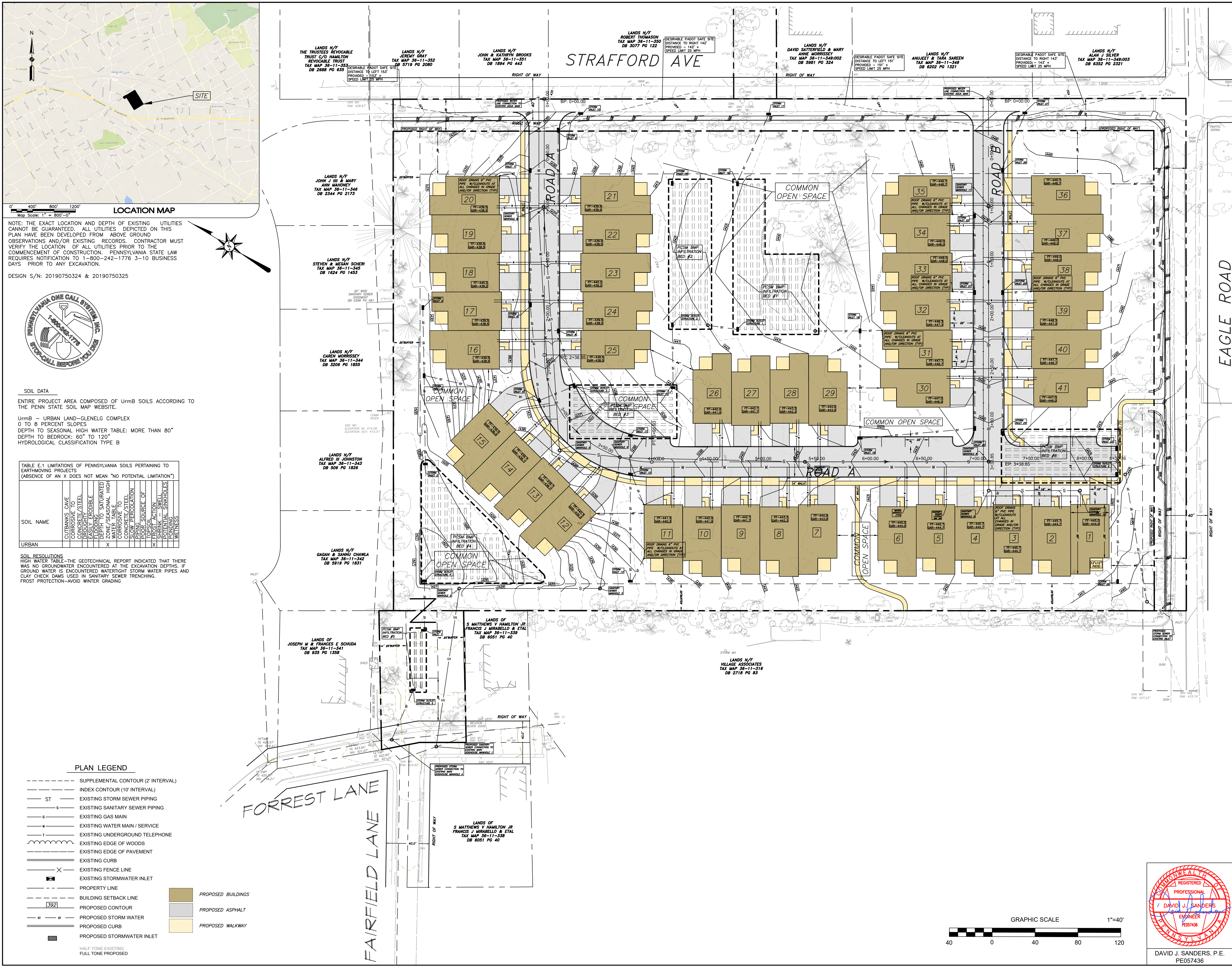
DESIGN S/N: 20190750324 & 20190750325



SOIL DATA
ENTIRE PROJECT AREA COMPOSED OF UrmB SOILS ACCORDING TO THE PENN STATE SOIL MAP WEBSITE.
UrmB - URBAN LAND-GLENELG COMPLEX
0 TO 8 PERCENT SLOPES
DEPTH TO SEASONAL HIGH WATER TABLE: MORE THAN 80"
DEPTH TO BEDROCK: 60" TO 120"
HYDROLOGICAL CLASSIFICATION TYPE B

TABLE E.1 LIMITATIONS OF PENNSYLVANIA SOILS PERTAINING TO EARTHMOVING PROJECTS (ABSENCE OF AN X DOES NOT MEAN "NO POTENTIAL LIMITATION")											
SOIL NAME	CUTBANKS CAVE	CONCRETE TO	CONCRETE/STEEL	DEPTH TO SATURATED	ZONE/SEASONAL HIGH	CONCRETE/STEEL	SLOW PERCOLATION	POOR SOURCE OF	TOPSOIL	POTENTIAL	POTENTIAL
URBAN											

SOIL RESOLUTIONS
HIGH WATER TABLE-THE GEOTECHNICAL REPORT INDICATED THAT THERE WAS NO GROUNDWATER ENCOUNTERED AT THE EXCAVATION DEPTHS. IF GROUND WATER IS ENCOUNTERED WATER TIGHT STORM WATER PIPES AND CLAY CHECK DAMS USED IN SANITARY SEWER TRENCHING.
FROST PROTECTION-AVOID WINTER GRADING

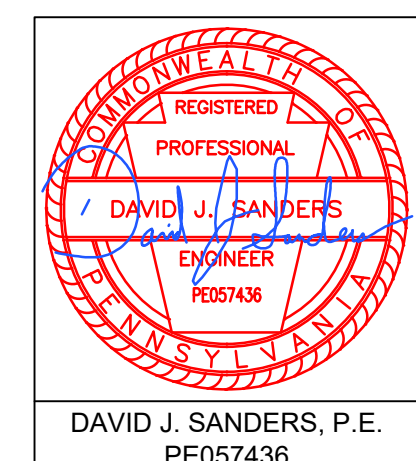
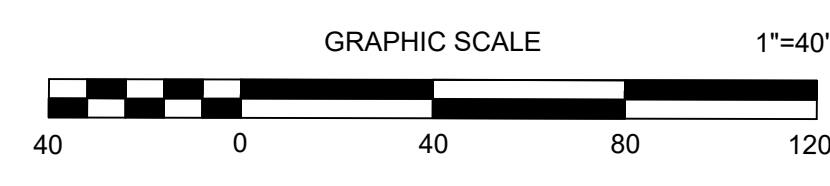


- PROJECT NOTES**
1. SHOULD ANY TREES NOT SCHEDULED OR PERMITTED TO BE REMOVED BE IRREPARABLY DAMAGED DURING CONSTRUCTION AND DIE WITHIN EIGHTEEN (18) MONTHS OF THE CONCLUSION OF CONSTRUCTION ACTIVITIES, THOSE TREES WILL BE REQUIRED TO BE REPLACED.
 2. GRADING AND EARTH-MOVING OPERATIONS SHALL BE MINIMIZED DURING THE PERIOD NOVEMBER 15 TO APRIL 1 WHEN RE-VEGETATION OF EXPOSED GROUND SURFACE IS DIFFICULT. MULCH, STRAW, STONE AND/OR SOD SHALL BE USED TO STABILIZE ALL AREAS DENUDE DURING THIS TIME PERIOD.
 3. ROOF DRAIN COLLECTION PIPING SHALL BE 6" PVC @ 0.02 FT/FT MIN. CLEANOUTS SHALL BE PROVIDED AT ALL CHANGES IN GRADE AND/OR DIRECTION.
 4. THE MAXIMUM TIME OF EXPOSURE FOR BARE SOIL AREAS SHALL BE TWENTY (20) DAYS BEFORE STABILIZATION MEASURES ARE IMPLEMENTED.
 5. ROUTINE END-OF-DAY CHECKS AND FOLLOWING STORMS SHALL BE REQUIRED DURING THE CONSTRUCTION TO ENSURE THE MEASURES ARE WORKING PROPERLY.
 6. NEWLY GRADED SLOPES OF OVER 25% MUST BE STABILIZED WITH SOD OR JUTE NETTING AND SEED.
 7. UTILITY LINES PER ONE CALL INFORMATION HAVE BEEN SHOWN. CONTRACTOR MUST VERIFY PRIOR TO COMMENCEMENT OF CONSTRUCTION.
 8. GAS, ELECTRIC, WATER OR ANY OTHER UTILITY TO BE ABANDONED SHALL BE PROPERLY SEALED/REMOVED.
 9. HIGH DURABILITY PAVEMENT MARKING MATERIAL SHALL BE USED FOR ALL PAVEMENT STRIPPING.
 10. THE RESPONSIBILITY FOR THE CONTINUED MAINTENANCE AND OPERATION OF THE SEEPAGE BED AND STORM PIPING SHALL BE THE OBLIGATION OF THE HOMEOWNERS ASSOCIATION.

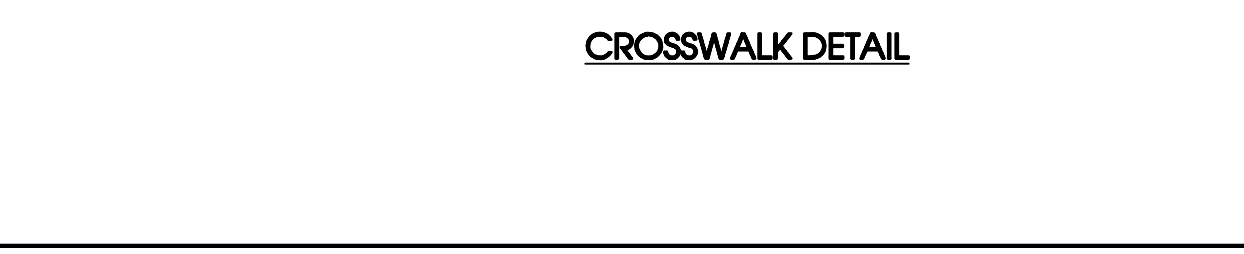
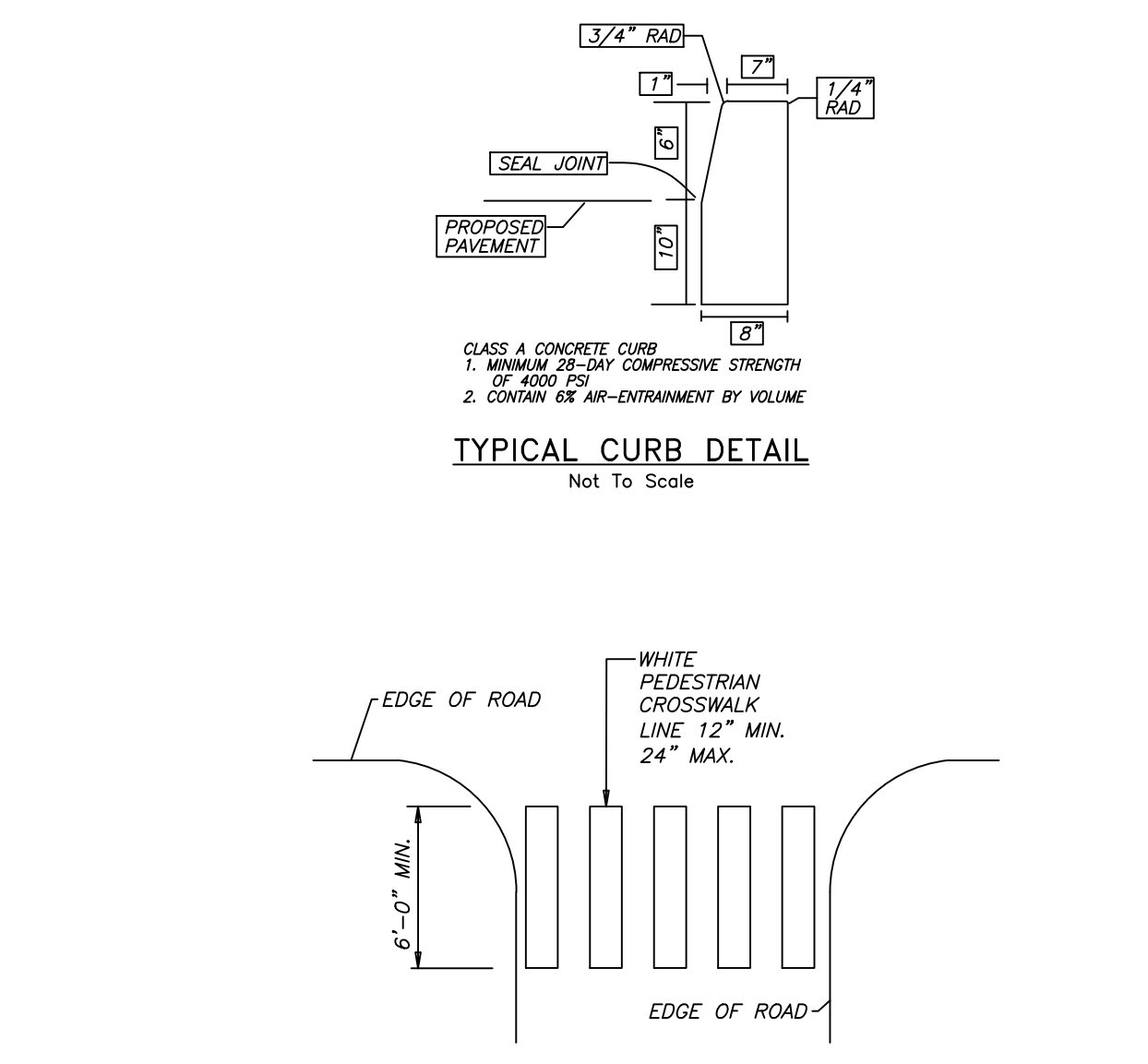
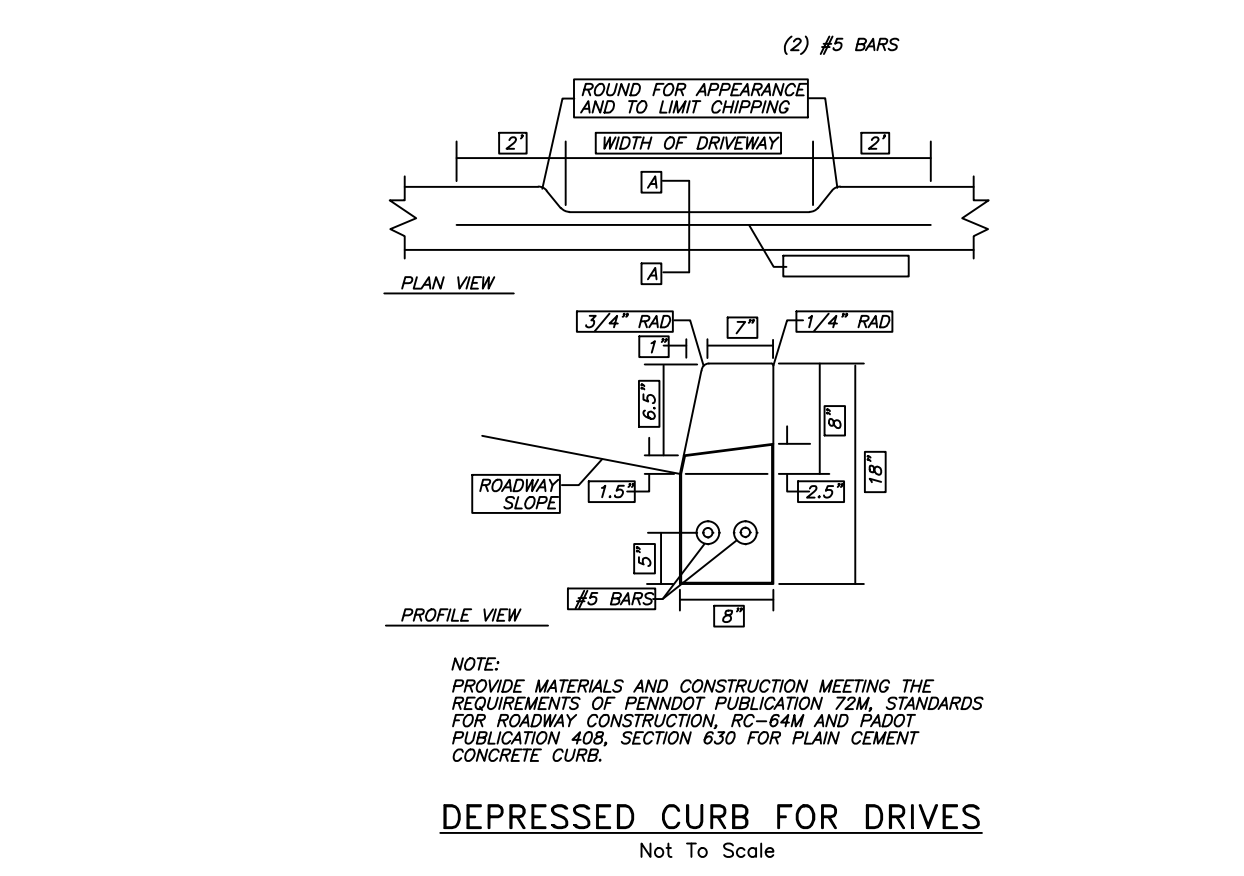
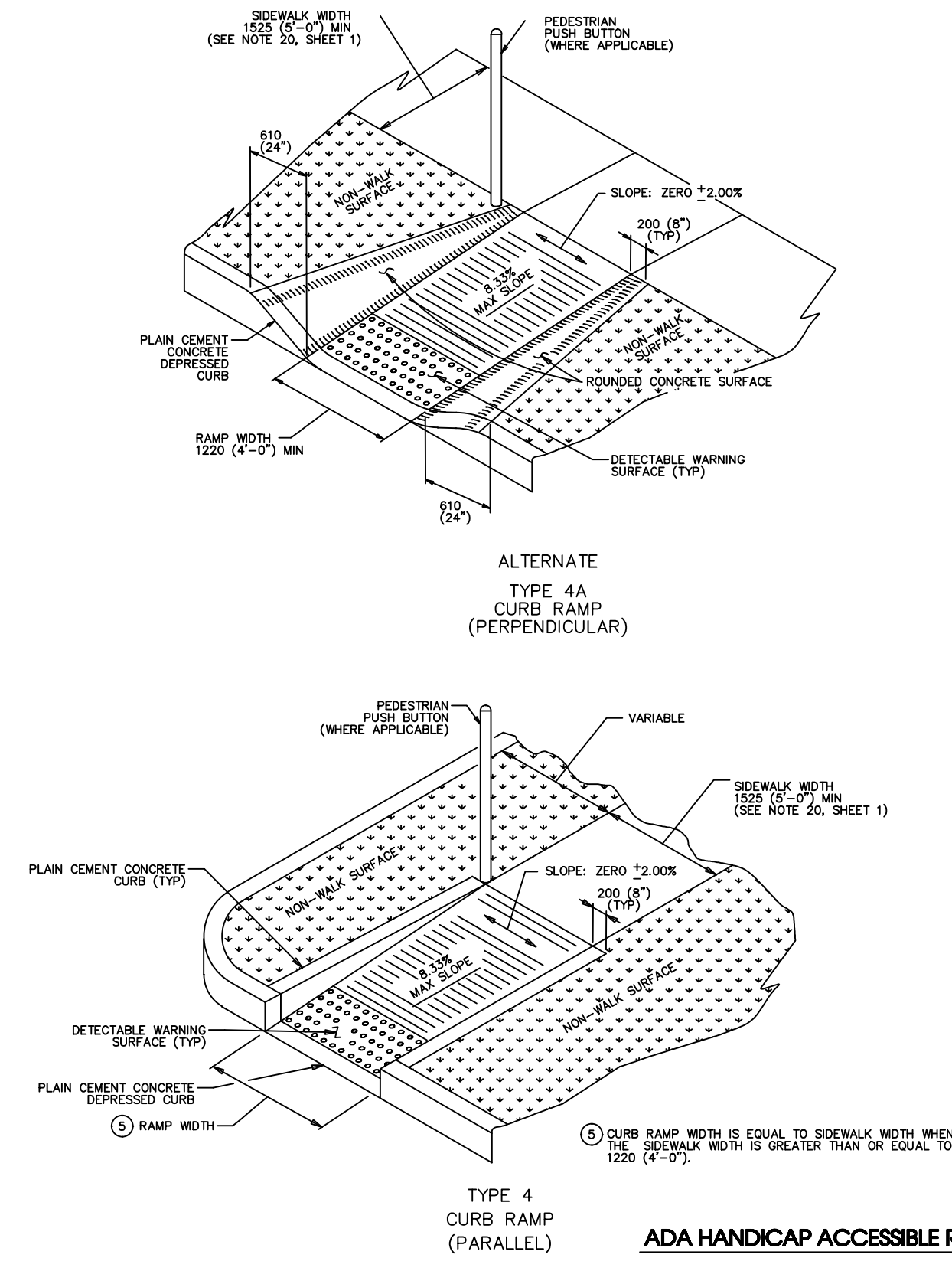
PLAN LEGEND

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- INDEX CONTOUR (10' INTERVAL)
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- S --- EXISTING SANITARY SEWER PIPING
- G --- EXISTING GAS MAIN
- W --- EXISTING WATER MAIN / SERVICE
- T --- EXISTING UNDERGROUND TELEPHONE
- E --- EXISTING EDGE OF WOODS
- P --- EXISTING EDGE OF PAVEMENT
- C --- EXISTING CURB
- X --- EXISTING FENCE LINE
- I --- EXISTING STORMWATER INLET
- P --- PROPERTY LINE
- B --- BUILDING SETBACK LINE
- [32] --- PROPOSED CONTOUR
- W --- PROPOSED STORM WATER
- C --- PROPOSED CURB
- I --- PROPOSED STORMWATER INLET

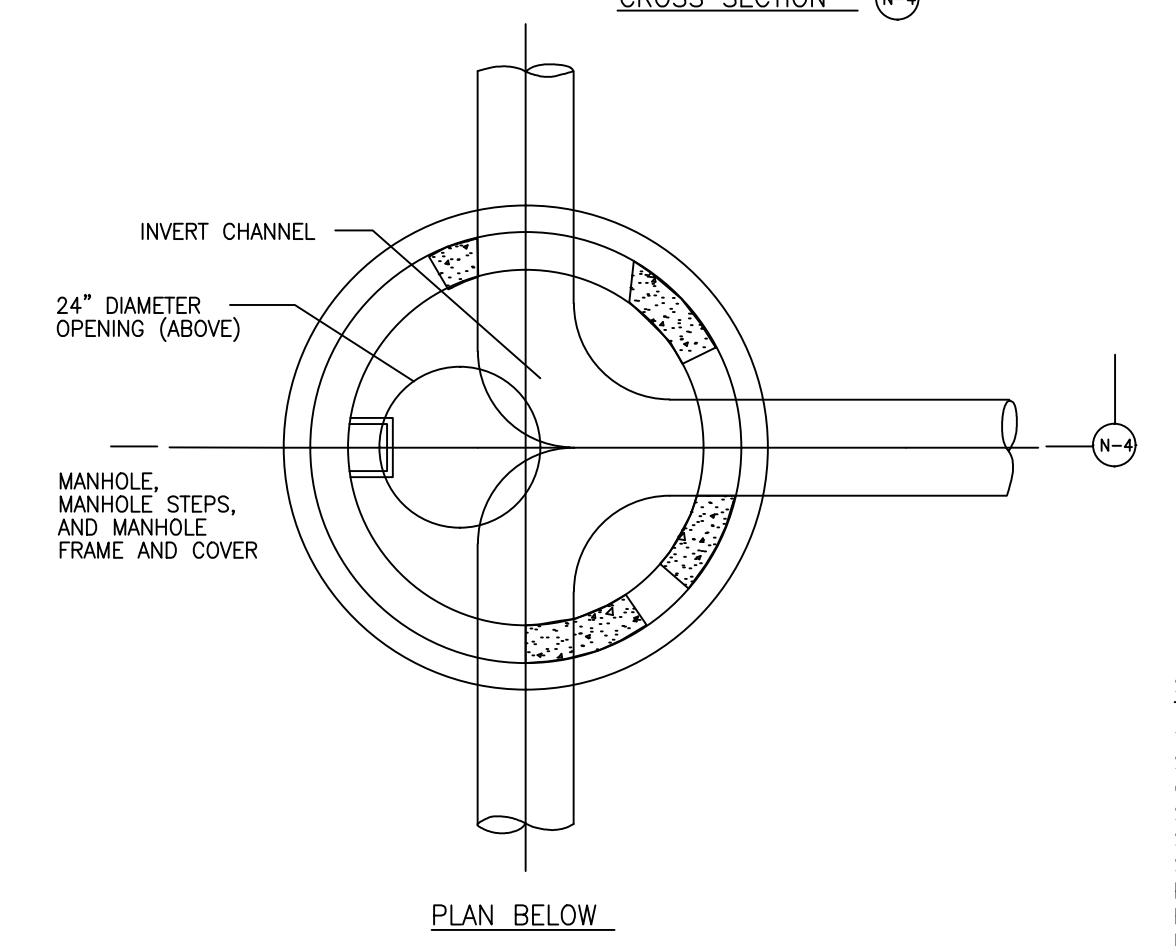
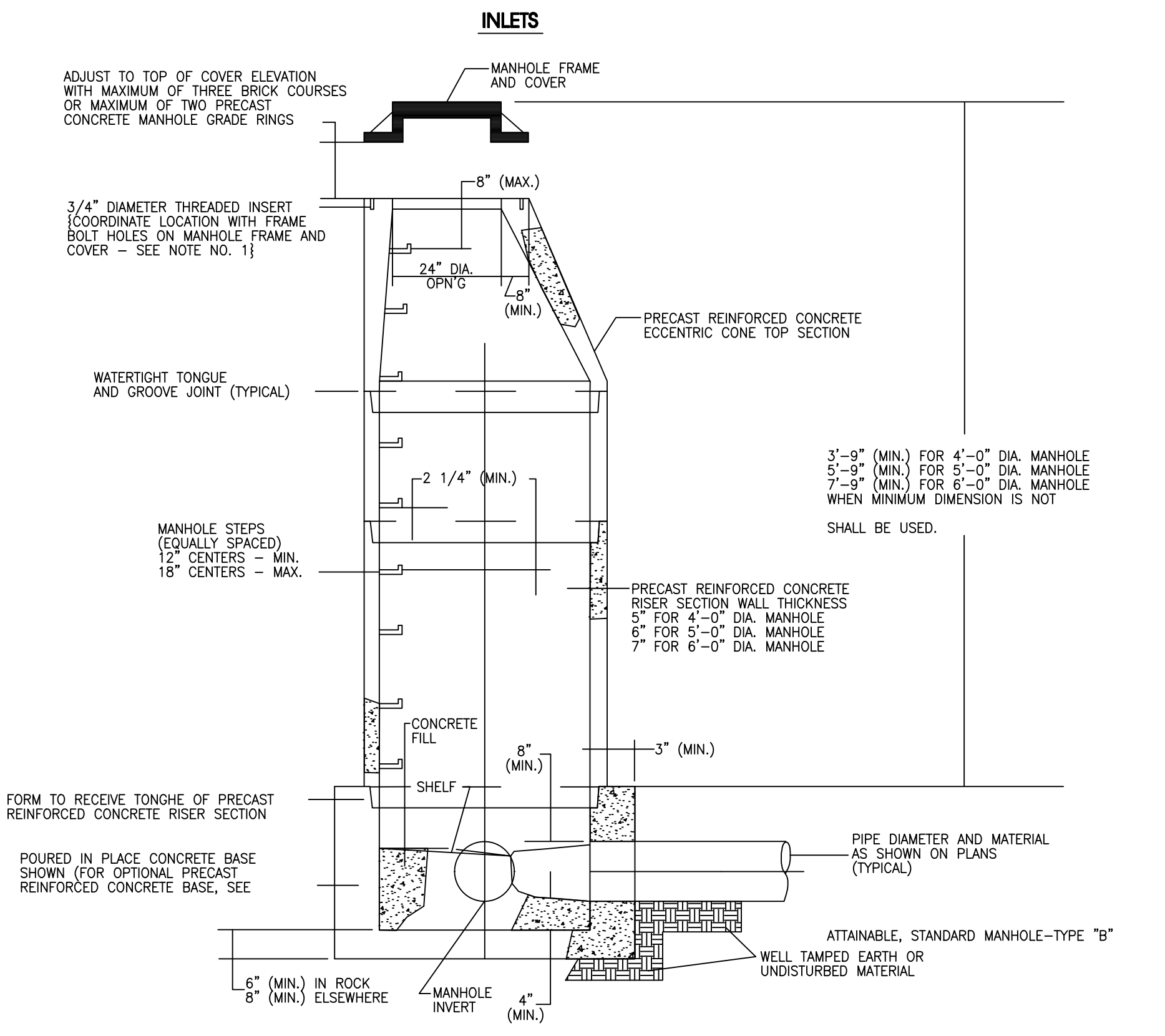
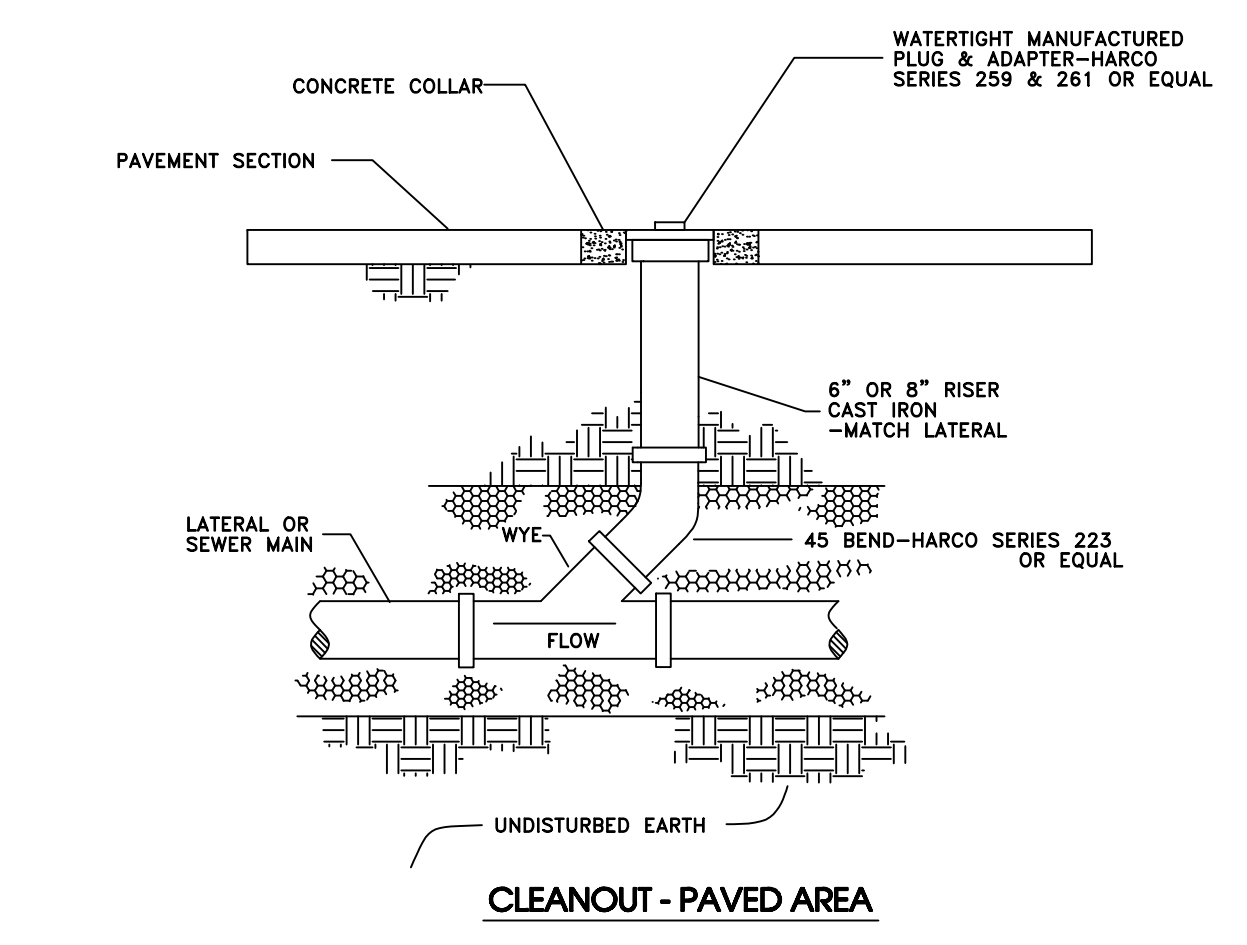
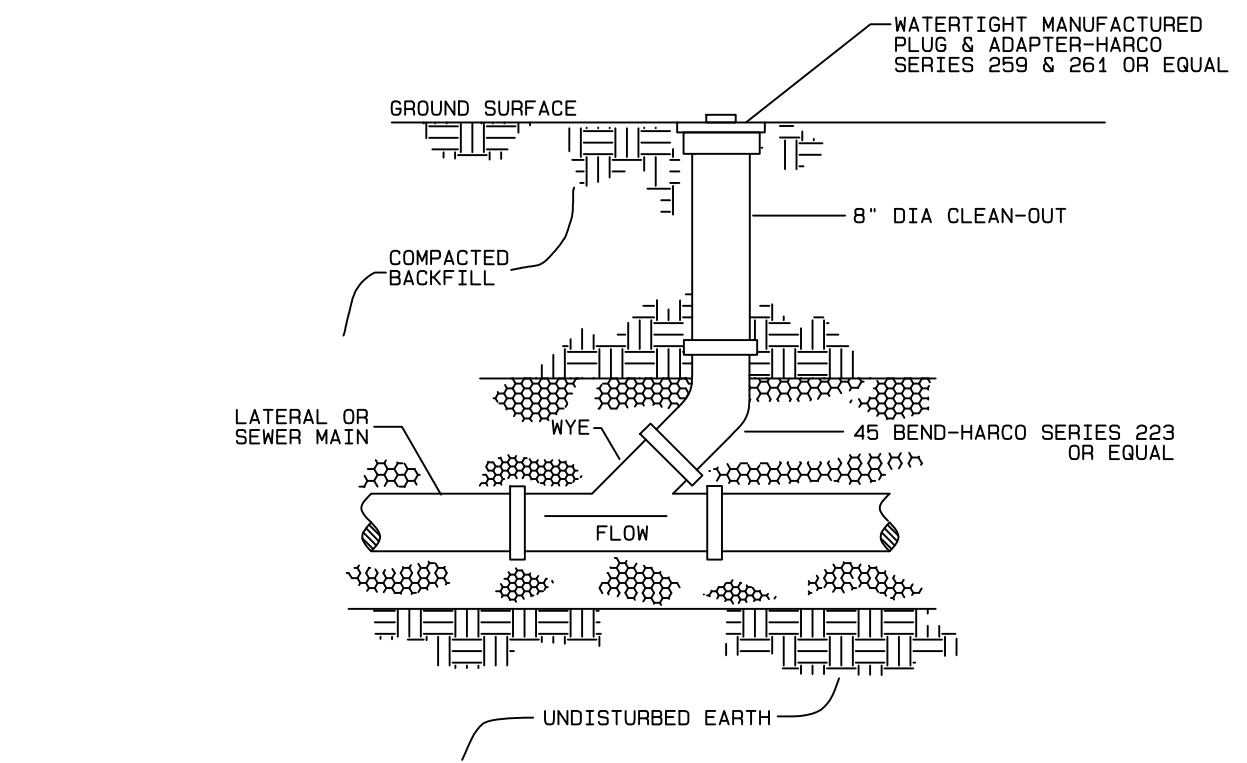
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- PROPOSED ASPHALT
- PROPOSED WALKWAY



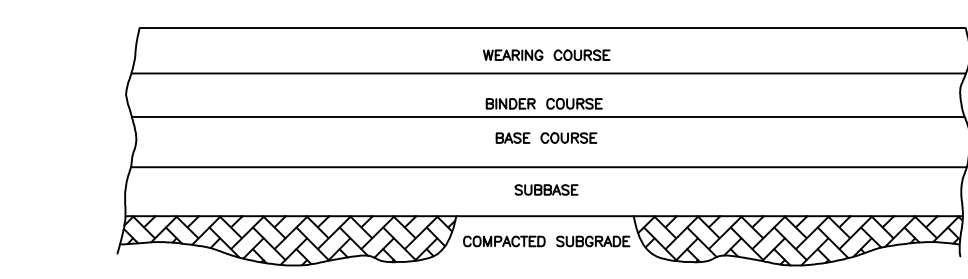
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PLAN PREPARED BY: SITE ENGINEERING CONCEPTS, LLC P.O. BOX 1992 SOUTHEASTERN, PA 19399 P: 610-240-0450 F: 610-240-0451 E: INFO@SITE-ENGINEERS.COM		
PLAN PREPARED FOR: HAMILTON ESTATE 208 & 228 STRAFFORD AVE AND 18 FORREST LANE WAYNE, PA 19087 RADNOR TOWNSHIP DELAWARE COUNTY PENNSYLVANIA DATE: AUGUST 27, 2020		
POST CONSTRUCTION STORMWATER PLAN		SHEET 5 of 15 SCALE: 1" = 40'



- NOTES
1. PROVIDE MATERIALS AND CONSTRUCTION MEETING THE REQUIREMENTS OF PUBLICATION 408, SECTIONS 306, 408, 520, 676 AND 694.
 2. PROVIDE EXPANSION JOINT MATERIAL 13 (1/2)" THICK WHERE CURB RAMP ADJOINS ANY RIGID PAVEMENT, SIDEWALK OR STRUCTURE WITH THE TOP OF
 3. CONSTRUCT DIAGONAL CURB RAMPS WITH A 1220 (4'-0") CLEARSPACE OUTSIDE OF TRAVEL LINES AT THE BOTTOM OF THE RAMP. IF DIAGONAL CURB RAMPS ARE PROVIDED AT MARKED CROSSINGS, THE 1220 (4'-0") CLEARSPACE IS LOCATED WITHIN THE MARKINGS AND OUTSIDE OF THE TRAVEL LANES.
 5. PROVIDE SLIP RESISTANT TEXTURE ON CURB RAMP BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP. EXTEND TEXTURE THE FULL WIDTH AND LENGTH OF THE CURB RAMP INCLUDING FLARED SIDE RAMPS.
 6. MODIFY CONSTRUCTION DETAILS TO ADAPT DIMENSIONS TO EXISTING CURB HEIGHTS WHERE THE CURB IS LESS THAN THE STANDARD
 7. CURB RAMP AND SIDE FLARE LENGTHS ARE VARIABLE AND BASED ON
 8. IT MAY BE NECESSARY TO LIMIT THE RUN OF A PARALLEL OR PERPENDICULAR CURB RAMP IN ORDER TO AVOID CHASING GRADE INDEFINITELY WHEN TRAVELING THE HEIGHT OF CURB. RAMP LENGTH NOT TO EXCEED 4000 (15'-0"). ADJUST RAMP SLOPE AS NEEDED TO PROVIDE ACCESS TO THE MAXIMUM EXTENT POSSIBLE.
 - 9.
 10. THE DETAILS DEPICT PEDESTRIAN PUSH BUTTON POLES TO ILLUSTRATE THE PREFERRED PLACEMENT OF PEDESTRIAN PUSH BUTTONS. PEDESTRIAN PUSH BUTTONS ARE TO BE INSTALLED WHERE APPLICABLE.
 11. CONSTRUCT BUILT-UP CURB RAMP OF BITUMINOUS MATERIAL AS INDICATED.
 12. ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE NOTED. US CUSTOMARY UNITS IN () PARENTHESES.
 13. ALIGN DETECTABLE WARNING DOMES ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF THE RAMP AND PERPENDICULAR TO CURB.
 14. PROVIDE DETECTABLE WARNING SURFACES (DWS) 610 (24") MINIMUM (IN THE DIRECTION OF PEDESTRIAN TRAVEL) ACROSS FULL WIDTH OF RAMP AT THE GRADE BREAK NEAR STREET EDGE. PROVIDE DWS THAT CONTRAST VISUALLY WITH ADJACENT WALKWAY SURFACES. OTHER LIGHT-ON-DARK OR DARK-ON-LIGHT FOR THE FULL WIDTH OF RAMP.
 15. FOR NEW CONSTRUCTION, DO NOT EXCEED 2.00% CROSS SLOPE ON THE CURB RAMP OR ACCESSIBLE ROUTE.
 16. FOR NEW CONSTRUCTION AND ALTERATIONS, CONSTRUCT CURB RAMP AND FLARE SLOPES WITH THE FLATTEST SLOPE POSSIBLE. THE SLOPES INDICATED IN THE DETAILS SHOW THE MAX ALLOWABLE SLOPES THAT EXCEED THOSE INDICATED IN THE DETAILS OR CONTRACT DOCUMENTS AS APPLICABLE. SLOPES THAT EXCEED THOSE INDICATED IN THE DETAILS OR CONTRACT DOCUMENTS AS APPLICABLE WILL NOT BE ADJUSTED AND WILL BE RECONSTRUCTED.
 17. THE IMMEDIATE ADJOINING SURFACE AT THE BOTTOM AND TOP OF CURB RAMPS IS NOT TO EXCEED ROADWAY PROFILE SLOPE WHEN LOCATED ADJACENT TO THE ROADWAY. IN AREAS WHERE THE ADJOINING SURFACE IS NOT ADJACENT TO THE ROADWAY, THE UNIFORM SLOPE IS NOT TO EXCEED 1:12. FOR ALL LOCATIONS DO NOT EXCEED 2.00% CROSS SLOPE. FOR LOCATIONS THAT REQUIRE A TURNING MANUEVER, THE MAXIMUM SLOPE IS 2.00% IN ALL DIRECTIONS. THE GRADE IN GRASS AT THE BOTTOM OF THE CURB RAMP AND ADJOINING ROAD SURFACE IS NOT TO EXCEED AN ALGEBRAIC DIFFERENCE OF 1.00%. SEE SHEET 8 FOR DETAILS.
 18. THE CONSTRUCTION STANDARDS DEPICTED ARE MOST APPROPRIATE FOR NEW CONSTRUCTION. ALL CONSTRUCTION MUST MEET THE STANDARDS CONTAINED HEREIN UNLESS OTHERWISE NOTED OR DIRECTED.
 19. ALL SLOPES ARE MEASURED WITH RESPECT TO A LEVEL PLANE. THEREFORE, THE LENGTH OF RAMP IS NOT SOLELY DEPENDANT ON THE HEIGHT OF CURB. (FOR EXAMPLE, A 150 (6") CURB DOES NOT NECESSARILY MEAN A RAMP LENGTH OF 1400 (6'-0") FOR A 1:12 (1:12) SLOPE.
 20. SIDEWALK WIDTH MAY BE REDUCED TO 1220 (4'-0") WHEN PASSING AREAS 1525 X 1525 (10'-0" X 10'-0") ARE PROVIDED EVERY 10 METERS (32.8').
 21. THE TRAVEL LANE IS DEFINED BY THE OUTSIDE EDGE OF THE WHITE PAVEMENT MARKING LINE. IF A WHITE PAVEMENT MARKING LINE DOES NOT EXIST, THE TRAVEL LANE IS DEFINED BY THE CONTRACT DOCUMENTS.
 22. DEPRESSED CURB FOR CURB RAMPS MUST BE FLUSH TO ADJACENT ROADWAY. EDGE OF ROAD ELEVATIONS AT THE FLUM LINE SHALL BE GRADED TO ENSURE POSITIVE DRAINAGE AND PREVENT PONDING.



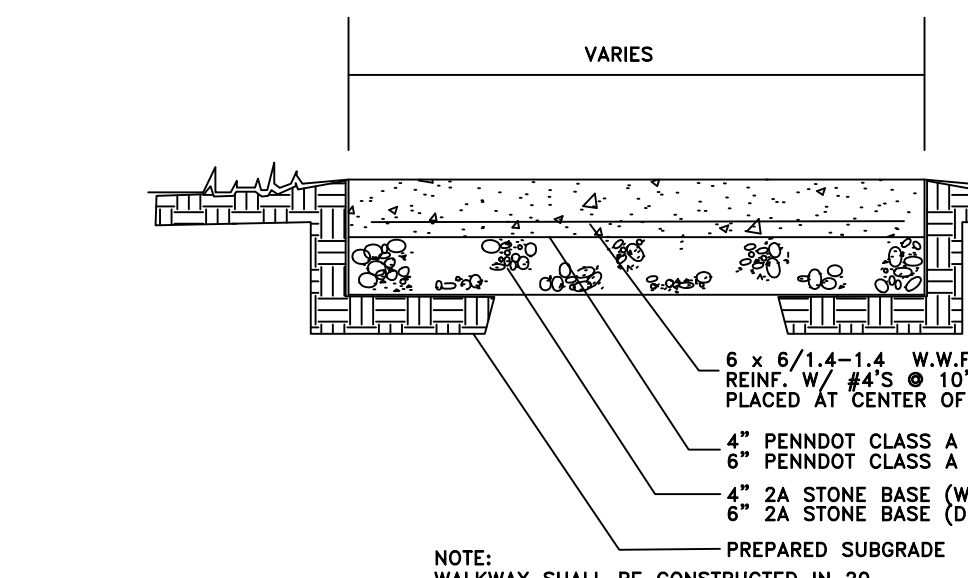
- STANDARD MANHOLE - TYPE "A" NOTES
1. MANHOLE FRAME TO BE BOLTED IN PLACE WITH TWO 3/4" DIA. BOLTS AND FLAT WASHERS.
 2. FOR PIPE CONNECTION TO CONCRETE BASE, SEE MANHOLE SECTION OF THE PROJECT MANUAL.
 3. ORIENTATION OF PIPES SHOWN FOR ILLUSTRATION PURPOSES ONLY. FOR ACTUAL LOCATIONS, SEE PLANS.
 4. THE SHELVE SHALL SLOPE TOWARD THE INVERT CHANNEL AT THE RATE OF 1" PER FOOT (MINIMUM).
 5. THE DEPTH OF THE INVERT CHANNEL SHALL BE NOT LESS THAN 3/4 OF THE DIAMETER OF THE PIPE.
 6. FOR MANHOLES HAVING 5'-6" OR 5'-8" DIAMETER BASE, REDUCTION IN DIAMETER TO 4'-0" SHALL START AT THE FIRST JOINT ABOVE THE UPPERMOST PIPE CONNECTION TO WALL (WHERE DEPTH IS SUFFICIENT).
 7. FOR LENGTH OF PIPE CONNECTIONS TO MANHOLES, SEE MANHOLE SECTION OF THE PROJECT MANUAL.
 8. FINISHED GRADE SHALL BE FLUSH WITH TOP OF COVER ON MANHOLE FRAME AND COVER, UNLESS OTHERWISE NOTED.
- SANITARY SEWER NOTES
1. CONNECTIONS TO EXISTING MANHOLES SHOULD BE MADE BY CORING THE REQUIRED OPENING SO AS TO PREVENT CRACKING AND SPALLING CONCRETE. MAKE OPENINGS OF SUFFICIENT SIZE TO ACCOMMODATE A LINK-SEAL SEGMENTED MECHANICAL SEAL AND THE PROPOSED PIPE. A NEW CHANNEL SHALL BE FORMED IN THE MANHOLE TO ACCOMMODATE THE NEW PIPE.



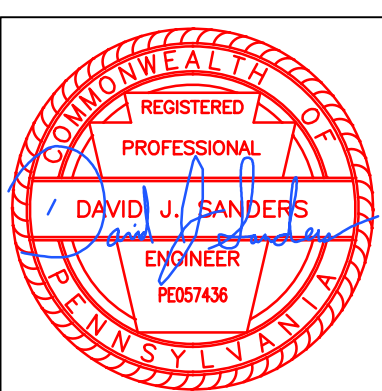
REQUIRED BITUMINOUS PAVEMENT SECTION		
N.T.S.		
DEPTH	LOCATION	INDIVIDUAL DRIVEWAYS
WEARING:	2"	2"
BINDER:	2" (10-2 OR FB-1)	NONE
SUBBASE:	8" PA 4A 2" SCREENINGS -VIBRATORY COMPACTED TO FILL VOIDS	8"

BITUMINOUS PAVEMENT SPECIFICATIONS, ALL SECTIONS, APPLICABLE	
WEARING COURSE	SUPERPAVE, ASPHALT MIXTURE DESIGN, HMA WEARING COURSE, PG 64-22, 3 TO <10 MILLION ESALS, 9.5 MM MIX, SRL-H
BINDER COURSE	SUPERPAVE, ASPHALT MIXTURE DESIGN, HMA WEARING COURSE, PG 64-22, 10 TO <30 MILLION ESALS, 25.0 MM MIX, SRL-H

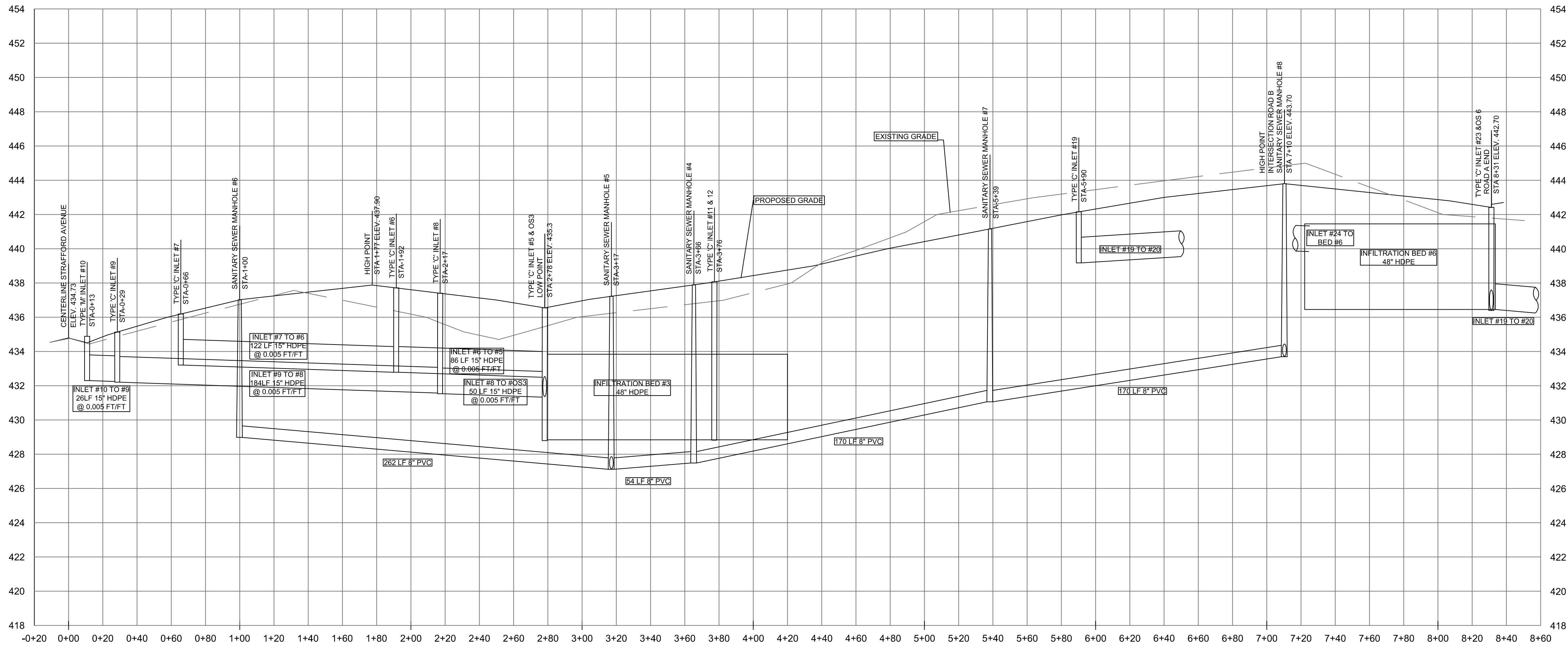
NOTE: CONSTRUCT COURSE AGGREGATE SUBBASE AND BITUMINOUS PAVEMENT IN ACCORDANCE WITH APPLICABLE SECTIONS OF PDOT PLG. 408.



NOTE: WALKWAY SHALL BE CONSTRUCTED IN 20 FOOT SECTIONS WHICH SHALL BE SEPARATED BY 1/4" THICK FELT IMPREGNATED WITH BITUMINOUS MATERIAL. FELT TO BE INSTALLED BETWEEN SECTIONS AND AT ALL STRUCTURES. WALK TO BE SCORED EVERY 4 FEET. CUT REINFORCEMENT BETWEEN SECTIONS. DRIVEWAY CONSTRUCTION MUST CONFORM TO TREDIFFRIN TOWNSHIP STANDARDS.

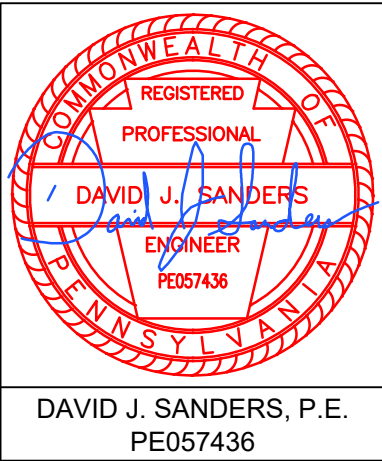


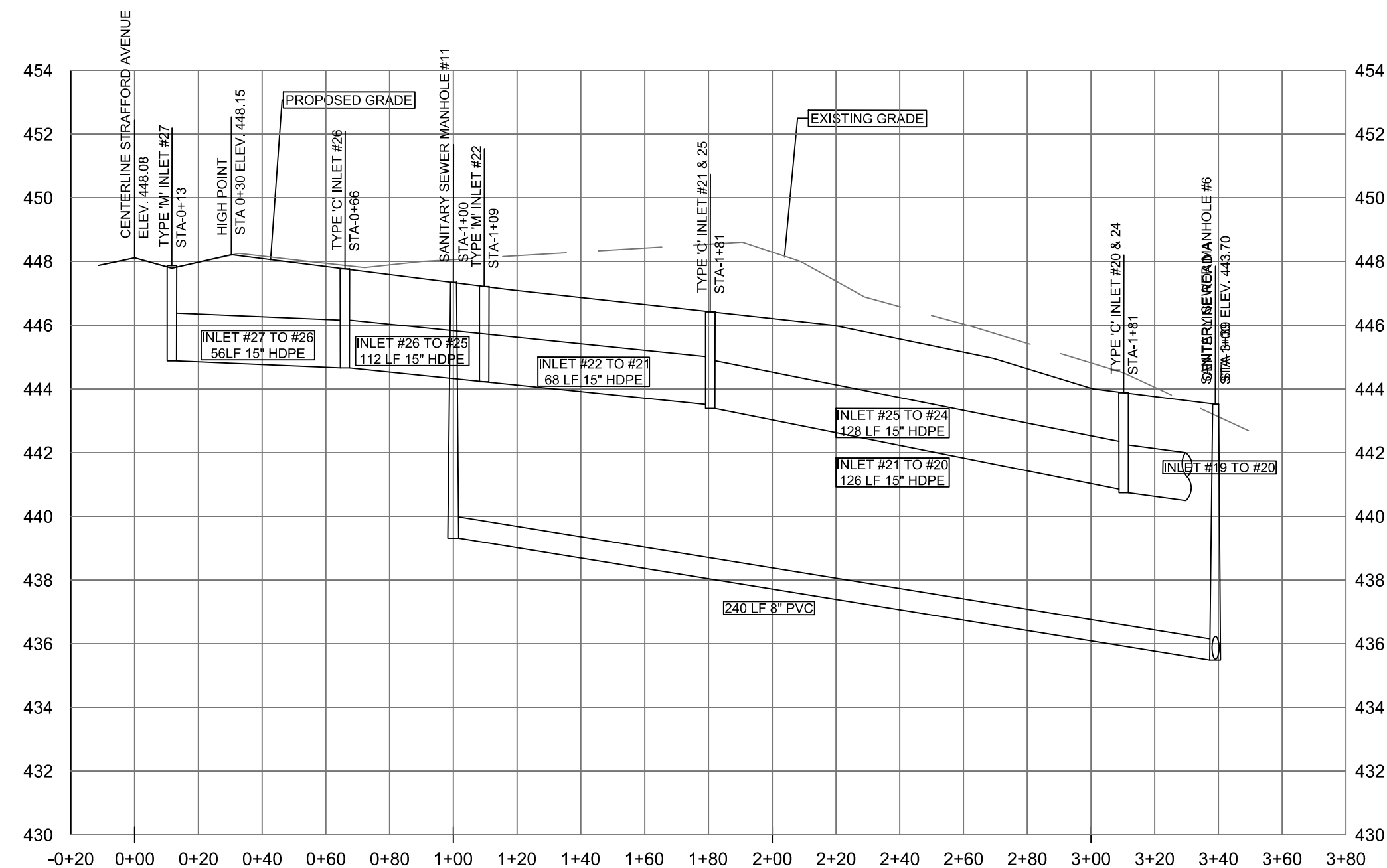
NUM.	DATE	REVISION
PLAN PREPARED BY:		
SITE ENGINEERING CONCEPTS, LLC		
P.O. BOX 1992		
SOUTHEASTERN, PA 19399		
P: 610-240-0450 F: 610-240-0451 E: INFO@SITE-ENGINEERS.COM		
PLAN PREPARED FOR:		
HAMILTON ESTATE		
208 & 228 STRAFFORD AVE AND 18 FORREST LANE		
WAYNE, PA 19087		
RADNOR TOWNSHIP DELAWARE COUNTY PENNSYLVANIA		
DATE: AUGUST 27, 2020		
CONSTRUCTION DETAILS		
SHEET 8 of 15		
NO SCALE		



ROAD A PROFILE

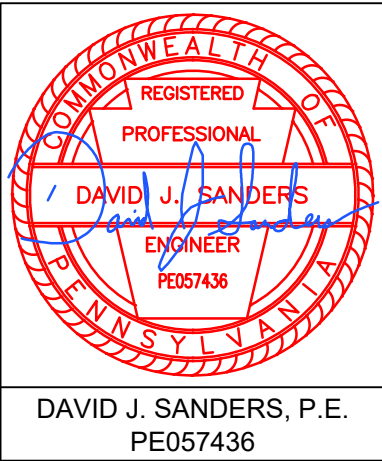
NUM.	DATE	REVISION
PLAN PREPARED BY: SITE ENGINEERING CONCEPTS, LLC P.O. BOX 1992 SOUTHEASTERN, PA 19399		
P: 610-240-0450 F: 610-240-0451 E: INFO@SITE-ENGINEERS.COM		
PLAN PREPARED FOR: HAMILTON ESTATE 208 & 228 STRAFFORD AVE AND 18 FORREST LANE WAYNE, PA 19087		
RADNOR TOWNSHIP DELAWARE COUNTY PENNSYLVANIA		
DATE: AUGUST 27, 2020		
PROFILES		SHEET 10 of 15
DAVID J. SANDERS, P.E. PE057436		SCALE: 1" = 40'

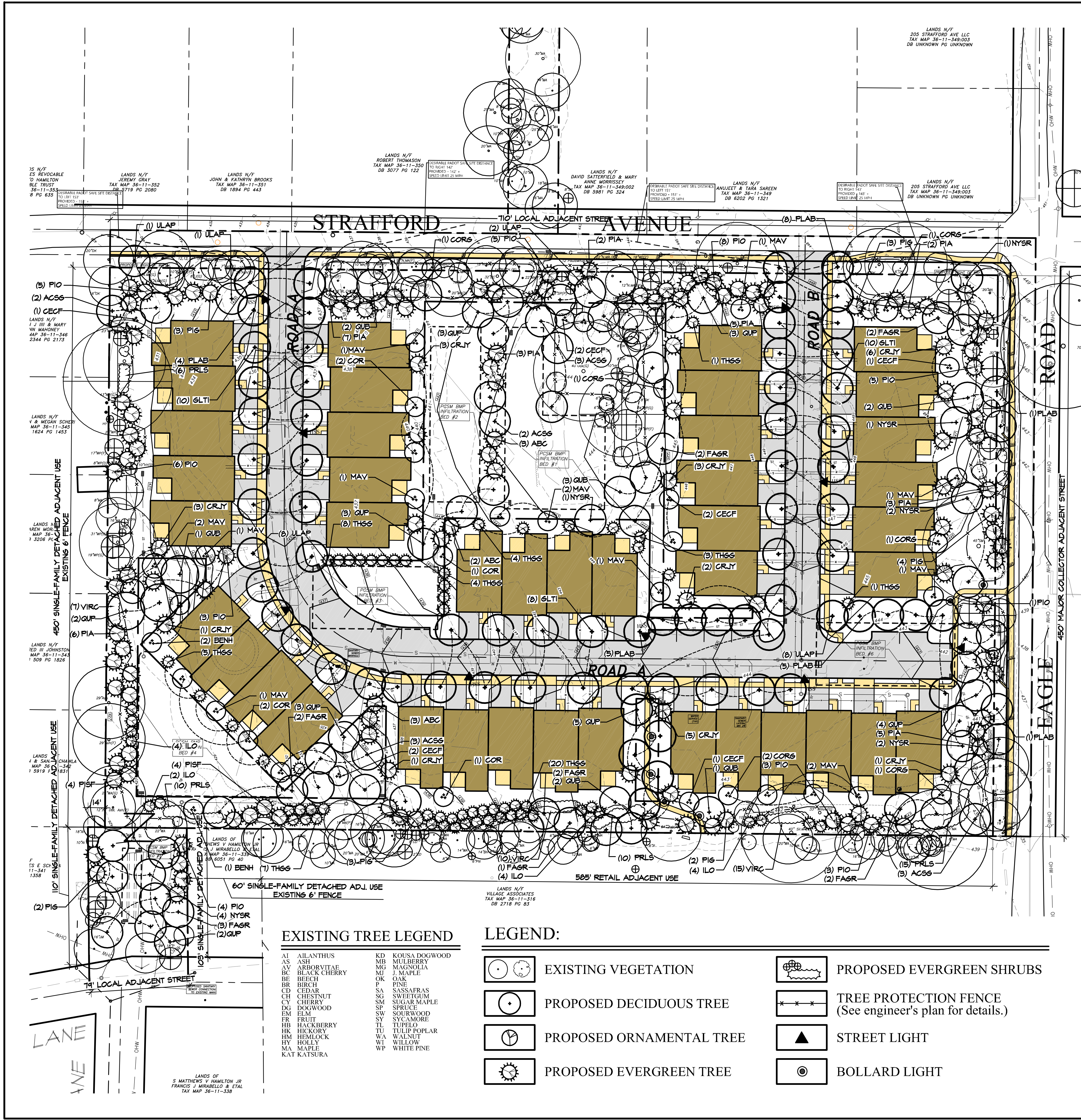




ROAD B PROFILE

NUM.	DATE	REVISION
PLAN PREPARED BY: SITE ENGINEERING CONCEPTS, LLC P.O. BOX 1992 SOUTHEASTERN, PA 19399 P: 610-240-0450 F: 610-240-0451 E: INFO@SITE-ENGINEERS.COM		
PLAN PREPARED FOR: HAMILTON ESTATE 208 & 228 STRAFFORD AVE AND 18 FORREST LANE WAYNE, PA 19087 RADNOR TOWNSHIP DELAWARE COUNTY PENNSYLVANIA		
PROFILES		SHEET 11 of 15 SCALE: 1" = 40'





GENERAL NOTES:

- 1. Base information including topography, grading, utilities, building and existing vegetation location prepared by Site Engineering Concepts, Inc. November 2019.
- 2. This plan is for landscaping purposes only.
- 3. The Landscape Architect's seal applies only to the landscape portion of this plan.
- 4. All plant material is to be field adjusted as necessary to address existing site conditions, proposed unit landscaping, and to ensure that clear sight triangles and clear sight lines are maintained.
- 5. Proposed shade trees shall not be planted over the top of any sewer or water laterals, mains or services. Sewer and water laterals, mains and services are shown on Landscape Plans for reference only. See engineers plans for location of all utilities.
- 6. Utilities are shown on Landscape Plans for reference only. See engineers plans for location of all utilities.
- 7. All trees shall be pruned as necessary to maintain required light levels.

COMPLIANCE WITH SECTION 255-38 Subsection B

SHADE TREES	REQUIRED	EXISTING TO REMAIN	PROPOSED
Existing local street length: 789'			
1 Street tree required at intervals not less than 30' on 1 side	27	28	0
Proposed road length 2,149':			
1 Street tree required at intervals not less than 30' on both sides	71	0	71 (6 shown along trails)

COMPLIANCE WITH SECTION 255-42 (Tables 1 and 2)

BUFFER SCREENS	LENGTH	REQUIRED	EXISTING TO REMAIN	PROVIDED
Buffer screen class 'A' required between proposed Planned Cluster Development use and Major Collector Street.	450'	11 canopy trees 15 evergreen trees	8 canopy trees 6 evergreen trees	3 canopy trees 9 evergreen trees
Buffer screen class 'B' required between proposed Planned Cluster Development use and existing Retail use.	585'	39 evergreen trees 73 evergreen shrubs	0 0	39 evergreen trees 73 evergreen shrubs

COMPLIANCE WITH SECTION 263-4

TREE REPLACEMENT	# TREES REMOVED	REPLACEMENTS REQUIRED	REPLACEMENTS PROVIDED
1 Replacement tree required for every 6"-18" DBH tree removed	156	156	156
3 Replacement trees required for every 19"-29" DBH tree removed (2) being large canopy trees	24	72 total (48 canopy)	72 total (48 canopy)
6 Replacement trees required for every 30"+ DBH tree removed (4) being large canopy trees	6	36 total (24 canopy)	36 total (24 canopy)
	187	264 total (72 to be canopy trees)	264 total (72 to be canopy trees)

Notes:
Replacement trees for hazardous and dead trees are not provided.

COMPLIANCE WITH SECTION 280-91-G

BUFFER	REQUIRED	PROVIDED
Density Modification development requires a 25' buffer along all residentially zoned districts	25' buffer	25' buffer

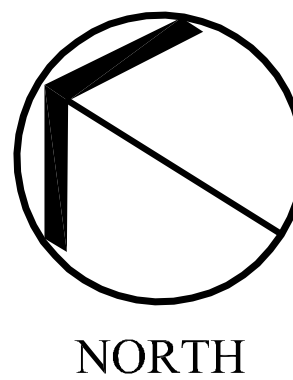
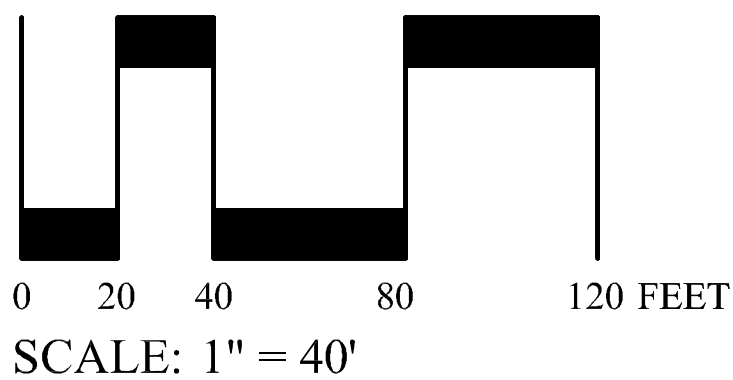
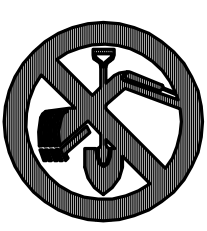
PLANT LIST:

KEY	QTY	BOTANICAL NAME	COMMON NAME	SIZE	REMARKS
DECIDUOUS TREES					
ACSG	13	Acer saccharum 'Green Mountain'	Green Mountain Sugar Maple	2"-2.5" cal.	B&B
BENH	3	Betula nigra 'Heritage'	Heritage River Birch	2"-2.5" cal.	B&B
CECF	9	Cercis canadensis 'Forest Pansy'	Forest Pansy Redbud	2"-2.5" cal.	B&B
COR	6	Cornus 'Rutcan'	Constellation Flowering Dogwood	2"-2.5" cal.	B&B
CORG	7	Cornus 'Rutcan'	Stellar Pink Flowering Dogwood	2"-2.5" cal.	B&B
FAGR	14	Fagus grandifolia	American Beech	2"-2.5" cal.	B&B
GLTI	28	Gleditsia triacanthos v. inermis 'Shademaster'	Shademaster Honeylocust	2"-2.5" cal.	B&B
MAV	14	Magnolia virginiana	Sweet Bay Magnolia	2"-2.5" cal.	B&B
NYSR	11	Nyssa sylvatica 'Red Rage'	Red Rage Black Gum	2"-2.5" cal.	B&B
PLAB	24	Platanus x acerifolia 'Bloodgood'	Bloodgood London Planetree	2"-2.5" cal.	B&B
QUB	11	Quercus bicolor	Swamp White Oak	2"-2.5" cal.	B&B
QUP	25	Quercus phellos	Willow Oak	2"-2.5" cal.	B&B
ULAP	20	Ulmus americana 'Princeton'	Princeton Elm	2"-2.5" cal.	B&B
EVERGREEN TREES					
ABC	8	Abies concolor	White Fir	8"-10' ht.	B&B
CRJY	25	Cryptomeria japonica 'Yoshino'	Yoshino Cryptomeria	8"-10' ht.	B&B
ILO	14	Ilex opaca	American Holly	8"-10' ht.	B&B
PIA	33	Picea abies	Norway Spruce	8"-10' ht.	B&B
PIG	17	Picea glauca	White Spruce	8"-10' ht.	B&B
PLO	43	Picea orientalis	Oriental Spruce	8"-10' ht.	B&B
PISF	8	Pinus strobus 'Fastigiata'	Columnar White Pine	8"-10' ht.	B&B
THGG	53	Thuja plicata 'Green Giant'	Green Giant Arborvitae	8"-10' ht.	B&B
SHRUBS					
PRLS	41	Prunus laurocerasus 'Schipkaensis'	Skip Laurel	24"-30" ht.	Cont.
VIRC	32	Viburnum rhytidophyllum 'Cree'	Cree Leatherleaf Viburnum	24"-30" ht.	Cont.

NOTE:
1. Where there is a discrepancy between the plant list and plan, the plan shall prevail.

Serial Number:

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Glackin Thomas Panzak, Inc.

Paoli Executive Green 1 Suite 300
41 Leopard Road
Paoli, Pennsylvania 19301
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Fax: 610.408.9477
E-mail: plans@glackinplan.com

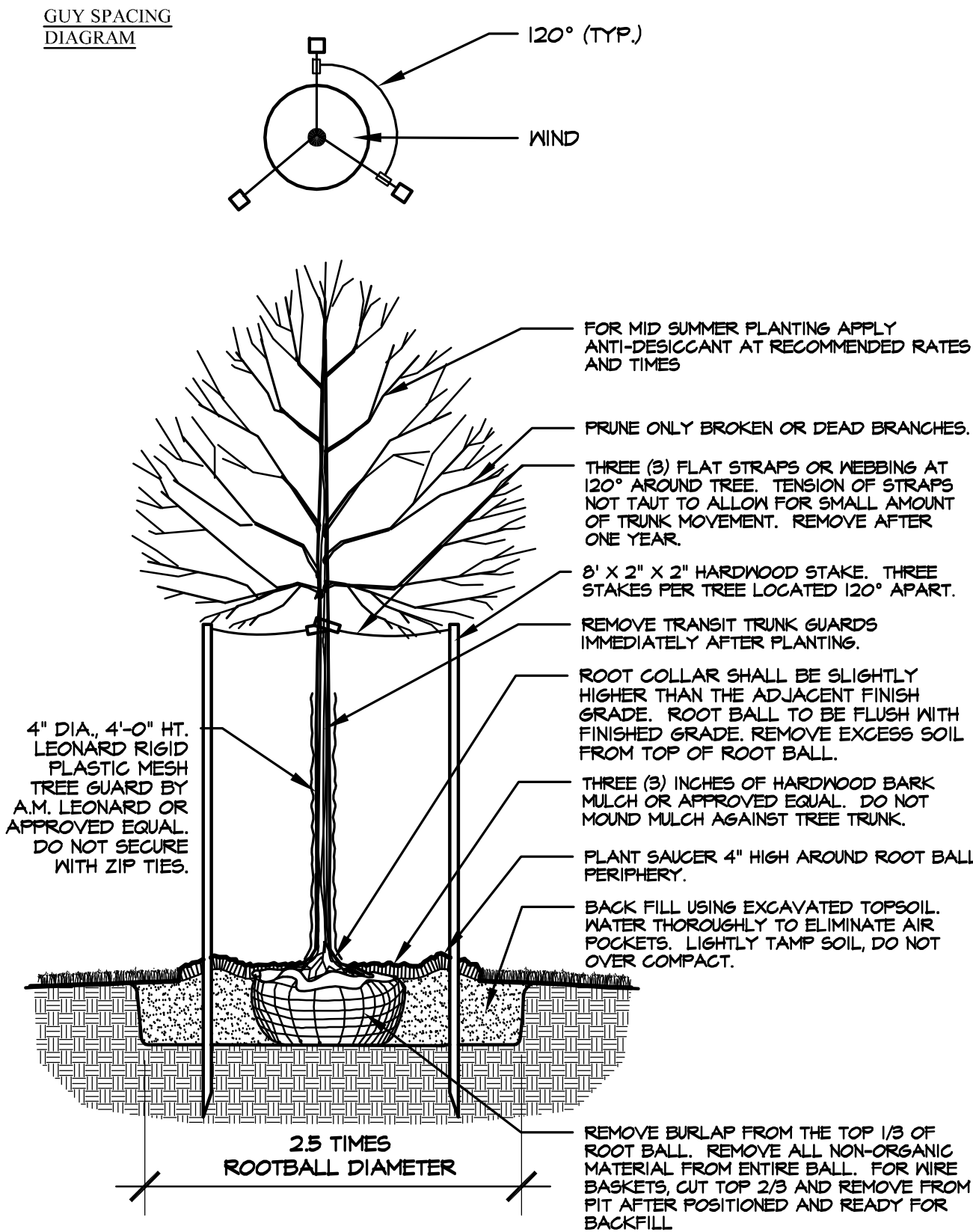


LANDSCAPE PLAN
Hamilton Property
208 & 228 STRAFFORD AVE. & 18 FORREST LANE
RADNOR TOWNSHIP, DELAWARE COUNTY, PENNSYLVANIA

PROJECT #: 19-033
DATE: 08/27/2020 (jr)
REV.:

SHEET:

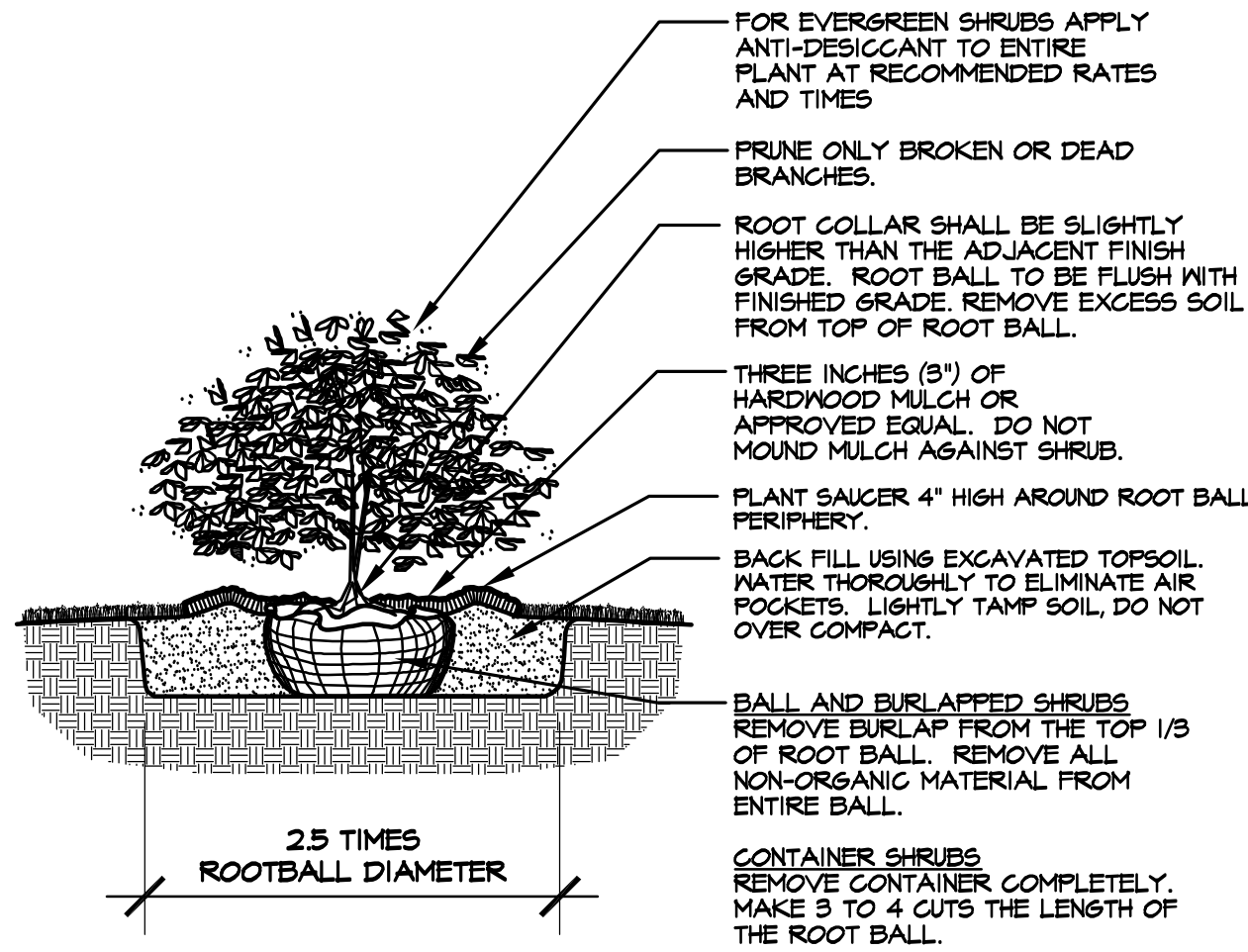
LP-1



NOTES:

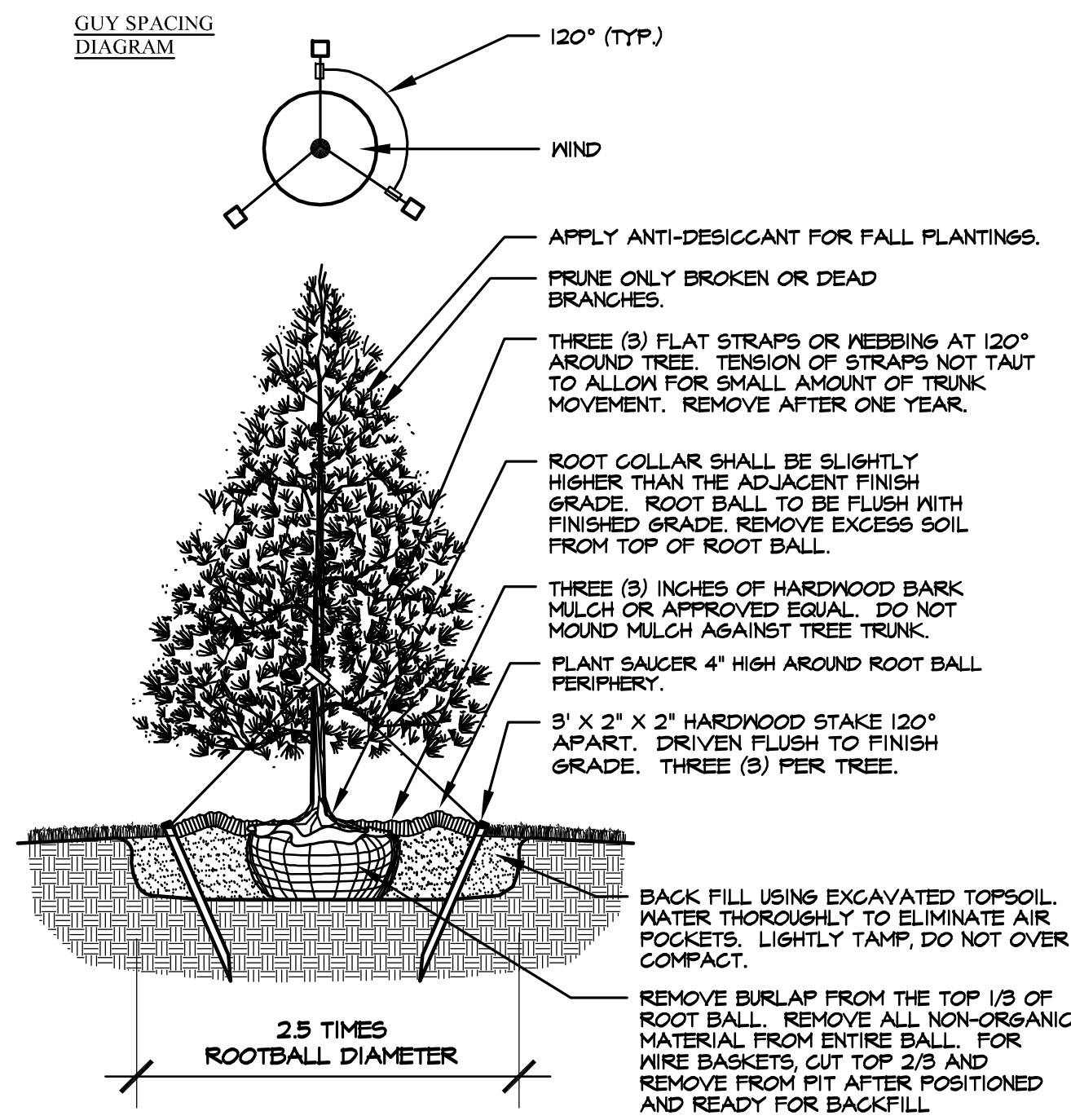
1. Trees with poor quality root balls or root balls that have been cracked or damaged shall be rejected.
2. Trees with central leader broken or dead shall be rejected
3. Trees that do not display the typical characteristics for their species shall be rejected.
4. Flood planting pit with water twice within 24 hours of planting.

1 DECIDUOUS TREE WITH STAKES DETAIL
NTS



NOTE: FLOOD PLANTING PIT WITH WATER TWICE WITHIN 24 HOURS OF PLANTING.

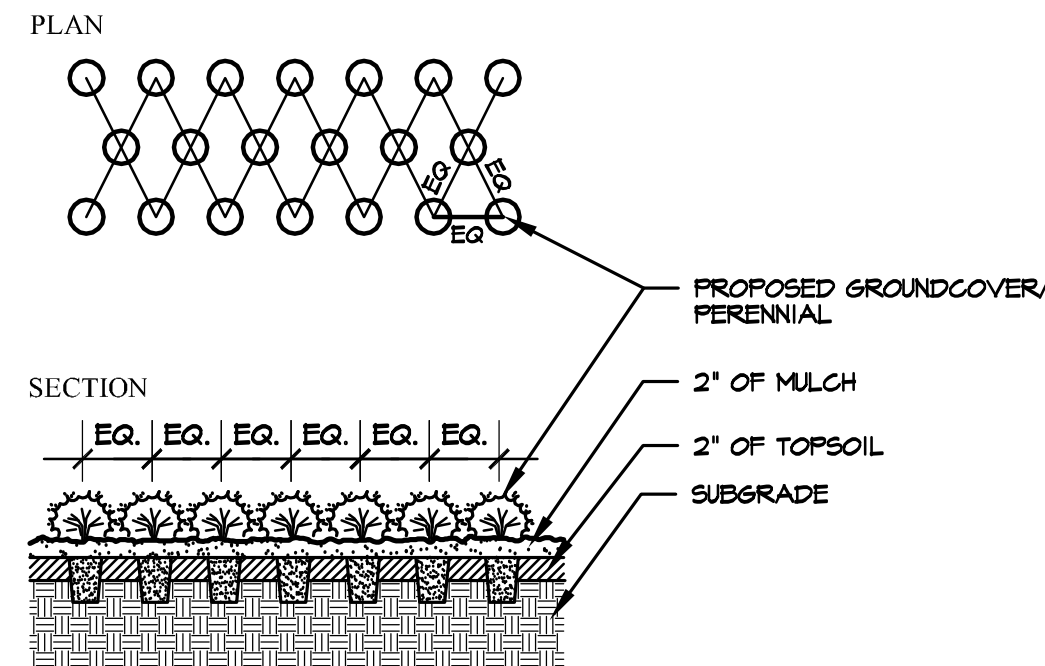
3 SHRUB DETAIL
NTS



NOTES:

1. Trees with poor quality root balls or root balls that have been cracked or damaged shall be rejected.
2. Trees with central leader broken or dead shall be rejected
3. Trees that do not display the typical characteristics for their species shall be rejected.
4. The owner shall be responsible for contacting the contractor, who shall right and secure any tree that leans out of plumb.
5. Flood planting pit with water twice within 24 hours of planting.

2 EVERGREEN TREE WITH STAKES DETAIL
NTS



NOTES:

1. Plants must be planted in bedding mix or topsoil not mulch.
2. See planting list for groundcover species, size, and spacing dimension.

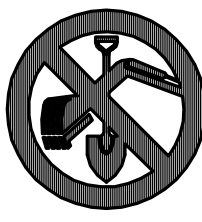
4 PLANTING DETAIL
NTS

PLANTING NOTES:

1. The contractor shall furnish and install all plants shown on the drawings, as specified, and in quantities indicated on the plant list.
2. All plants shall be nursery grown and freshly dug.
3. All plants shall be in accordance with The American Standard for Nursery Stock, latest edition.
4. All plants shall be hardy under climatic conditions similar to those in the locality of the project.
5. Fall Digging Hazard: Any species listed below, if included on the plant list, must not be dug in the fall (October through December) because of risk to the tree's survival. Special exceptions may be granted if the owner is notified in writing and an extended warranty on these plants is agreed upon prior to digging. The following varieties should not be dug in fall: Betula, Carpinus, Celtis, Cercidiphyllum, Crataegus, Cryptomeria, Fagus, Halesia, Ilex (tree form varieties), Liquidambar, Liriodendron, Nyssa, Ostrya, Prunus, Pyrus, Quercus (except Quercus palustris), Salix weeping varieties, and Tilia tomentosa. Digging for Malus and Zelkova varieties should be avoided in fall only when in leaf.
6. All plants shall be typical of their species or variety and shall have a normal habit or growth. They shall be sound, healthy and vigorous, well branched and densely foliated when in leaf. They shall be certified by appropriate State and Federal authorities to be free of disease and insect pests, eggs or larvae. They shall have healthy, well-developed root systems.
7. Plants that do not display typical characteristics for their species shall be rejected.
8. Substitutions: When plants of a specified kind or size are not available within a reasonable distance, substitutions may be made upon written request by the contractor, if approved by the owner and/or municipal authority.
9. Measurement: Dimensions of trees and shrubs shall conform to The American Standard for Nursery Stock, latest edition.
10. Size: All plants shall conform to the measurement specified on the plant list, unless authorized in writing by the Township Arborist and the owner.
11. Balled and burlapped plants shall be dug with firm natural balls of earth, of diameter and depth to include most of the fibrous roots. Container grown stock shall have been grown in a container long enough for the root system to have developed sufficiently to hold its soil together firm and whole. No plants shall be loose in the container.
12. Trees with central leader broken or dead shall be rejected
13. Plants with broken root balls or excessive damage to the crown shall be replaced prior to planting.
14. Plants with poor quality root balls or root balls that have been cracked or damaged shall be rejected.
15. Root balls of all plants shall be adequately protected at all times from sun and drying winds or frost.
16. Conditions detrimental to plants: The contractor shall notify the project representative in writing of all soil or drainage conditions which the contractor considers detrimental to the growth of plants. The contractor shall state the conditions and submit a proposal for correcting the conditions, including any change in cost, for review and acceptance by the project representative.
17. The owner or the owner's representative shall be notified prior to beginning planting operations.
18. All planting shall be at the locations indicated on the drawings. The contractor shall be responsible for planting at the correct grades, alignment, and to the indicated layout of the planting beds.
19. Layout of planting: The contractor shall lay out with identifiable stakes the location of all planting beds as indicated on drawing. The layout of planting shall be approved by the project representative prior to any excavation of plant pits or plant beds.
20. Minor adjustments to plant locations may be necessary due to field conditions and final grading. The contractor shall notify the owner if major adjustments are required.
21. Install plant materials after final grades are established and prior to to planting of lawns unless otherwise acceptable to the owner.
22. Do not install plants when ambient temperatures may drop below 35°F or above 90°F.
23. Do not install plants when wind velocity exceeds 30 mph.
24. Planting shall be done within the following dates - trees, shrubs, groundcover, and perennials shall be planted September 1 through December 1, or April 15 to May 31, and only when local climatic and soil conditions favor satisfactory planting operations. Planting may be done beyond these limits only if requested in writing and approved by the Landscape Architect.
25. Planting operations to commence only when preceding work within the area has been completed. Proceed with and complete planting as rapidly as portions of the site become available, working within seasonal limitations.
26. Planting soil shall be excavated native soil from the planting pit. Planting soil shall be thoroughly mixed, with all rocks, clods and roots removed.
27. All trees shall be staked and guyed according to accepted industry practice, and as noted on the planting details.
28. Each tree and shrub shall be pruned in accordance with The American Nursery and Landscape Association Standards to preserve the natural character of the plant. All dead wood or suckers and all broken or badly bruised branches shall be removed.
29. Mulch: Immediately after planting operations are completed, all trees and shrub planting pits shall be covered with a 3" (three inch) layer of double shredded hardwood bark mulch, or other material approved by the owner or the owner's representative. A granular pre-emergent weed control shall be spread prior to mulching. The limit of this mulch for deciduous trees and single evergreen trees shall be the area of the pit. For shrub and perennial beds and for evergreen tree clusters, a continuous, mulched bed shall be created.
30. Trees in leaf, including evergreens, when planted shall be treated with anti-desiccant such as Wilt-Pruf® if planted during the months of June through September, November, and December.
31. Weed control: All planting areas shall be free from weeds prior to the beginning of planting operation. Contact herbicide sprays should only be used as required and all manufacturer's specifications followed.
32. Guarantee: All plant material shall be guaranteed by the contractor for twelve (12) months from the date of installation. The owner shall be responsible for maintenance unless otherwise agreed with contractor. It shall be the contractor's responsibility to monitor the project during the guarantee and notify the owner if problems develop with the plant material. Any material that is 25 % dead or more shall be considered dead and must be replaced at no charge. A tree shall be considered dead when the main leader has died back, or there is 25% of the crown dead.
33. All debris resulting from landscape contracting operations shall be cleaned up and removed from the site on a weekly basis.
34. Watering: Landscape contractor is responsible for watering sufficiently at the time of planting and until the job is completed, accepted and turned over to the owner.

Serial Number:

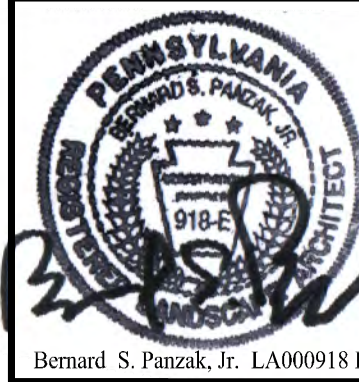
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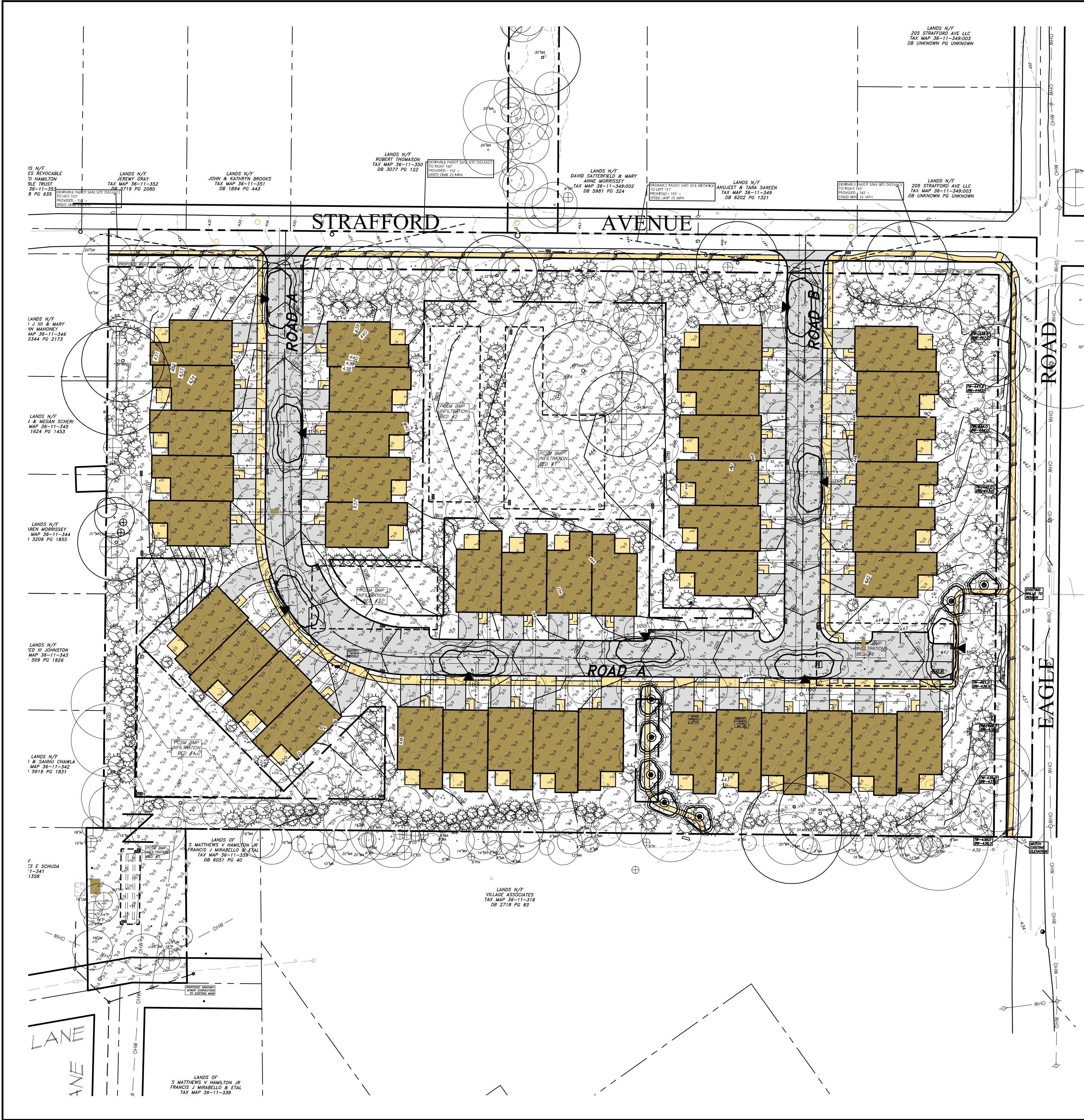


LANDSCAPE PLAN
Hamilton Property
208 & 228 STRAFFORD AVE. & 18 FORREST LANE
RADNOR TOWNSHIP, DELAWARE COUNTY, PENNSYLVANIA

PROJECT #: 19-033
DATE: 08/27/2020 (jr)
REV.:

SHEET:

LP-2



GENERAL NOTES:

- 1. Base information including topography, grading, utilities, building and existing vegetation location prepared by Site Engineering Concepts, Inc. November 2019
- 2. This plan is for lighting purposes only.
- 3. The Landscape Architect's seal applies only to the lighting portion of this plan.

LIGHTING NOTES:

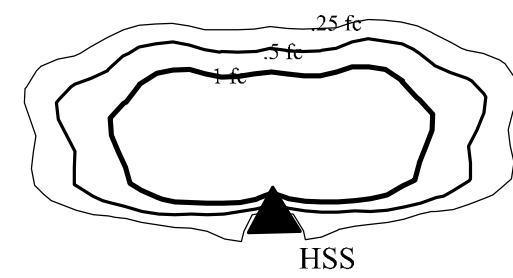
- 1. Lighting to be installed to meet all appropriate national and local codes.
- 2. All light fixtures to be approved by Owner or Owner's representative.
- 3. All circuits to be installed by a licensed electrician.
- 4. All cable to be run in conduit unless approved in writing by the owner.
- 5. Contractor to provide sufficient nighttime adjustment to all lighting to satisfy Township requirements and Owner or Owner's representative. Fixture on/off control by circuit.
- 6. All lighting to be installed according to manufactures recommendations.
- 7. All electrical conduits shall be sch 40 pvc and all bends shall be "sweep" type for pulling wire. Conduit to extend beyond edge of hardscape element by 12" min. Conduit to be temporarily capped and location staked prior to backfilling.
- 8. While extending conduits under existing structures the contractor shall be responsible for damage to existing structures such as, but not limited to, paving, irrigation, pool piping, masonry piers and underground drainage pipe etc.
- 9. All lighting fixtures to be controlled by photo-cells.
- 10. All final light locations to be approved by Owner or Owner's representative.
- 11. All final light locations to be field adjusted, "aimed" as necessary so as to avoid shining of light into windows and doors. Contractor to arrange a pre-construction meeting with the owner and owners representatives including masons, and other individuals familiar with any underground facilities. As-built drawing should be requested of all such facilities.
- 12. Contractor shall spray paint trench layout prior to commencement. Owner or Owner's representative shall approve layout.
- 13. Electrical contractor to prepare a plan of all as-built underground electric runs.
- 14. Project electrical engineer to provide power plan, and ConCheck calculations as required.
- 15. In the event that the contractor proposes light fixture substitutions, requests must be received by the Landscape Architect for review (14) fourteen days prior to bid date. Failure to submit within that deadline renders all substitution requests void, and originally specified fixture(s) will be supplied. The contractor shall submit the following:
 - A. Township required copies of all mandatory submittals for both the originally specified fixture(s) and the proposed substitution(s).
 - B. Contractor's written certification that the proposed substitution(s) conforms to all requirements of the contract documents in every respect and is appropriate for the applications indicated in the documents.
 - C. Contractor's written statement indicating the effect of the substitution(s) on the construction schedule compared to the schedule with the originally specified fixture(s).
 - D. Contractor-net unit price for the originally specified fixture(s) and for the proposed substitute fixture(s).
 - E. One sample of the proposed substitution fixture(s) with specified lamps and cord and plug connection for 277 volt operation.
 - F. Contractor's written certification that any alterations that may result from the proposed lighting fixture substitution(s) will be designed and constructed at the contractor's expense.
 - G. Reimbursement to the landscape Architect for all time associated with review of fixture substitutions(s). Payment shall be made in advance of the review, based on the Landscape Architect's hourly rates for the personal involved in the review.
 - H. Contractor's written waiver of rights to additional payment and/or time that may become necessary should the proposed substitution(s) fail to perform in a manner that is equivalent to the originally specified fixture(s).
- 16. Post-approval alterations to lighting plans or intended substitutions for approved lighting equipment shall be submitted to the Township for review and approval.

LIGHTING LEGEND:

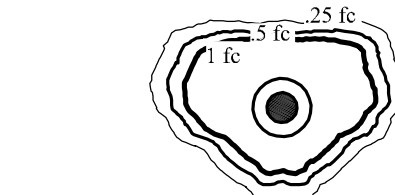
SYM.	LIGHT TYPE	QTY.	MFG.	MODEL #	REMARKS
	STREET LIGHT	9	AAL	PROV2-36L-325-3K7-3-BL-HS	AAL Providence Medium, LED, Type III Optics, 3K color temperature, photocell control by circuit, 14' height, mounted on 4" AAL POLE MODEL #DB6, Fixture and Pole Finish: Black
	BOLLARD LIGHT	7	AAL	PROB-Y3-3030-BL-RD	AAL Providence Bollard, LED, Type III Optics, 3K color temperature, photocell control by circuit, Round Shaft 5" Diameter, Finish: Black

ISO FOOTCANDLE DIAGRAM :

SITE LIGHT:

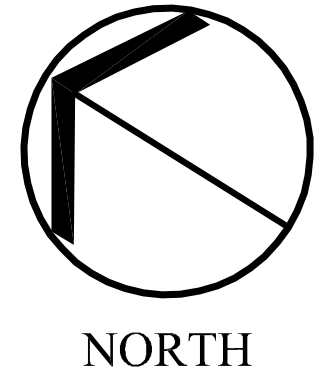
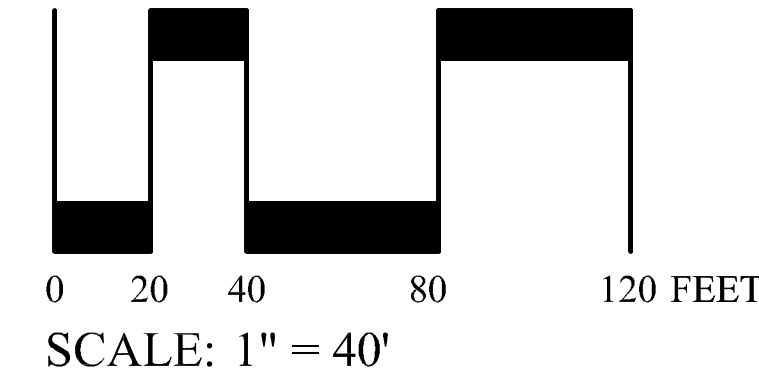


BOLLARD LIGHT:



Typical footcandle distribution values are indicated in diagram. The same values, 0.25 fc, 0.50 fc and 1.00 fc, are indicated on all proposed light fixtures.

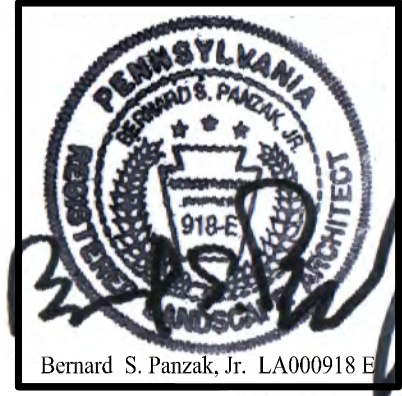
Serial Number:
CALL BEFORE YOU DIG!
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3 WORKING DAYS NOTICE FOR
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DAYS IN DESIGN STAGE-STOP CALL
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Glackin Thomas Panzak, Inc.

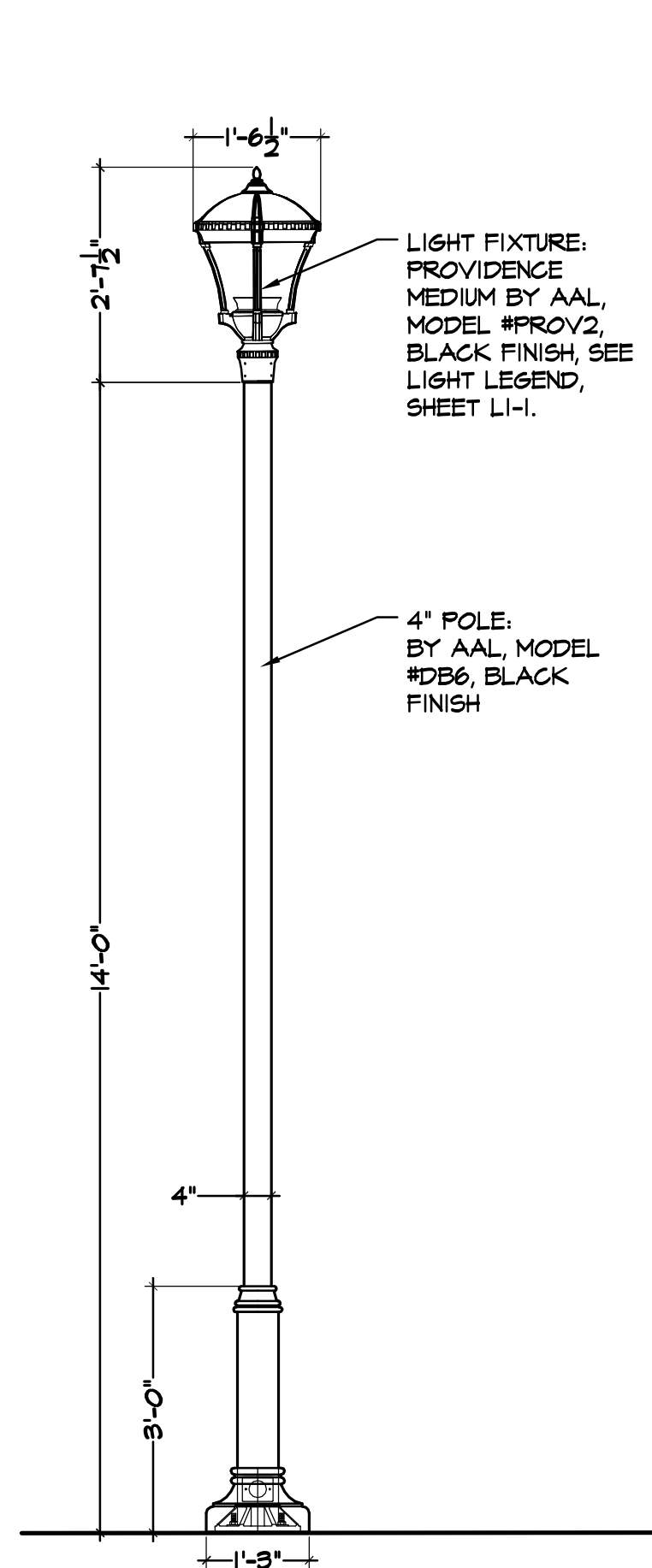
Paoli Executive Green I Suite 300
41 Leopard Road
Paoli, Pennsylvania 19301
610.408.9011
Fax: 610.408.9477
E-mail: plans@glackinplan.com



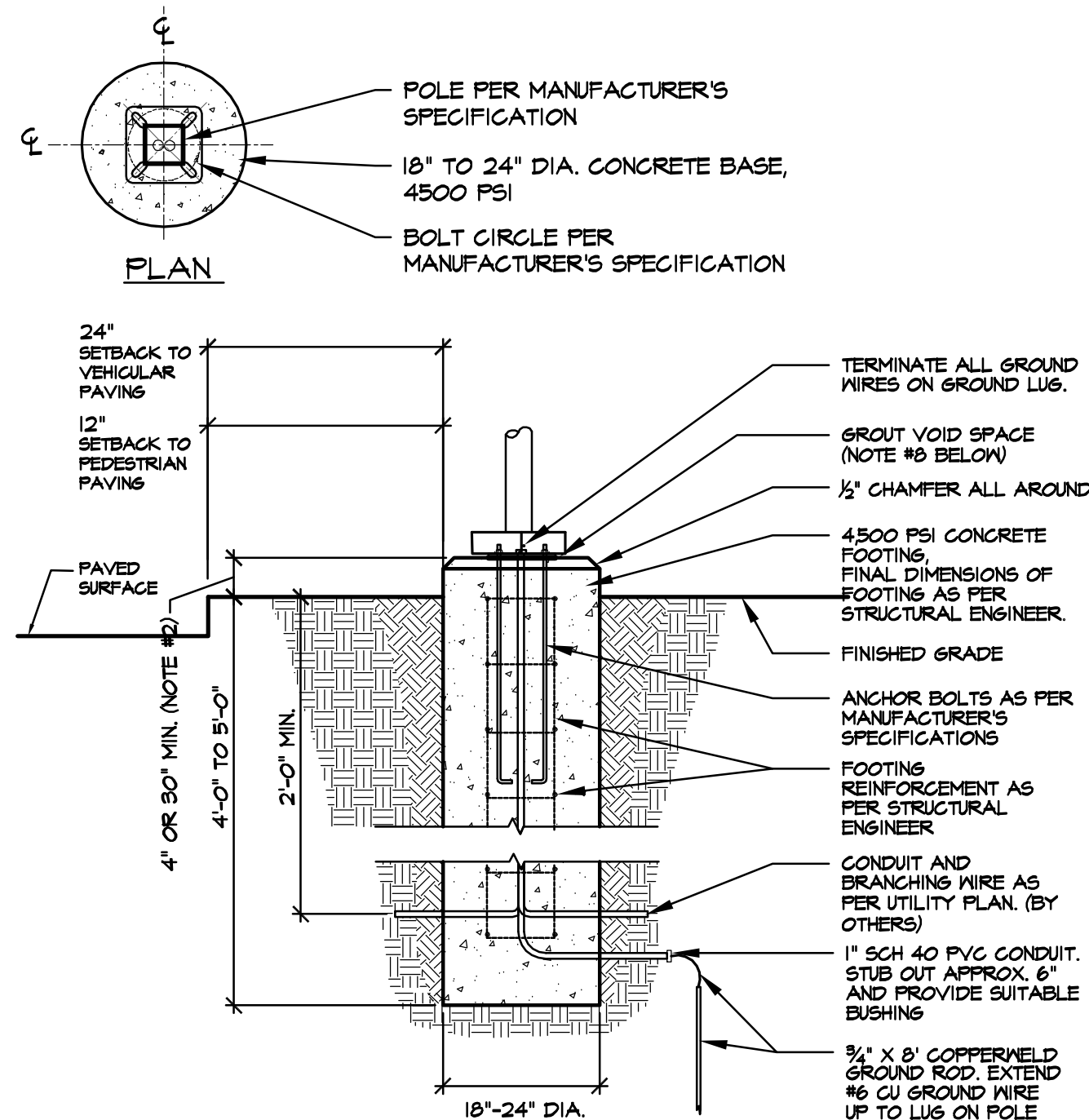
LIGHTING PLAN
Hamilton Property
208 & 228 STRAFFORD AVE. & 18 FORREST LANE
RADNOR TOWNSHIP, DELAWARE COUNTY, PENNSYLVANIA

PROJECT #: 19-033
DATE: 08/27/2020 (jr)
REV.:

SHEET:
LI-1

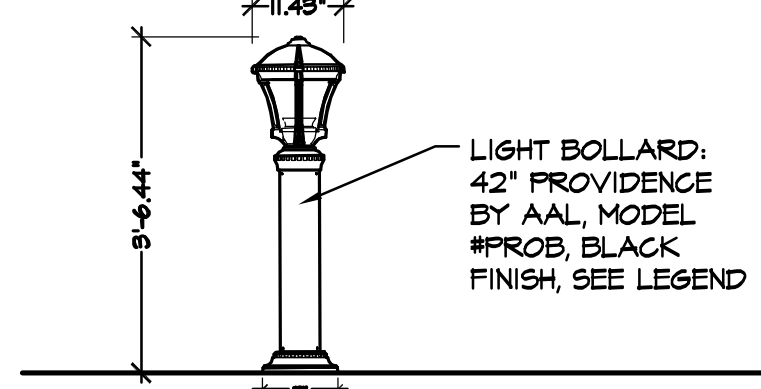


1 STREET LIGHT DETAIL
Scale: 1/2"=1'-0"

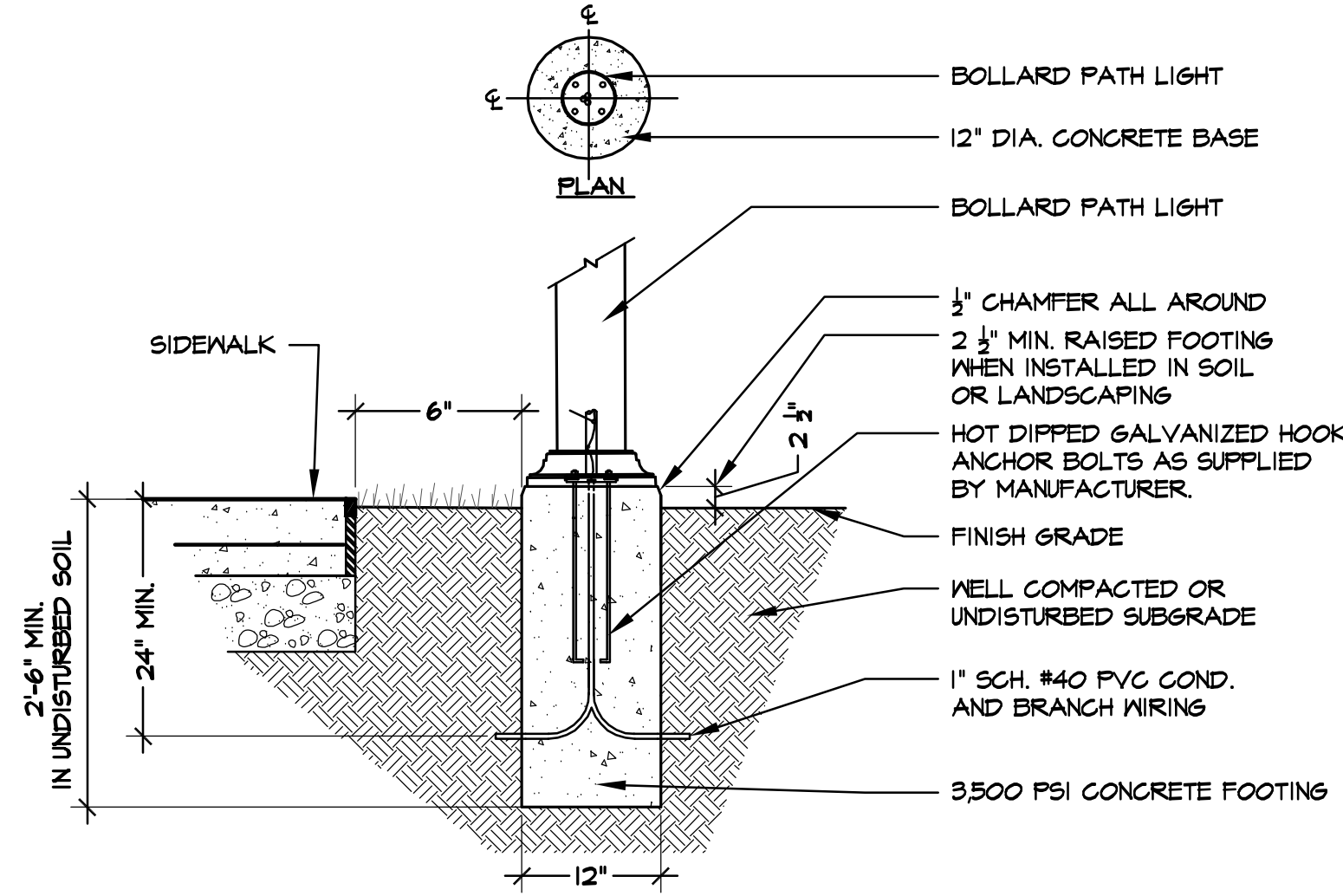


- NOTES:
1. THIS DETAIL IS FOR BID AND BUDGETARY PURPOSES ONLY.
 2. A SCHEDULE FOOTING SHALL EXTEND 30" ABOVE FINISHED GRADE WHERE LIGHT POLES ARE TO BE DIRECTLY ADJACENT TO A PARKING SPACE.
 3. CONTRACTOR SHALL ENSURE DESIGN IS PREPARED BY A QUALIFIED STRUCTURAL ENGINEER CONSIDERING LIGHTING MANUFACTURER REQUIREMENTS, LOCAL WIND LOADS AND SITE SPECIFIC SOIL PARAMETERS.
 - A. THE STRUCTURAL ENGINEER SHALL BE NOTIFIED OF THE INTENT TO MOUNT ANYTHING ASIDE FROM THE LIGHT FIXTURE INCLUDING (BUT NOT LIMITED TO) CAMERAS, BANNERS, FLAGS, SIGNAGE, ETC. AS IT WILL IMPACT THE POLE AND FOUNDATION DESIGN.
 - B. SOME SITE LOCATIONS AND/OR CONDITIONS MAY REQUIRE VIBRATION DAMPENING MEASURES AS DETERMINED BY A STRUCTURAL ENGINEER.
 4. ACTUAL DIAMETER AND DEPTH OF FOOTING SHALL BE DETERMINED BY THE STRUCTURAL ENGINEER.
 5. ALL REBAR SHALL BE MAINTAINED AT A 3" MIN. CLEARANCE FROM SURFACES.
 6. FIXTURE MUST BE GROUNDED IN ACCORDANCE WITH LOCAL CODES OR THE NATIONAL ELECTRICAL CODE. FAILURE TO DO SO MAY RESULT IN SERIOUS PERSONAL INJURY.
 7. POLES SHOULD NEVER BE ERECTED WITHOUT THE LUMINAIRE INSTALLED. WARRANTY IS VOIDED IF THE POLE IS ERECTED WITHOUT THE LUMINAIRE.
 8. THE WARRANTY IS VOIDED IF THE POLE IS NOT GROUTED UNDER THE ENTIRE BASE AFTER INSTALLATION.

2 TYPICAL LIGHT POLE FOOTING DETAIL
Scale: 1/2"=1'-0"



3 BOLLARD LIGHT DETAIL
Scale: 1/2"=1'-0"



4 BOLLARD FOOTING DETAIL
Scale: 1"=1'-0"

DATE: LOCATION: TYPE: PROJECT: CATALOG #:

ORDERING GUIDE

Example: PROV2 36L 25 LED 295 4500K Macosco Crossbar MK7 5000K 75 CCT 2 Type I AGN Antique Green

Mounting	LED Quantity	Lumen Output	CCT/CRI	Distribution	Finish
PROV2	36L 25 LED	295	4500K Macosco Crossbar	MK7 5000K 75 CCT	2 Type I AGN Antique Green
PROV2	Providence Medium 250	325	5200K 4500 Lumens	MK7 4000K 75 CCT	2 Type I BL Black
		150	3000K 4500 Lumens	MK7 3000K 75 CCT	3 Type II BLT Matte Black
		625	3000K Macosco Crossbar	AW Type III WGR	CRD Corbin
		615	4500K 7500 Lumens	KG Type V Square	DB Dark Bronze
				SW Type VI Wide	DRN Dark Green
					GT Granite
					LG Light Gray
					MAL Matte Aluminum
					MOB Metallic Bronze
					MG Metallic Gray
					TF Titanium
					VBV Venetian Blue
					WOB Weathered Blue
					WH White
					CC Custom Color **

HOUSING

PROV2

36L 25 LED

295

4500K Macosco Crossbar

MK7 5000K 75 CCT

2 Type I AGN Antique Green

325

5200K 4500 Lumens

MK7 4000K 75 CCT

2 Type I BL Black

150

3000K 4500 Lumens

MK7 3000K 75 CCT

3 Type II BLT Matte Black

625

3000K Macosco Crossbar

AW Type III WGR

CRD Corbin

615

4500K 7500 Lumens

KG Type V Square

DB Dark Bronze

SW Type VI Wide

DRN Dark Green

GT Granite

LG Light Gray

MAL Matte Aluminum

MOB Metallic Bronze

MG Metallic Gray

TF Titanium

VBV Venetian Blue

WOB Weathered Blue

WH White

CC Custom Color **

Mounting

CL Clear Lens

DL Diffused Lens 1

HS House Side Street 2

PRN Brass Cabinet Finish

SPK Brass Cabinet Shells

SP Single (available 177)

DF Double End (205, 295)

Options

MS House Side Street 2

PRN Brass Cabinet Finish

SPK Brass Cabinet Shells

SP Single (available 177)

DF Double End (205, 295)

Mounting Options

ADS Adjustable 37° COI Free

PGA-C Horizontal Adjuster

EPA-C Egress Adjuster Contemporary

Voltage

UNV 120V/277V

Dimensions

18" 3" (475 mm)

31 5/8" (800 mm)

Notes:

1. Varies by body.
2. Diffused lens is available only with T2 and T34.
3. House Side Street is available only with T1, T2, T3 and T34.
4. Consult factory for custom color match and consult with options.

Page 2 of 2 Rev: 04/16/20
PROV2

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Hubbell Lighting

5 STREET LIGHT CUT SHEET
NTS

6 BOLLARD LIGHT CUT SHEET
NTS

Providence® LED™ – Bollard PROB TYPE

FEATURES

- Reliable uniform illumination
- Types III, IIII, IV and V distribution
- 3000K, 4000K, 5000K CCT
- 0-15V dimmable
- 25K/50KVA surge suppression
- Thermal Protection
- 18 standard powder coat finishes
- Die-Cast housing
- Full Cut-off optical system

SPECIFICATIONS

11 07020 Lumens

6.68 160mm

14.24 357mm

Bottom View

3000K and warmer CCTs only

ORDERING INFORMATION

1. MODEL: PROB

2. DISTRIBUTION: Y2, Y3, Y4, Y5

3. LUMEN PACKAGE: 3000, 4000, 5000 CCT, 45 watt

4. COLOR: WH, AC, BL, BLT, DB, DRN, GT, LG, MAL, MOB, MG, TF, VBV, WOB, WH, CC

5. OPTIONS: FLS, FLS-C, FLS-CF, FLS-CF-C, FLS-CF-CF, FLS-CF-CF-C

6. MOUNTING: FLS, FLS-C, FLS-CF, FLS-CF-C, FLS-CF-CF, FLS-CF-CF-C

ARCHITECTURAL AREA LIGHTING
16201 East Dale Ave., 1 City of Industry, CA 91745
P: 626.955.5555 | F: 626.955.5555 | www.aal.com
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JOB TYPE NOTES

6 BOLLARD LIGHT CUT SHEET
NTS

Serial Number:

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3 WORKING DAYS NOTICE FOR
CONSTRUCTION PHASE AND 10 WORKING
DAYS IN DESIGN STAGE-STOP CALL
Pennsylvania One Call System, Inc.
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Glackin Thomas Panzak, Inc.

Paoli Executive Green I
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E-mail: plans@glackinplan.com

PENNSYLVANIA
LANDSCAPE
ARCHITECT
Bernard S. Panzak, Jr. LA000918 E

LIGHTING PLAN
Hamilton Property
208 & 228 STRAFFORD AVE. & 18 FORREST LANE
RADNOR TOWNSHIP, DELAWARE COUNTY, PENNSYLVANIA

PROJECT #: 19-033
DATE: 08/27/2020 (jr)
REV.:

SHEET:
LI-2

**DENSITY MODIFICATION
REDEVELOPMENT OF
208 AND 228 STRAFFORD AVENUE AND
18 FORREST LANE
DEVELOPMENT IMPACT STATEMENT**

**FOR SUBMISSION TO:
RADNOR TOWNSHIP, DELAWARE COUNTY, PA**

PREPARED FOR:

Trustees of the Dorrance Hamilton 3/15/1996 Revocable Agreement of Trust
c/o Mr. D. Charles Houder
551 W. Lancaster Avenue, Suite 307,
Haverford, PA 19041

COMPILED BY:

George W. Broseman, Esquire
Daniel P. Rowley, Esquire
Kaplin Stewart Meloff Reiter & Stein
910 Harvest Drive
P.O. Box 3037
Blue Bell, PA 19422

I. INTRODUCTION

This Development Impact Statement is being submitted in connection with the Conditional Use application for the redevelopment of the properties now known as 208 and 228 Strafford Avenue and 18 Forrest Lane (collectively “**Tract**”). The Tract includes Delaware County Folio Numbers: 36-01-00538-00; 36-01-00539-00; 36-01-00540-00; and 36-01-00236-02.

The Applicant has filed with the Township a conditional use application under Code § 280-29.B(1) for Density Modification Development to permit the construction of forty-one townhomes and related improvements (collectively “**Project**”). The Applicant has also filed with the Township a preliminary land development application for the Project.

The Tract and the Project are shown on the plan set entitled “208 & 228 Strafford Avenue, 18 Forrest Lane Conditional Use and Preliminary Land Development Plans,” containing 15 sheets dated August 27, 2020 (sheets 1-11 prepared by SITE Engineering Concepts, LLC, sheets 12-15 prepared by Glackin Thomas Panzak) (collectively referred to as “**Plans**”).

This Development Impact Statement addresses the applicable Development Impact Statement requirements for a conditional use application under Radnor Township Code §280-135.G as follows (*responses to Code requirements in italics*):

II. CODE §280-135.G.(1) & (2) -DEVELOPMENT IMPACT STATEMENT

Code §280-135.G.(1)

Comprehensive Plan

(A) An analysis of the consistency of the proposed use with the Radnor Township Comprehensive Plan, as amended. The analysis shall include, but not be limited to, the compatibility with environmental and natural resources; housing, demographics, and socioeconomics; business and economic development; transportation and circulation plan; open space and recreation; historical and archaeological resources; community services and facilities; and the land use plan sections of the Comprehensive Plan.

Under Pennsylvania law a township’s Comprehensive Plan is not regulatory. “A comprehensive plan is not forever binding, nor does it actually regulate land use.” Michaels Dev. Co. v. Benzinger Twp. Bd. of Sup’rs, 413 A.2d 743, 747 (1980). “A comprehensive plan is, by nature, abstract and recommendatory and does not forever control a zoning ordinance enacted to give it legal effect.” Appeal of Molnar, 441 A.2d 487, 489 (1982). “The comprehensive plan is a general guideline to the legislative body of the municipality for its consideration of the municipality’s program of land utilization and the needs and desires of the community.” Forks Twp. Bd. of Sup’rs v. George Calantoni & Sons, Inc., 297 A.2d 164, 166–67 (1972). Additionally, when a particular use is permitted by conditional use, there is a legislative acceptance that the use is consistent with the zoning plan and is thus consistent with the spirit, purposes, and intent of a Township’s Comprehensive Plan. In re Cutler Grp., Inc., 880 A.2d 39, 45 (Pa. Commw. Ct. 2005)

The Project is permitted by conditional use under Code § 280-29.B(1) as a Density Modification Development and therefore there exists a legislative determination that Density Modification Developments, such as this Project, are consistent with the spirit, purpose and intent of the Township's Comprehensive Plan.

The following is an analysis of the Project under the Radnor Township Comprehensive Plan for the sections enumerated in Code §280-135.G(1)(a):

i. Environmental and Natural Resources:

The Properties do not contain any water resources, wetlands, floodplains or riparian areas. The Comprehensive Plan advises to direct development to areas of minimum environmental sensitivity, such as the Tract. (Radnor Comprehensive Plan pgs ii, 2-1). Furthermore, the Tract currently has no stormwater management controls. The Project would provide stormwater management controls in furtherance of the Township's desire to promote the management of water resources. (Radnor Comprehensive Plan pgs ii, 2-1).

ii. Housing, Demographics, and Socioeconomics:

The Comprehensive Plan includes the Township's goals to "allow for increased housing density immediately surrounding commercial areas and particularly near transit centers" and to "promote conservation development strategies for new development on large parcels." (Radnor Comprehensive Plan pgs vi, 3-1). The proposed Project is located in close proximity to the commercial corridor along Lancaster Avenue and the Strafford Station along the SEPTA Paoli/Thorndale Regional Rail Line and promotes conservation development strategies by clustering development, providing common open space and providing for stormwater management.

iii. Business and Economic Development:

The zoning of the Tract does not allow for commercial use. The Project is located in close proximity to the commercial corridor along Lancaster Avenue and will serve as a transitional development from the commercial area along Lancaster Avenue and along Eagle Road to the residential areas northward from Lancaster Avenue. The Project will provide an expanded customer base for these commercial uses along Lancaster Avenue, such as the Lancaster Farmer's market in the Strafford Shopping Center and the various shops and restaurants in the adjoining Eagle Village Shops. The Tract is within walking distance of these businesses and will allow visits from the Project without adding traffic.

iv. Transportation and Circulation Plan:

The Project's close proximity to the Strafford Station SEPTA Regional Rail Station will encourage new residents to use this public transit station, which furthers the goals of the Comprehensive Plan. (Radnor Comprehensive Plan pgs x, 5-2). For additional information regarding traffic related to the Project, a Traffic Impact Study prepared by Frank Tavani, P.E., PTOE was submitted with the Conditional Use application. The Project will also improve pedestrian circulation by constructing sidewalks on the exterior and interior of the Project tract. (Radnor Comprehensive Plan pgs x, 5-2).

v. *Open Space and Recreation:*

The Project proposes common open space in a centralized green area and along the edges of the Tract for the use of the residents. These areas will be restricted from further development through a Planned Community Declaration.

vi. *Historical and Archaeological Resources:*

The Property contains no historical or archaeological resources.

vii. *Community Services and Facilities:*

*An analysis of the Project's impact on community services and facilities can be found in the Fiscal Impact Study prepared by Erik Hetzel, AICP/PP, LEED AP that was submitted with the Conditional Use application and is attached to this report as **Appendix A**.*

viii. *Land Use Plan:*

One of the stated goals of the Comprehensive Plan is to "accommodate reasonable growth using innovative growth management techniques such as transit oriented development, traditional neighborhood design, and other flexible design techniques that harmonize with and enhance the existing communities." (Radnor Comprehensive Plan pgs xxv, 10-1, 10-2). The Project utilizes the flexible design technique of density modification development as permitted in the R-4 District in Article XIX of the Zoning Ordinance. The use of this provision allows for a layout that includes common open space that would not be required under a conventional development under the R-4 District provisions.

Furthermore, the Project is located in close proximity to the commercial corridor along Lancaster Avenue and the Strafford Rail Station and will provide an expanded customer base for the commercial uses.

Natural Features

(B) The impact of the proposed use on floodplains, waterways, heavily wooded areas, steep slopes, and other sensitive natural features located upon and adjacent to the site, if any.

There are no floodplains, waterways, heavily wooded areas, or steep slopes on the site. The Project is located in a fully developed area of Radnor Township, surrounded by other residential and commercial development.

Transportation

(C) The proposed use's impact on the Township and regional transportation system(s) and the ability of adjacent streets and intersections to efficiently and safely handle the traffic generated by the proposed development. This analysis shall include all modes of transportation and shall be based on current Pennsylvania Department of Transportation (PennDOT) requirements within their Policies and Procedures for Transportation Impact Studies (PennDOT Strike-off Letter 470-

09-4, dated 2009, as amended). The applicant shall address any measures proposed to be implemented in order to mitigate any adverse impacts.

*A Traffic Impact Study prepared by Frank Tavani, P.E., PTOE was submitted with the Conditional Use application and is attached to this report as **Appendix B**. This Traffic Impact Study addresses the Project's impact on the Township's transportation systems including the surrounding streets and intersections. Additionally, the Project's close proximity to the Strafford Rail Station and commercial uses within walkable distance will reduce traffic and will encourage new residents to use this public transit station.*

It is also noted that traffic concerns are not a basis for a denial of a conditional use application. Even evidence of a significant increase in traffic does not warrant denial of a conditional use application unless it is shown that the potential for a substantial increase would "by a high degree of probability, pose a substantial threat to the health and safety of the community" that is not typically expected from a similar development. In re Brickstone Realty Corp., 789 A.2d 333, 341 (Pa. Commw. Ct. 2001).

School District

(D) The proposed use's impact on the Radnor School District, including an estimate of new pupils generated by the proposed development.

*A Fiscal Impact Study prepared by Erik Hetzel, AICP/PP, LEED AP was submitted with the Conditional Use application and is attached to this report as **Appendix A**. This Fiscal Impact Study addresses the proposed use's impact on the Radnor School District. By way of further response, it is anticipated that an estimated 9 school-aged children will live in the proposed development, although all of them may not utilize the Radnor School District's public schools.*

Because the conditional use is contemplated and permitted by the ordinance, there is a presumption that the governing body considered the effect of the use when enacting the ordinance and determined that the use is consistent with the health, safety, and welfare of the community so long as it meets the objective requirements of the ordinance. Marr Dev. Mifflinville, LLC v. Mifflin Twp. Zoning Hearing Bd., 166 A.3d 479, 483 (Pa. Commw. Ct. 2017). Whether or not a proposed development adds school children to the local school district is not a factor that can be considered in the review of a conditional use application or a land development application.

Commercial Impact

(E) The proposed use's impact on nearby commercial facilities within the Township and surrounding municipalities.

*A Fiscal Impact Study prepared by Erik Hetzel, AICP/PP, LEED AP was submitted with the Conditional Use application and is attached to this report as **Appendix A**. This Fiscal Impact Study addresses the proposed use's economic impacts. By way of further response, the new residents of the proposed development will use goods and services in and around Radnor Township which will have a positive impact on nearby commercial facilities.*

Public Utilities

(F) The proposed use's impact on public utilities, including but not limited to sewage disposal, water supply, storm drainage, and electrical utilities.

It should be noted that the Tract and the immediate area is currently served by public utilities and the proposed density is permitted under Article XIX of the Zoning Ordinance, therefore these impacts are already considered under the Zoning Ordinance. No adverse impacts on public utilities are anticipated.

Police and Fire Protection

(G) The proposed use's impact upon the provision of police and fire protection.

*A Fiscal Impact Study prepared by Erik Hetzel, AICP/PP, LEED AP was submitted with the Conditional Use application and is attached to this report as **Appendix A**. This Fiscal Impact Study addresses the proposed use's impact on public safety services. It should be noted that the Tract is currently served by the police and fire department and the proposed density is permitted under Article XIX of the Zoning Ordinance, therefore these impacts are already considered under the Zoning Ordinance. The eventual homeowners of the Project will also pay taxes to Radnor Township which will increase Township revenues.*

Open Space and Recreation Facilities

(H) The proposed use's impact on the Township's open space and recreation facilities.

The Conditional Use Plans propose to designate at least 15% of the Tract as common open space. This proposed common open space will be preserved through the recordation of the approved plan and through a Planned Community Declaration. The proposed density is permitted under Article XIX of the Zoning Ordinance, therefore these impacts are already considered under the Zoning Ordinance. The Radnor Township Subdivision and Land Development Ordinance also requires recreational land dedication or that the Applicant pay a fee-in-lieu to address impacts on the Township's open space and recreation facilities.

Neighborhood Impact

(I) The proposed use's impact upon the character of the surrounding neighborhood. The applicant must show the proposed development will not adversely affect the surrounding neighborhood and what measures are proposed to mitigate any potential impacts.

The site is located in the R-4 Residence District and is in close proximity to one of the Township's primary commercial corridors along Lancaster Avenue. The Tract serves as a transitional area from the commercial areas along Lancaster Avenue to the residential areas to the north and northeast of the site.

The Project as a residential use is consistent with the residential uses in the area. Significant landscaping is proposed along the street frontages will help maintain the visual character along the public ways. The Project also provides a buffer along the residential property lines.

Fiscal Impacts

(J) An analysis of the proposed use's fiscal impacts upon the county, Township, and school district.

*A Fiscal Impact Study prepared by Erik Hetzel, AICP/PP, LEED AP was submitted with the Conditional Use application and is attached to this report as **Appendix A**. This Fiscal Impact Study addresses the proposed use's economic impacts. It should be noted that the proposed density is permitted under Article XIX of the Zoning Ordinance, therefore these impacts are already considered under the Zoning Ordinance.*

III. Required Documentation under Code §280-135.G for a Development Impact Statement (as applicable)

(A) A conditional use plan for the proposed development, identifying all proposed uses to be located on the site, and demonstrating compliance with the area, bulk and dimensional requirements for the proposed use. The conditional use plan shall be submitted in the form of a sketch plan containing the information required by § 255-19 of the Township Subdivision and Land Development Ordinance.

Conditional Use Plans for the proposed development were submitted with the Conditional Use application. The Conditional Use Plans for the proposed development exceeds the requirements of a sketch plan under Code §255-19.

(B) The applicant shall provide conceptual architectural renderings (perspectives and elevations) of the proposed development.

Conceptual architectural renderings, as they are envisioned at this time, were submitted with the Conditional Use application. It is noted that there are no objective standards in the Code for architecture, except as in Code §280-93. The Project complies with these standards.

(C) The location and size of the site, with evidence supporting the general adequacy for development.

Information regarding the size, location, and adequacy for development can be found in the Plans for the Project.

(D) The proposed residential density of the development and the percentage mix of the various dwelling types.

The proposed residential density is 5.5 dwelling units per acre and all of the proposed dwellings are townhouses. The proposed use and density are permitted in the R-4 District and are detailed on the Plans.

(E) The location, size, accessibility and proposed use of the open space, manner of ownership and maintenance, and a copy of the covenant to be incorporated in the individual deeds, if applicable.

The Conditional Use Plans propose to designate at least 15% of the Tract as common open space for use of the residents of the development. The ownership and maintenance of the open space will be through the homeowners association to be formed.

(F) Conceptual landscape plans showing locations of trees and shrubs and other landscape improvements (e.g., berms, fences) as necessary to mitigate the adverse visual impacts which the proposed actions will have on the property, adjoining properties and the Township in general. This shall include improvements to the streetscape adjacent to the property boundaries.

Conceptual landscape plans are included in the Conditional Use Plans, specifically on Sheets 12 and 13.

(G) Conceptual plans of proposed utility and drainage systems.

Conceptual plans of the proposed utility and drainage systems are included in the Conditional Use Plans, which were submitted with the Conditional Use application. The Post Construction Stormwater Details and Construction Details can be found on Sheets 7 and 8 of the Conditional Use Plans.

(H) A phasing plan describing how the proposed development will be implemented (if applicable).

The development is proposed to be constructed in one phase.

(I) Plans and renderings indicating the design, unity and aesthetic relationship of building and landscaping within the proposed development with that of the surrounding area.

The Plans show this information.

(j) The text of covenants, easements and existing restrictions or those to be imposed upon the land or structure, including provisions for public utilities, and trails for such activities as hiking or bicycling, if applicable.

A draft Planned Community Declaration was submitted with the Conditional Use application. This Declaration will be finalized at a later stage of the process.

APPENDIX A
FISCAL IMPACT STUDY

ERIK W. HETZEL, AICP/PP, LEED AP
16 MANOR ROAD
PAOLI, PA 19301
610.322.7154
erik@erikhetzel.com

Memorandum

To: D. Charles Houder, Haverford Properties Inc.

From: Erik Hetzel, AICP/PP, LEED AP

Date: September 17, 2020

Re: Fiscal Impact Analysis – Townhouse Residential Development, Radnor Township

On behalf of Trust U/A Dorrance H. Hamilton dated 3/15/96, Haverford Properties Inc. is proposing a residential development on the site of the Hamilton estate in Radnor Township, Delaware County. Forty-one (41) single-family attached townhome units are planned at the northwest corner of the intersection of Strafford Avenue and Eagle Road. This memorandum describes the anticipated future annual fiscal impacts related to the proposed development. A concise summary of fiscal impacts associated with the proposed development is presented in a table on the last page of this memorandum.

Fiscal impacts presented in this analysis were estimated using a methodology developed by the Rutgers University Center for Urban Policy Research, as originally described in The New Practitioner's Guide to Fiscal Analysis¹ and further developed in a later publication by the same authors entitled Development Impact Assessment Handbook². It is projected that, in total, the proposed development will result in beneficial, net-positive annual fiscal impacts to both Radnor Township and the Radnor Township School District.

Revenue Impacts

Real Estate Property Tax - At full build-out under the proposed concept, the development will have a total market value of approximately \$34.85 million, which translates to an assessed value of approximately \$17.425 million. This assessment

¹ Burchell, Robert W., David Listokin, et al. *The New Practitioner's Guide to Fiscal Impact Analysis*, New Brunswick, NJ: Rutgers, The State University of New Jersey, 1985.

² Burchell, Robert W., David Listokin, et al. *Development Impact Assessment Handbook*. Washington, D.C.: ULI-the Urban Land Institute, 1994.

calculation is based on the current (2020-2021) Delaware County common-level ratio of 2.00, which estimates assessed value at approximately 56.00% of market value. The proposed development will generate ongoing real estate tax revenue to the local taxing authorities, with an estimated \$76,813 going to the Township and \$445,486 going to the School District annually.

Real Estate Transfer Tax – The Township levies the Real Estate Transfer Tax at the rate of 1.0% of sale price, and the School District levies the Real Estate Transfer Tax at 0.5%. This analysis anticipates that an average of approximately 5% of the residential units in the proposed development will transfer ownership (be sold) in any given year, which equates to annual Transfer Tax Revenues totaling \$15,421 to the Township and \$7,711 to the School District. In addition to the annual Transfer Tax revenues estimated here, it is also important to note that the Township and School District will also receive significant Transfer Tax revenues from the initial sales of every residential unit from the builder to the first homeowners. With a total market value of \$34,850,000 in 2020, this equates to an estimated \$348,500 in initial-sales transfer taxes to the Township (\$34,850,000 market value x 1.0% transfer tax rate = \$348,500). For the School District, initial home sales would generate transfer taxes totaling \$174,250 (\$34,850,000 market value x 0.5% transfer tax rate = \$174,250).

Other Revenues - In addition to the aforementioned tax revenues, the proposed development is expected to generate approximately \$26,811 in Township revenues from other sources, as reported in the Radnor Township 2020 General Fund Budget. These sources include licenses & permits, fines & costs, and departmental earnings. Similarly, the School District will realize additional revenues totaling approximately \$44,999 in revenues from non-tax sources (LEA activities and rentals) and intergovernmental sources (State and Federal funding) as identified in the Radnor Township School District 2020-2021 Final General Fund Budget.

Population Impacts

An estimated 91 residents will be living in the proposed development, including 9 school-aged children. These population estimates are based on demographic multipliers published by the Rutgers University Center for Urban Policy Research (2006)³. This source uses information from the U.S. Census to derive population

³ Burchell, Robert W., David Listokin, et al. *Residential Demographic Multipliers (Pennsylvania)*. New Brunswick, New Jersey: Center for Urban Policy Research, Edward J. Bloustein School of Planning and Public Policy - Rutgers, the State University of New Jersey (2006).

multipliers specific to the Commonwealth of Pennsylvania, categorized by household structure type (i.e., single-family detached, single-family attached, apartments, etc.), and by the number of bedrooms per household.

Economic Impacts

The proposed development will have a beneficial economic “ripple effect” in the local economy, as the new residents and employees use goods and services in and around Radnor Township. In addition, the development phase of the project will provide construction jobs and result in construction-related consumption expenditures in the local and regional economies. These impacts are not reflected in the summary table included in this memorandum but will provide economic benefits over and above the annual revenues described therein.

Cost Analysis

Annual Township and School District expenditures attributable to the proposed development were projected using the Per Capita Multiplier Method described in the Development Impact Assessment Handbook. In calculating the per capita expenditure value for the Township, the methodology uses information from the current (2020) Township budget and accounts for the fact that costs are divided differently among serving both residential and non-residential portions of the Township, based on the actual mix of land uses provided by the County Board of Assessment. Overall, it is estimated that the proposed development will result in Township costs totaling approximately \$84,604 annually, which is more than offset by revenues totaling \$119,045 annually from the tax and non-tax sources described previously.

A similar per capita cost calculation is used for the Radnor Township School District. Based on current (2020-2021) enrollment and budget information provided by the School District, the estimated annual total cost-per-student in the Radnor Township School District used in this analysis is approximately \$24,082. With 9 school-aged children projected to reside in the proposed development, this results in projected annual costs of \$216,741 to the School District, which is more than offset by revenues from taxes and other sources totaling \$498,196. This estimate assumes that all 9 school-aged children will be attending public schools; however, it is likely that some will attend private schools, which would result in lower costs to the Radnor Township School District than estimated here, and a higher net-positive fiscal impact.

Impacts to Public Safety Services and Facilities

Overall, impacts on public safety services and facilities from the proposed development are expected to be minimal. Demands for Police, Fire, and Emergency Management Services (EMS) personnel, vehicles, and facilities are presented in the following table, using planning standards published in the Development Impact Assessment Handbook. Personnel, vehicles, and facilities for Police and Fire are based on the noted planning standards per 1,000 population. EMS standards for personnel and vehicles are per 30,000 population, and EMS calls are per 1,000 population per year.

**Public Safety Demand Factors and
Projected Demand from Proposed Development**

	Residential Planning Standard	Projected Demand
<u>Police</u>		
Personnel	1.50	0.14
Vehicles	0.45	0.04
Facilities (square feet)	150	14
<u>Fire</u>		
Personnel	1.24	0.11
Vehicles	0.15	0.01
Facilities (square feet)	187.5	17
<u>EMS</u>		
Personnel	3.08	0.01
Vehicles	0.75	0.00
Calls per year per 1,000 population	27.4	2

Conclusions and Summary

In conclusion, the net positive fiscal impact in terms of projected revenues over costs for Radnor Township is expected to be over \$34,000 annually at project completion. The projected net positive fiscal impact to the Radnor Township School District is projected to be over \$280,000 per year. The combined net positive fiscal impact for both taxing authorities is estimated at over \$300,000 annually. Not included in these annual estimates are significant real estate transfer fees that will be paid on the initial sales of every unit from the builder to the first homeowners. The table on the next page summarizes the fiscal impacts to the Township and School District.

Fiscal Impact Summary

	Townhomes
Residential Units	41
Value per Unit	\$850,000
Market Value of Proposed Development	\$34,850,000
Assessed Value (approx. 50% of market value)	\$17,425,000
New Residential Population	91
New School-Aged Children	9
Radnor Township	
Real Estate Tax Revenue (4.4082 mills)	\$ 76,813
Non-Property Tax Revenue	\$ 26,811
Real Estate Transfer Tax (0.5%)	\$ 15,421
Total Township Revenues	\$ 119,045
Total Township Expenditures	\$ (84,604)
Net Township Fiscal Impact	\$ 34,441
Radnor Township School District	
Real Estate Tax Revenue (25.5659 mills)	\$ 445,486
Non-Property Tax Revenue	\$ 4,428
Intergovernmental Revenue	\$ 40,571
Real Estate Transfer Tax (0.5%)	\$ 7,711
Total School District Revenues	\$ 498,196
Total School District Expenditures	\$ (216,741)
Net School District Fiscal Impact	\$ 281,455
Total Development-Generated Revenues (Township + School District)	\$ 617,241
Total Development-Generated Expenditures (Township + School District)	\$ (301,345)
Total Net Annual Fiscal Impact	\$ 315,895

APPENDIX B
TRAFFIC IMPACT STUDY



F. Tavani and Associates, Inc.
Traffic Engineering and Planning

248 Beech Hill Road • Wynnewood • PA • 19096 • (215) 625-3821 Phone • (484) 792-9495 Fax
www.FTAVANIASSOCIATES.COM

25 August 2020

Charlie Houser
Haverford Properties
551 W. Lancaster Avenue, SU 307
Haverford, PA 19041

VIA EMAIL ONLY

**RE: Traffic Engineering Investigations of
Strafford Ave 41-unit Residential TH Site
Radnor Township, Delaware County, PA**

FTA Job #219-011

Dear Mr. Houser:

F. Tavani and Associates, Inc. (FTA) has conducted traffic engineering investigations for the above-referenced project in Strafford. This report has been prepared in accordance with Radnor code requirements and follows the recommended outline as identified in said ordinance.

GENERAL SITE DESCRIPTION

This study considers the traffic impact of a proposed townhouse community of 41 units. The housing is proposed to be for sale and will feature a mix of 3 and 4 bedrooms. The housing is proposed to be market-rate and not age-restricted. The process of entitlements, construction, and occupancy is expected to take 3-5 years. The site is immediately surrounded by other residential properties and the Eagle Village Shopping Center. Beyond them, there is a mix of office and retail buildings within a 1 mile radius of the site. Ample mass transit opportunities are also within a short distance of the site.

The site is located on the west side of Strafford Avenue, north of Eagle Road and is known as the Hamilton Estate. The site is presently developed with some existing housing, namely 6 total dwellings.

The site location and surrounding area are presented in figures which are attached to the end of this report, namely **Figure 1** and **Figure 2**. A reduced version of recent site plans for the project is featured in **Figure 3**. There are no other known approved land development projects in the vicinity of the site.

Note that technical appendices are provided following the figures. **Appendix A** is reserved for future project correspondence. Photodocumentation of the study area / surrounding intersections is provided in **Appendix B**.

TRANSPORTATION FACILITIES DESCRIPTION

The site is surrounded on two sides by existing, two-way, one-lane-per-direction, public roadways, namely Strafford Avenue and Eagle Road. The roadways generally do not feature on-street public parking. Posted speed limit signs are present in the vicinity of the site along both Strafford Avenue and Eagle Road, where the posted speed limit is 25 mph. There are limited sidewalk facilities in the study

area. The major intersections closest to the site are all-way stop-controlled intersections with no painted crosswalks. There are existing SEPTA mass transit opportunities near the site including bus route 106 and a regional rail station (Strafford), each of which are within approximately one half mile of the site. No traffic signals (save for a flashing beacon at the all-way stop-controlled intersection of Strafford Avenue and Eagle Road) exist or are proposed in the immediate vicinity of the site. More site driveway and surrounding intersection details can be seen in photodocumentation log as provided in **Appendix B**.

The site has 41 units and is proposed to feature internal roadways, 2 site driveways (both on Strafford Avenue), garage/driveway parking, and visitor parking (approximately 11 defined spaces). Sidewalks are also proposed.

There are no known planned roadway improvements in the vicinity of the site. None of the streets surrounding the site are “SR”s (state roadways) – instead they are all local roadways. Eagle Road is a “G” roadway, meaning it is not an SR but is eligible for liquid fuels funding and PennDOT does maintain traffic count data along it, as seen in **Appendix C**.

EXISTING TRAFFIC CONDITIONS

FTA conducted traffic counts at the intersections of:

- Strafford Avenue and Eagle Road,
- Strafford Avenue and Grant Lane/Hedgerow Lane, and
- Eagle Road and N Wayne Avenue.

The counts were conducted on Thursday, 16 May 2019 from 7:00 AM to 9:00 AM and from 4:00 PM to 6:00 PM. The counts were conducted during the school year, in fair weather, and on a typical weekday. Existing peak hours of 8:00 AM to 9:00 AM and 4:30 PM to 5:30 PM were selected for study based on a system-wide peak hour investigation. The corresponding existing peak hour traffic volumes are plotted and seen in **Figure 4**. Raw traffic volumes are attached in **Appendix D**, as is a spreadsheet which describes the system peak investigation.

With existing peak hour volumes established, present-day “levels of service” can be assessed. Level of service (or LOS) is a descriptive mechanism which is employed by traffic engineers to relate quality of traffic flow to both a letter grade and estimate of delay in seconds per vehicle. LOS results are assessed for traffic which must stop or yield to other traffic. Free-flowing traffic theoretically has no delay, and therefore no LOS rating. Existing levels of service were determined using *Synchro version 10* software, with HCS2010-format outputs selecting for performance reporting purposes. A **LOS Comparison Matrix** was prepared and is attached to the end of this report. The matrix summarizes AM and PM peak hour performance for existing and future (see next section) conditions for all intersections. As shown, existing levels of service are all LOS A and B, with all calculated delays being very low (10 seconds or less in most cases – an acceptable condition). No congestion locations (LOS E/F) are noted.

TRANSPORTATION IMPACT OF THE DEVELOPMENT

Site traffic was estimated using the Institute of Transportation Engineers (ITE) publication, Trip Generation, 10th edition. ITE website trip generation outputs are attached and provided in **Appendix E**. Raw trip generation could have been modified to reflect how this site is located in a setting which is within walking distance of several businesses as well as SEPTA bus route 106 plus the Strafford train station, though **no such multimodal credits were taken**. Instead, *all* site traffic was assigned (trip distributed) to the surrounding roadway network in accordance with existing traffic patterns as well as an understanding of

existing road network connectivity, current traffic/congestion patterns, and relative locations of major highway interchanges (Interstates 476, 76, 202, and 422 as well as Business Route 30). The assignments are summarized as follows:

- 30% to/from Routes 202 & 422 via Strafford Ave to Old Eagle School Rd;
- 30% to/from Routes 476 & 76 via Eagle Rd to King of Prussia Rd;
- 15% to/from Business Rt 30 West via Eagle Rd and Strafford Ave;
- 15% to/from Business Rt 30 East via Eagle Rd and Strafford Ave, West Ave., and/or Banbury Way; &
- 10% to/from Conestoga Road via Eagle Road.

The trip distribution model for the community is shown in **Figure 5** and the resultant assignment of new, site-generated, vehicular peak hour traffic is shown in **Figure 6**. A site trip generation summary table follows below. Note that a credit for the previously-mentioned 6 existing dwelling units was applied to the trip generation (net new 35 townhomes)

TABLE 1
PROJECTED VEHICULAR TRIP GENERATION

AM PEAK HOUR			PM PEAK HOUR		
<u>IN</u>	<u>OUT</u>	<u>TOTAL</u>	<u>IN</u>	<u>OUT</u>	<u>TOTAL</u>
4	14	18	14	9	23

Average daily site traffic was also calculated and determined to be approximately 250 trips for the proposed community. See **Appendix E** for more details.

ANALYSIS OF TRANSPORTATION IMPACT

Future traffic conditions are a function of three components: (1) existing traffic volumes, (2) additional traffic due to general background growth as well as other known approved developments in the immediate proximity of the site, and (3) site traffic.

As mentioned earlier, there are no other known approved land development projects in the vicinity of the site. Regarding background growth, the currently promulgated background growth rate for Delaware County is 0.00% per year as reported by PennDOT. This means that future 'no build' traffic volumes and levels of service are identical to existing traffic volumes and levels of service.

The projected future 'build' (no build plus site traffic) peak hour volumes are shown in **Figure 7**. The related projected levels of service are once shown in **LOS Comparison Matrix**. As shown projected 'build' levels of service once again remain essentially the same as they are today, and are all LOS B or better. The impact of site traffic is no added delay at all intersections/turning movements (i.e, the impact of site traffic never amounts to *any* added delay at *any* impacted turning movement), and this again is while taking no credits for multi-modalism. Even with this conservative approach, no congestion locations (LOS E/F) are noted.

No road improvements are necessary to offset the impact of added site traffic. No proposed site driveway will feature traffic volumes which warrant the installation of a traffic signal. The acceptable operation of each site driveway (LOS A and B) in unsignalized state underscores this conclusion. Level of service worksheets are provided in **Appendix F**.

AUXILIARY LANE ANALYSIS

The need for new auxiliary left- and right-turn lanes at the site driveways was investigated. Investigations were based on PennDOT Strike Off Letter 560-08-4 as well as PennDOT *Publication 46* Chapter 11 page 11-46 ("Turn Lane Warrants") using PennDOT-provided worksheets, and focusing on the highest peak hour. Investigations conclude that new auxiliary left- and right-turn lanes are not warranted at the site driveways. More details are provided in **Appendix G**.

CONCLUSIONS

As mentioned earlier, a **LOS Comparison Matrix** is provided to afford a simple means to review and assess site traffic impact in the study area. In locations where levels of service are not forecasted to change from one scenario to the next (i.e., from Existing to No Build, or from No Build to Build), hyphens are used. As shown, there are many instances in which the impact of site traffic results in essentially no measurable change in traffic performance and the underlying traffic performance is already acceptable, and with very low delays.

Other key conclusions are as follows:

- The study area is presently well-served by transit opportunities.
- There are no streets or intersections operating below LOS C under existing or future conditions.
- Both site driveways are forecasted to operate at LOS A/B during both peak hours, and for all turning movements.
- No site driveway requires new left-turn or right-turn auxiliary lanes per investigations using standard PennDOT tools.
- The foregoing conclusions were reached taking no credits for walking or transit, even though at least some of either/both are likely.

I hope this has been helpful. Please let me know if I can answer any questions.

Thank you,

F. TAVANI AND ASSOCIATES

FRANK TAVANI, P.E., PTOE
Principal



attachments

cc: George Broseman, Esq.
Rob Lambert, P.E.

**LEVEL OF SERVICE AND EXPECTED DELAY
FOR UNSIGNALIZED INTERSECTIONS***

<u>LEVEL OF SERVICE</u>	<u>CONTROL DELAY PER VEHICLE (SECONDS)</u>
a	0 to 10.0
b	10.1 to 15.0
c	15.1 to 25.0
d	25.1 to 35.0
e	35.1 to 50.0
f	Over 50.0

* Transportation Research Board's Highway Capacity Manual

**LEVEL OF SERVICE AND EXPECTED DELAY
FOR SIGNALIZED INTERSECTIONS***

<u>LEVEL OF SERVICE</u>	<u>DESCRIPTION</u>	<u>CONTROL DELAY PER VEHICLE (IN SECONDS)</u>
A	Very short delay, good progression; most vehicles do not stop at intersection.	≤ 10.0
B	Generally good signal progression and/or short cycle length; more vehicles stop at intersection than Level of Service A.	10.1 to 20.0
C	Fair progression and/or longer cycle length; significant number of vehicles stop at intersection.	20.1 to 35.0
D	Congestion becomes noticeable; individual cycle failures; longer delays from unfavorable progression, long cycle length, or high volume/capacity ratios; most vehicles stop at intersection.	35.1 to 55.0
E	Usually considered <u>limit of acceptable delay</u> indication of poor progression, long cycle length, or high volume/capacity ratio; frequent individual cycle failures.	55.1 to 80.0
F	Could be considered excessive delay in some areas, frequently an indication of saturation (i.e., arrival flow exceeds capacity), or very long cycle lengths with minimal side street "green" time. Capacity is not necessarily exceeded under this level of service.	> 80.0

* Transportation Research Board's Highway Capacity Manual

LEVEL OF SERVICE COMPARISON TABLES

1. Stafford Ave & Grant Ln / Hedgerow Ln							
Direction	Movement	AM Peak Hour			PM Peak Hour		
Stafford Ave		Existing (2019)	No Build (2024)	Build (2024)	Existing (2019)	No Build (2024)	Build (2024)
Eastbound	LTR	A 8	--	--	A 9	--	--
Westbound	LTR	A 9	--	--	A 8	--	A 9
Grant Ln / Hedgerow Ln							
Northbound	LTR	A 8	--	--	A 7	--	--
Southbound	LTR	A 8	--	--	A 7	--	--
OVERALL:		A 9	--	--	A 9	--	--

Control
Type:
AWSC

2. Stafford Ave & Eagle Ave							
Direction	Movement	AM Peak Hour			PM Peak Hour		
Stafford Ave		Existing (2019)	No Build (2024)	Build (2024)	Existing (2019)	No Build (2024)	Build (2024)
Eastbound	LTR	A 10	--	--	B 11	--	--
Westbound	LTR	A 9	--	--	B 10	--	--
Eagle Ave							
Northbound	LTR	A 9	--	--	B 11	--	--
Southbound	LTR	A 10	--	--	B 11	--	--
OVERALL:		A 9	--	A 10	B 11	--	--

Control
Type:
AWSC

3. N Wayne Ave & Eagle Ave							
Direction	Movement	AM Peak Hour			PM Peak Hour		
N Wayne Ave		Existing (2019)	No Build (2024)	Build (2024)	Existing (2019)	No Build (2024)	Build (2024)
Eastbound	LTR	A 4	--	--	A 5	--	--
Westbound	LTR	A 3	--	--	A 5	--	--
Eagle Ave							
Northbound	LTR	B 20	--	--	B 20	--	--
Southbound	LTR	B 19	--	--	B 20	--	--
OVERALL:		A 8	--	--	B 10	--	--

Control
Type:
Signal

4. Strafford Ave & TH Site Drive N							
Direction	Movement	AM Peak Hour			PM Peak Hour		
TH Site Drive		Existing (2019)	No Build (2024)	Build (2024)	Existing (2019)	No Build (2024)	Build (2024)
Eastbound	LR			A 9			B 10
Eagle Ave							
Northbound	L			A 9			A 9
OVERALL:				A 1			A 1

Control
Type:
TWSC

5. Strafford Ave & SFDU Site Drive S							
Direction	Movement	AM Peak Hour			PM Peak Hour		
SFDU Site Drive		Existing (2019)	No Build (2024)	Build (2024)	Existing (2019)	No Build (2024)	Build (2024)
Eastbound	LR			A 10			A 10
Eagle Ave							
Northbound	L			A 9			A 9
OVERALL:				A 1			A 1

Control
Type:
TWSC

Future No Build volumes are identical to Existing volumes, so LOS are also identical

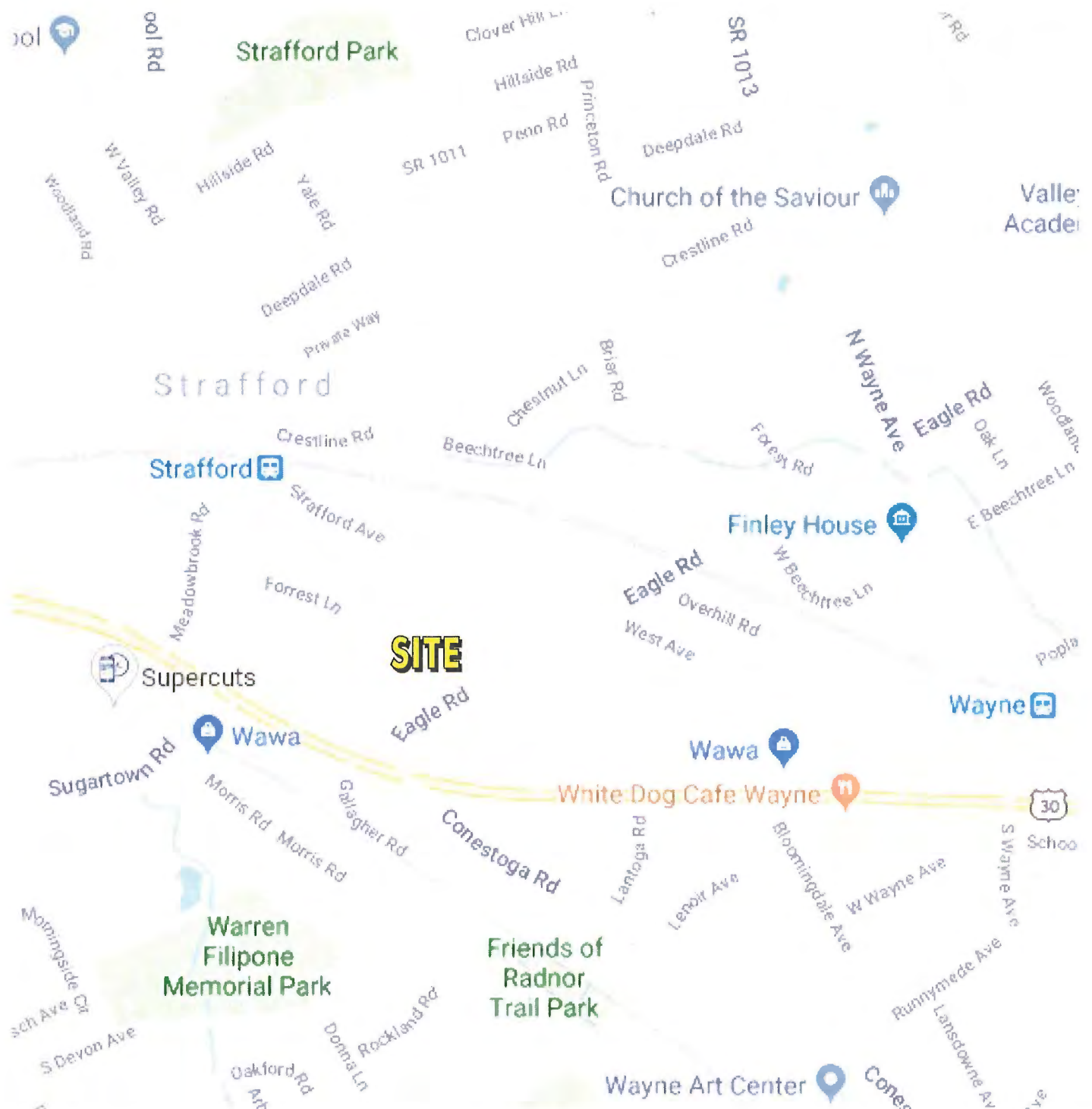
-- indicates no change from the previous scenario



Site and Surrounding Area – Map View

Strafford Avenue Residential - Townhouses
Radnor Township,
Delaware County, Pennsylvania

April 2020*



* Figure preparation date. See report for data collection date(s).

Site and Surrounding Area – Aerial View

Strafford Avenue Residential - Townhouses

Radnor Township,

Delaware County, Pennsylvania

April 2020

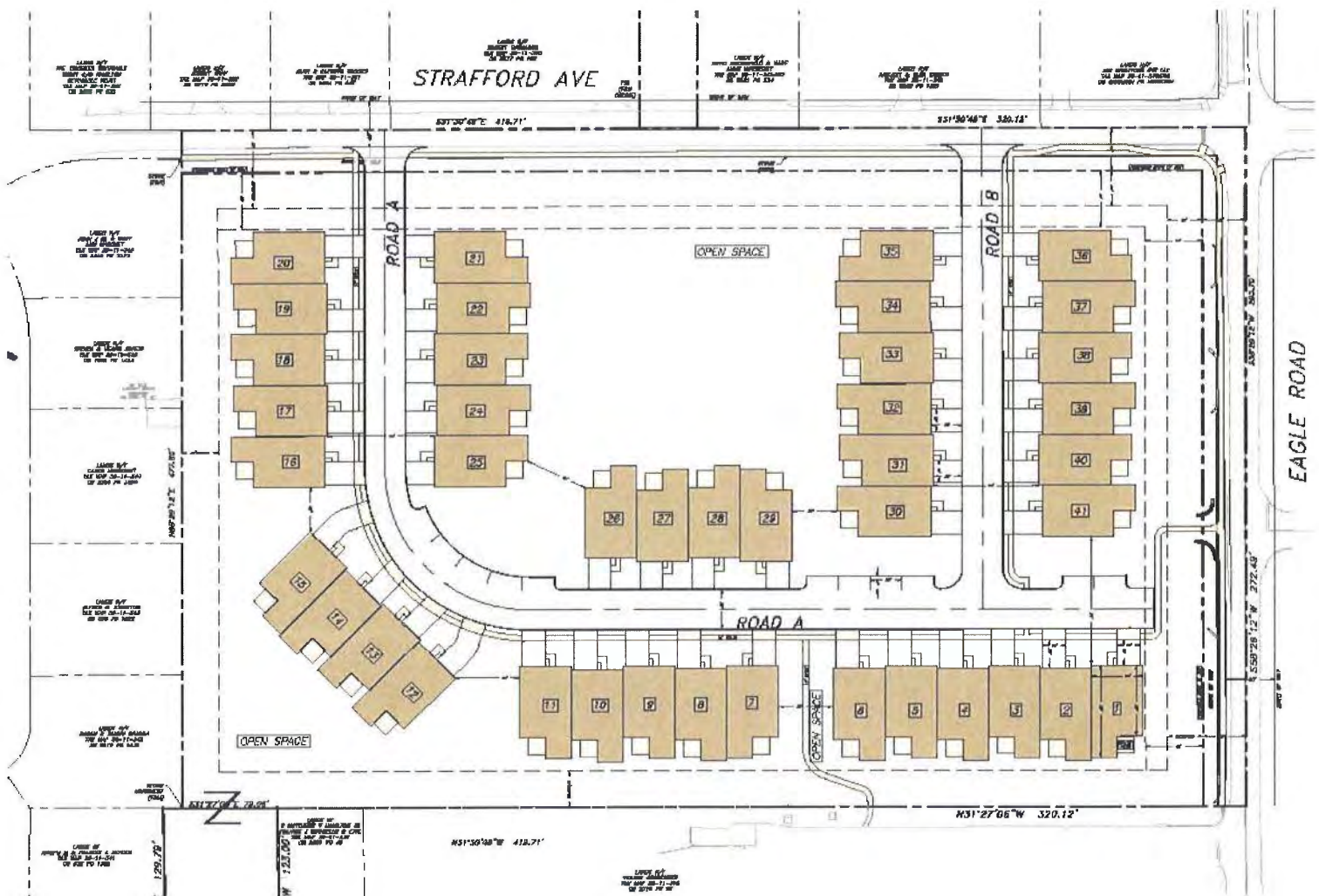




Site Plan Excerpt

Strafford Avenue Residential - Townhouses
Radnor Township,
Delaware County, Pennsylvania

August 2020





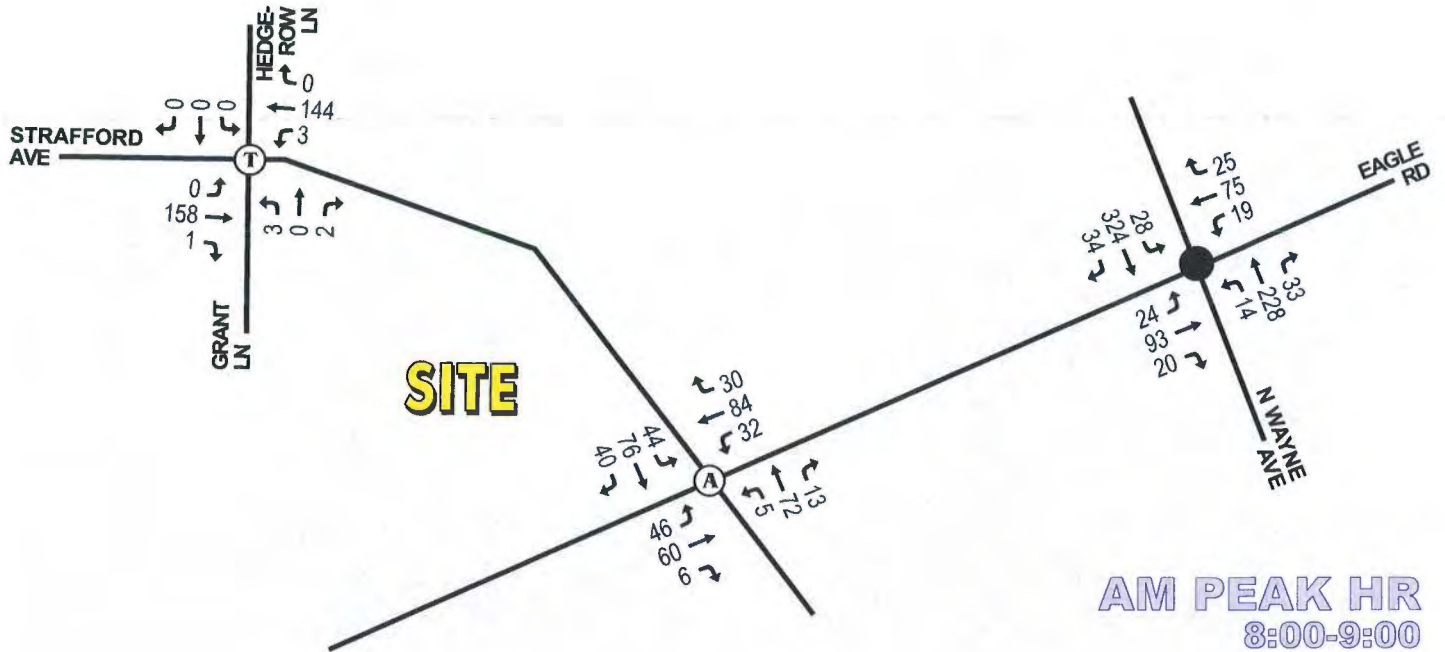
Existing (2019) Peak Hour Traffic Volumes

Strafford Avenue Residential - Townhouses

Radnor Township,

Delaware County, Pennsylvania

April 2020



Signalized Intersection



Unsignalized Intersection,
All-Way Stop Control



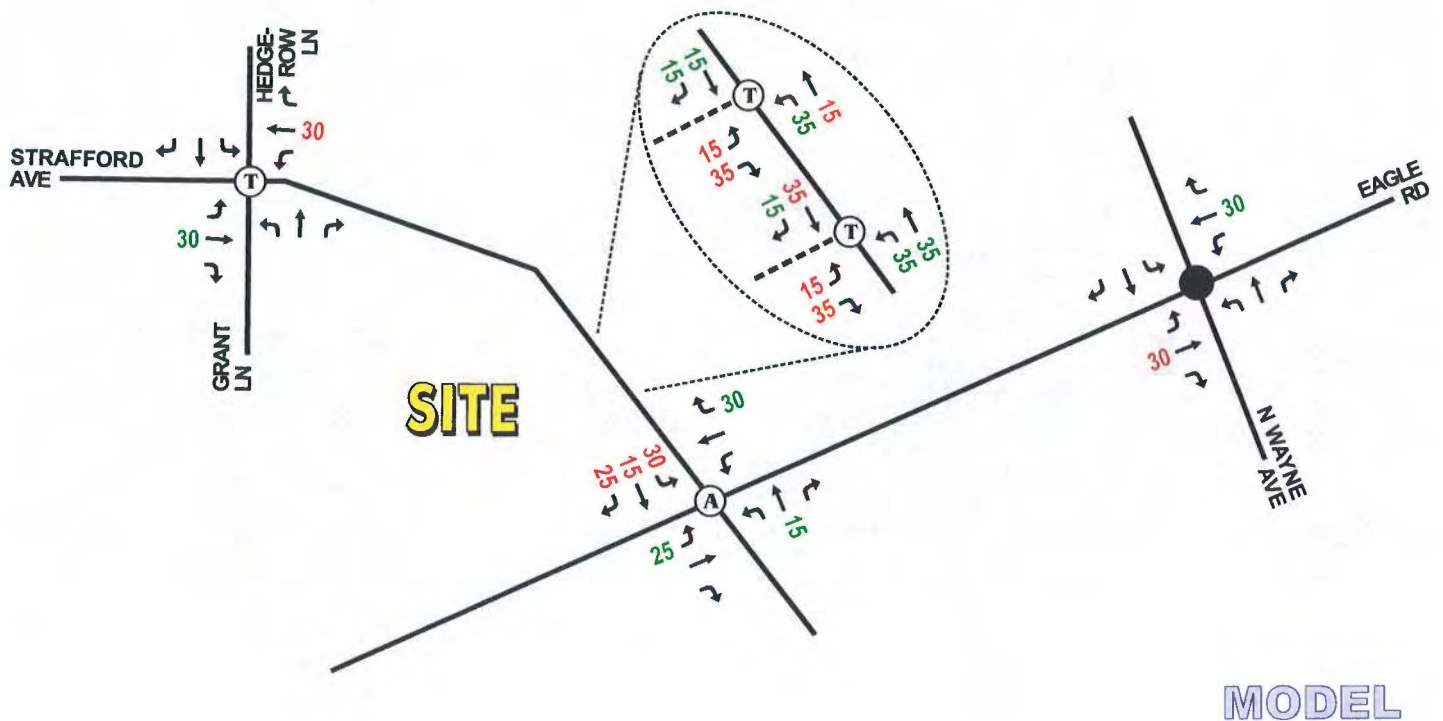
Unsignalized Intersection,
Two-Way Stop Control (Side Street Only)

INBOUND
OUTBOUND

Site Peak Hour Traffic – Model

Strafford Avenue Residential - Townhouses
Radnor Township,
Delaware County, Pennsylvania

April 2020



MODEL



Signalized Intersection



Unsignalized Intersection,
All-Way Stop Control



Unsignalized Intersection,
Two-Way Stop Control (Side Street Only)

INBOUND
OUTBOUND



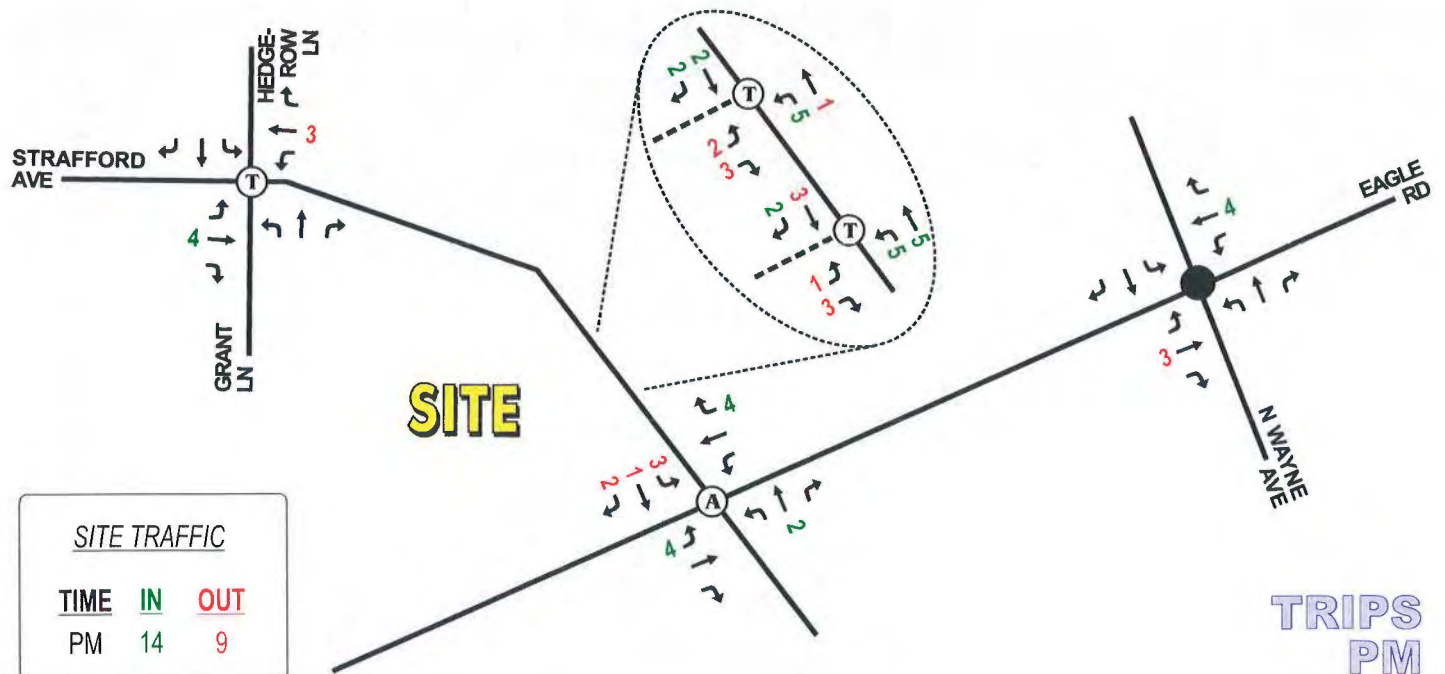
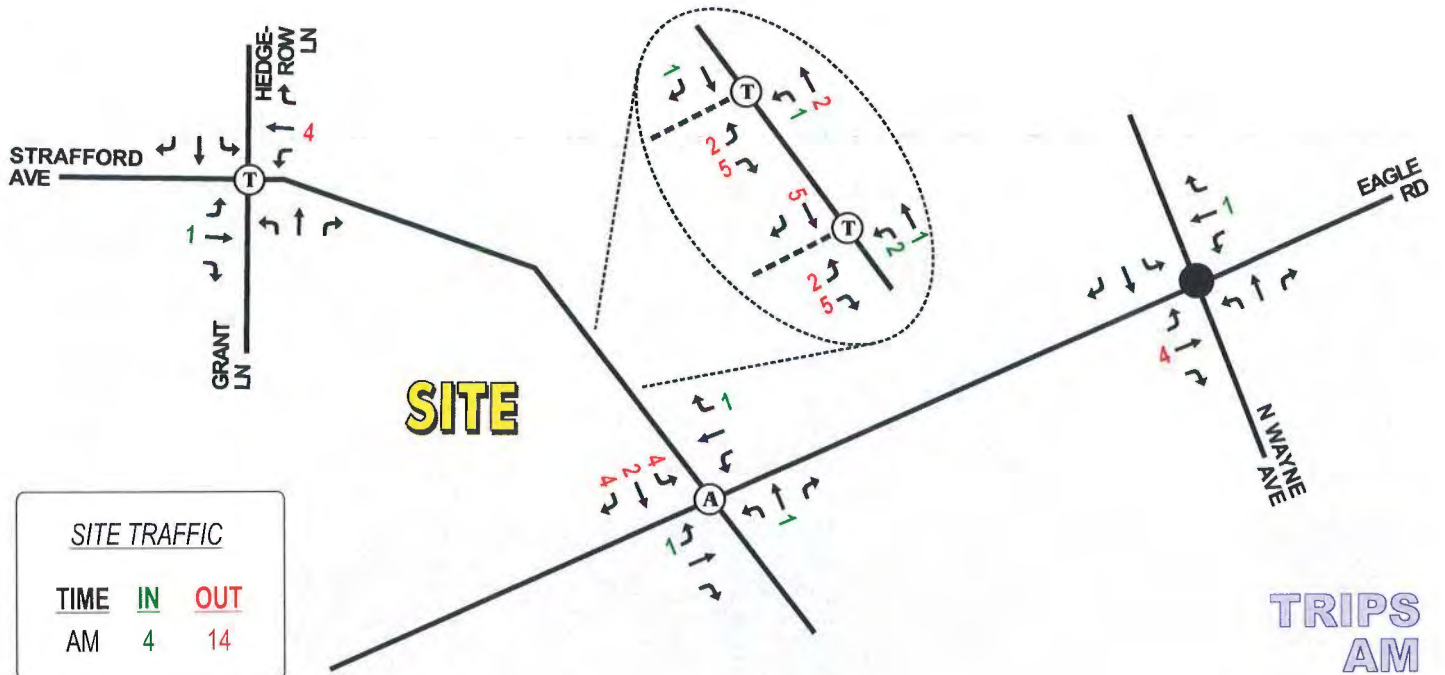
Site Peak Hour Traffic – Volumes

Strafford Avenue Residential - Townhouses

Radnor Township,

Delaware County, Pennsylvania

April 2020



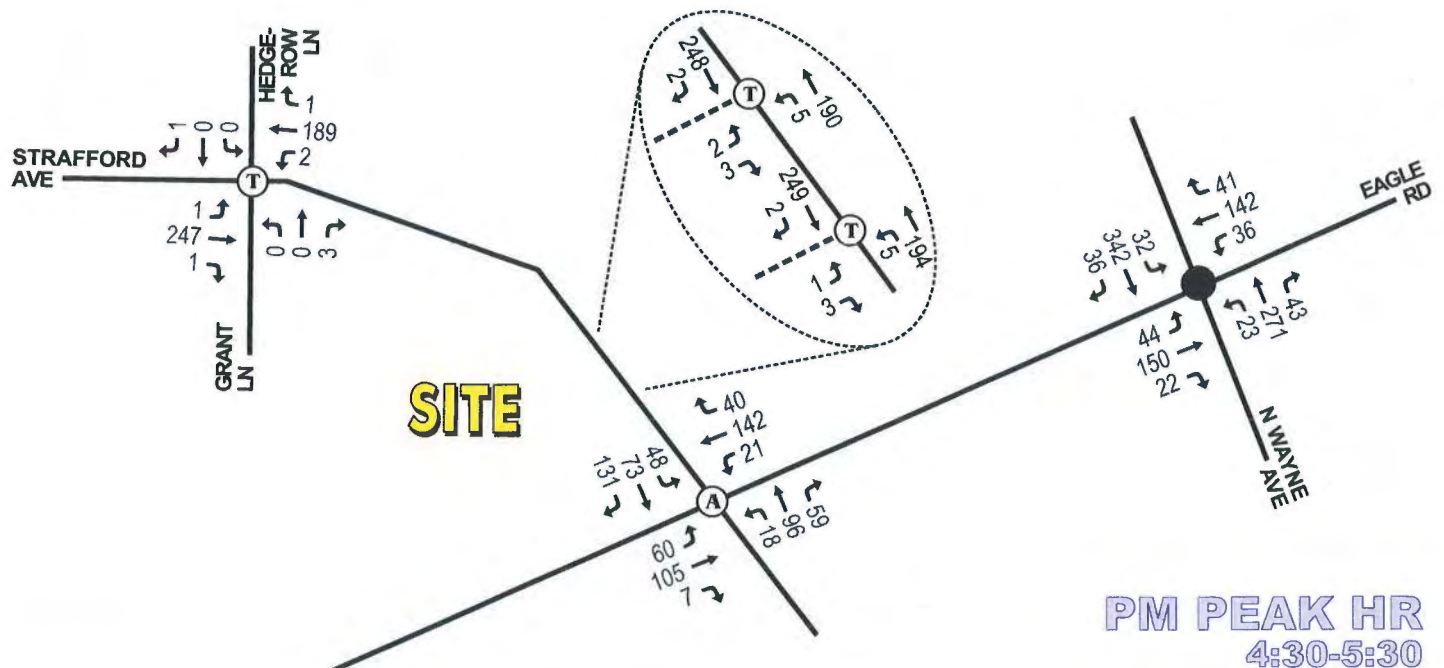
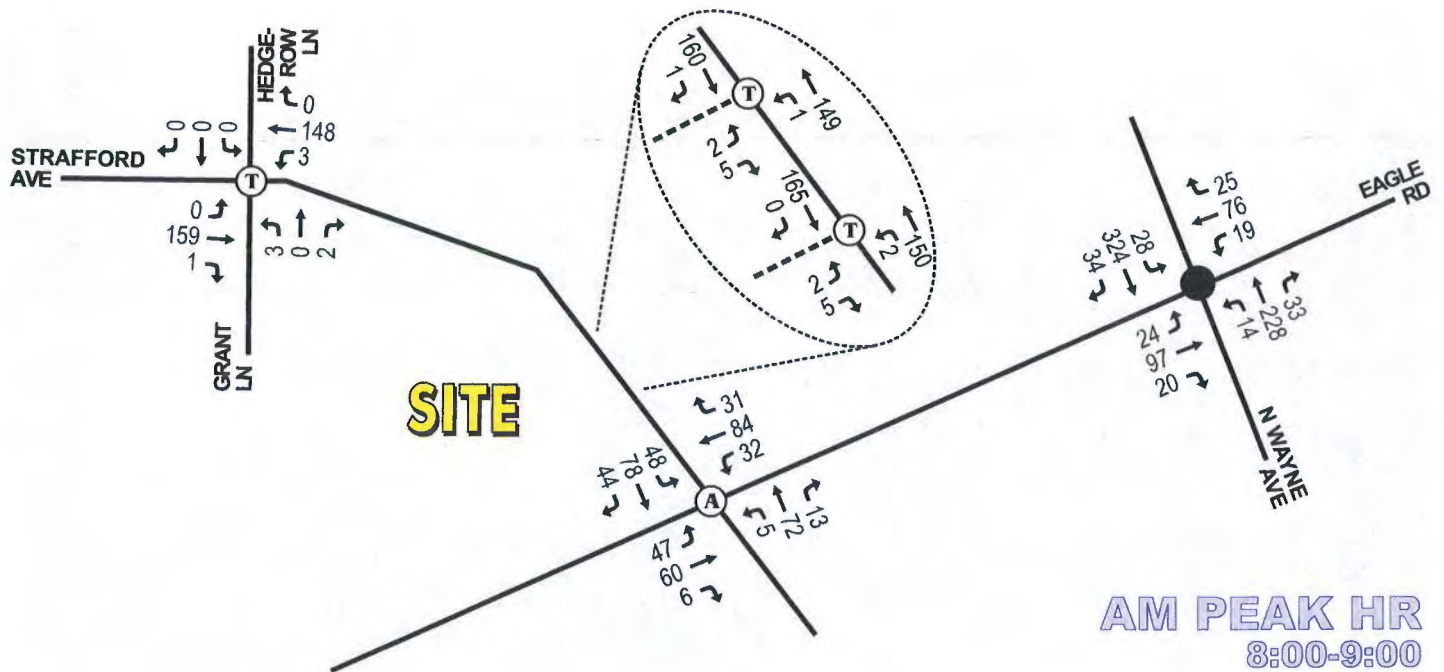
Future (2024) Build Peak Hour Traffic Volumes

Strafford Avenue Residential - Townhouses

Radnor Township,

Delaware County, Pennsylvania

April 2020



APPENDIX A

Correspondence

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APPENDIX B

Photodocumentation

Job #: 219-011

Road name (# of pages)

1. Eagle Road & Strafford Road (3)
2. Strafford Avenue & Grant Lane/Hedgerow Lane (3)
3. Eagle Road & Wayne Avenue (3)



Aerial image of intersection



Photo # 1 - Description: Eastbound Strafford Road



Photo # 2 - Description: Westbound Strafford Road



Photo # 3 - Description: Northbound Eagle Road

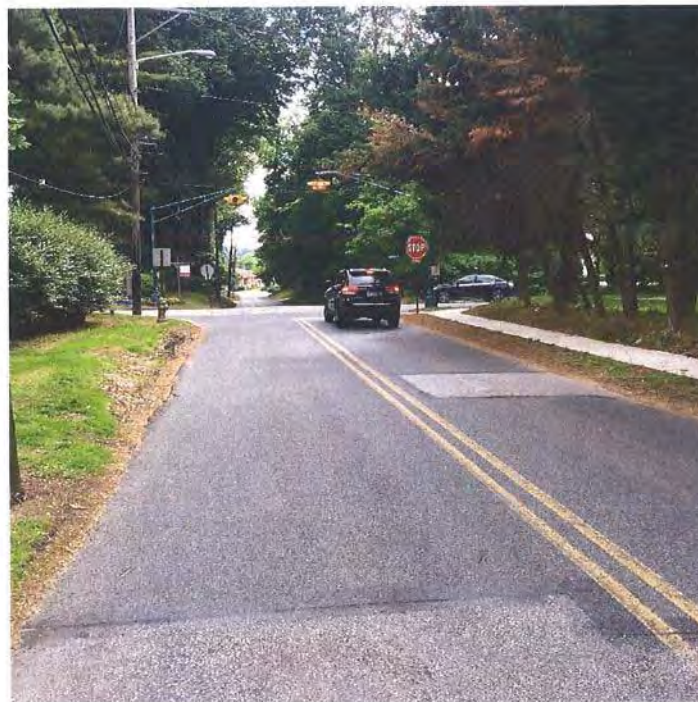
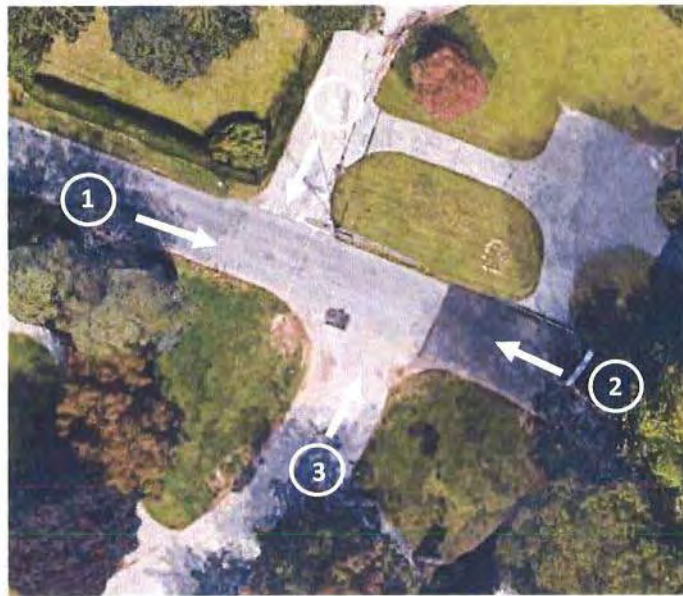


Photo # 4 - Description: Southbound Eagle Road



Aerial image of intersection



Photo # 1 - Description: Eastbound Strafford Road



Photo # 2 - Description: Westbound Strafford Road



Photo # 3 - Description: Northbound Grant Lane



Photo # 4 - Description: Southbound Hedgerow Lane



Aerial image of intersection



Photo # 1 - Description: Eastbound Wayne Avenue

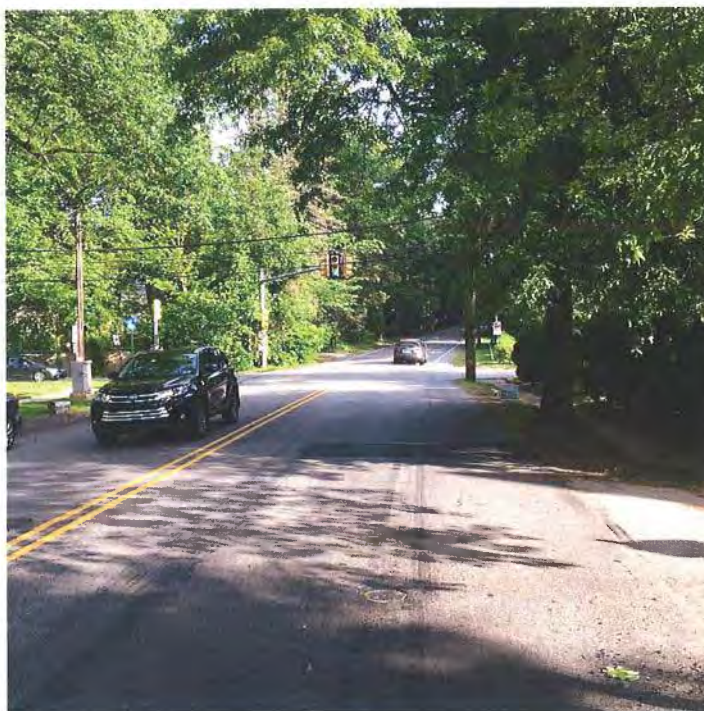


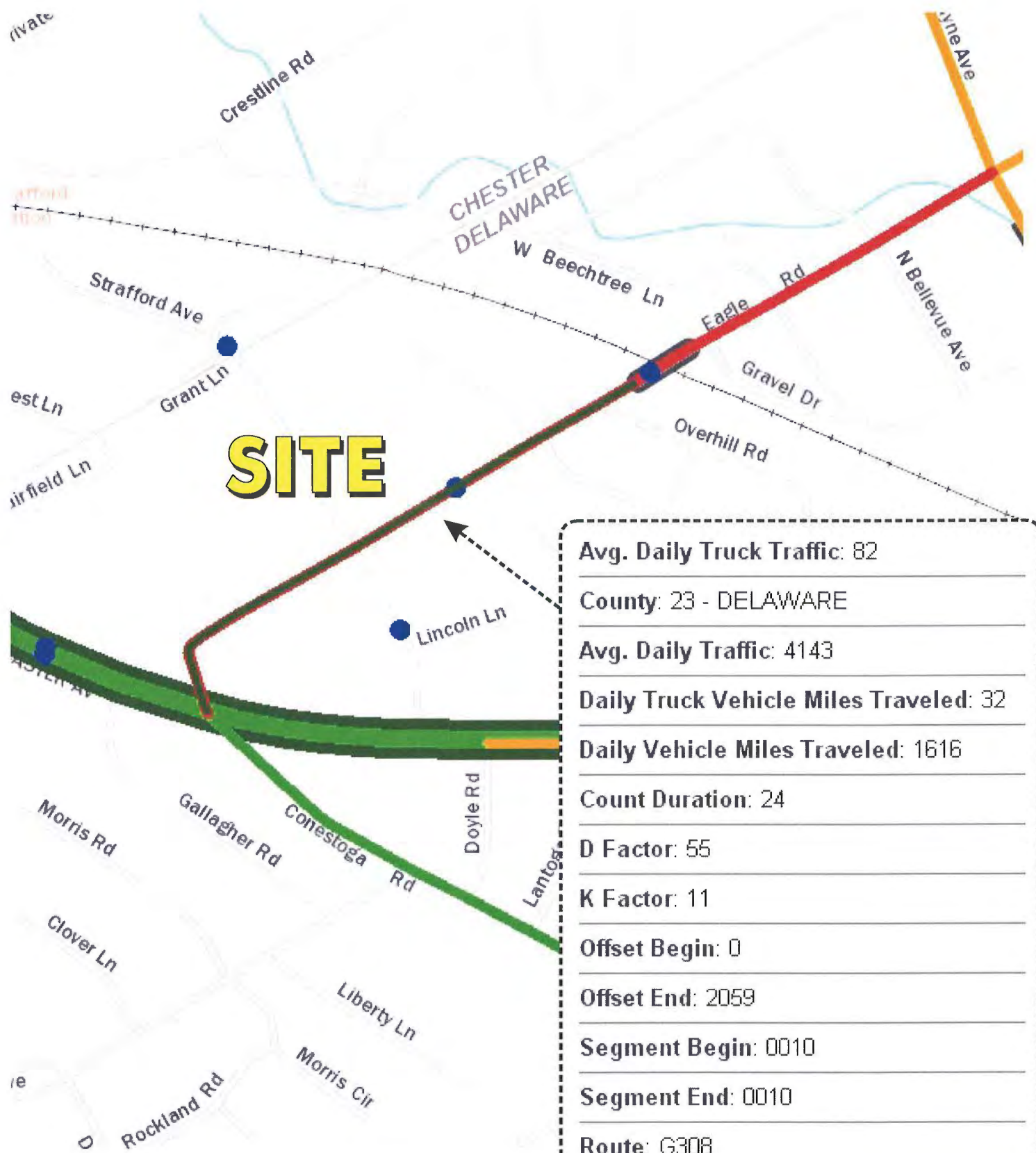
Photo # 2 - Description: Westbound Wayne Avenue



Photo # 3 - Description: Northbound Eagle Road



Photo # 4 - Description: Southbound Eagle Road



Avg. Daily Truck Traffic: 82

County: 23 - DELAWARE

Avg. Daily Traffic: 4143

Daily Truck Vehicle Miles Traveled: 32

Daily Vehicle Miles Traveled: 1616

Count Duration: 24

D Factor: 55

K Factor: 11

Offset Begin: 0

Offset End: 2059

Segment Begin: 0010







Segment End: 0010

Route: G308

Traffic Pattern Group: 05 - URBAN - MINOR ARTERIALS, COLLECTORS, LOCAL ROADS

Truck Percent: 2



-  INTERSTATE HIGHWAYS (1, 11)
-  OTHER FREEWAYS AND EXPRESSWAYS (12)
-  OTHER PRINCIPAL ARTERIAL HIGHWAYS (2, 14)
-  MINOR ARTERIALS (6, 16)
-  URBAN COLLECTOR OR RURAL MAJOR COLLECTOR (7, 17)
-  RURAL MINOR COLLECTOR (8)

APPENDIX E

Trip Generation, Background Growth & Other Developments

Growth Factors for August 2018 to July 2019				
County	Urban Interstate	Rural Interstate	Urban Non-Interstate	Rural Non-Interstate
ADAMS	*	*	0.98	0.75
ALLEGHENY	0.86	2.18	0.00	0.38
ARMSTRONG	0.85	*	0.00	0.38
BEAVER	0.80	1.96	0.00	0.35
BEDFORD	*	2.13	0.00	0.44
BERKS	1.16	2.43	0.26	0.58
BLAIR	0.81	1.94	0.00	0.37
BRADFORD	1.14	*	0.07	0.51
BUCKS	1.36	2.33	0.60	0.61
BUTLER	1.79	2.75	0.71	0.76
CAMBRIA	0.40	*	0.00	0.19
CAMERON	*	*	*	0.16
CARBON	1.35	2.60	0.38	0.64
CENTRE	1.53	2.55	0.70	0.69
CHESTER	1.74	3.02	0.58	0.82
CLARION	0.96	2.02	0.00	0.41
CLEARFIELD	0.99	2.09	0.01	0.44
CLINTON	0.95	2.26	0.00	0.47
COLUMBIA	1.19	2.29	0.35	0.56
CRAWFORD	0.95	2.00	0.09	0.44
CUMBERLAND	1.58	2.56	0.80	0.70
DAUPHIN	1.37	*	0.47	0.64
DELAWARE	0.99	*	0.00	*
ELK	*	*	0.00	0.31
ERIE	1.00	2.16	0.00	0.45
FAYETTE	0.84	*	0.00	0.40
FOREST	*	*	*	0.67
FRANKLIN	1.36	2.57	0.53	0.66
FULTON	*	2.13	*	0.52
GREENE	1.23	2.63	0.00	0.57
HUNTINGDON	*	1.96	0.00	0.39
INDIANA	1.22	*	0.17	0.53
JEFFERSON	*	2.13	0.00	0.44
JUNIATA	*	*	*	0.57
LACKAWANNA	0.85	2.30	0.00	0.44
LANCASTER	1.79	2.67	1.14	0.80
LAWRENCE	0.80	2.09	0.00	0.37
LEBANON	*	2.48	0.45	0.62
LEHIGH	1.58	2.88	0.48	0.74
LUZERNE	0.77	2.17	0.00	0.40
LYCOMING	1.02	2.18	0.04	0.47
MCKEAN	0.66	*	0.00	0.34
MERCER	0.69	1.99	0.00	0.35
MIFFLIN	0.80	*	0.00	0.39
MONROE	1.44	2.49	0.73	0.68
MONTGOMERY	1.21	*	0.34	0.58
MONTOUR	1.53	2.64	0.34	0.67
NORTHAMPTON	1.33	2.56	0.47	0.65
NORTHUMBERLAND	0.83	2.09	0.00	0.41
PERRY	*	*	0.98	0.65
PHILADELPHIA	0.75	*	0.00	*
PIKE	2.20	2.84	1.64	0.98
POTTER	*	*	*	0.48
SCHUYLKILL	0.64	1.92	0.00	0.35
SNYDER	1.21	*	0.40	0.57
SOMERSET	0.65	1.76	0.00	0.34
SULLIVAN	*	*	*	0.43
SUSQUEHANNA	1.16	2.26	0.33	0.54
TIOGA	*	*	*	0.50
UNION	1.57	2.46	0.87	0.70
VENANGO	*	1.71	0.00	0.29
WARREN	*	*	0.00	0.38
WASHINGTON	1.32	2.63	0.15	0.60
WAYNE	*	2.25	0.21	0.53
WESTMORELAND	0.96	2.09	0.00	0.42
WYOMING	*	*	0.00	0.44
YORK	1.39	2.56	0.60	0.67

* = Functional Class Doesn't Exist in County

Questions? Please contact Andrew O'Neill at the Bureau of Planning and Research, 717-346-3250 or andoneill@pa.gov

NOTE: The projected growth factors are derived using historical VMT (Vehicle Miles Traveled) data (1994 to 2017), as well as Woods and Poole demographic and economic data. The factors should be compounded when calculating future values. The factors should not be used to project traffic beyond a 20-year period. Please be aware that these factors are estimates, and unforeseen events (opening of shopping centers, fast food franchises, gas stations, etc) could cause growth to change over time.

Growth Factors for August 2019 to July 2020				
County	Urban Interstate	Rural Interstate	Urban Non-Interstate	Rural Non-Interstate
ADAMS	*	*	0.93	0.73
ALLEGHENY	0.81	*	0.00	0.37
ARMSTRONG	0.79	*	0.00	0.36
BEAVER	0.73	1.93	0.00	0.33
BEDFORD	*	2.10	0.00	0.42
BERKS	1.10	2.41	0.20	0.57
BLAIR	0.75	1.91	0.00	0.36
BRADFORD	1.08	*	0.01	0.49
BUCKS	1.31	2.31	0.54	0.59
BUTLER	1.75	2.74	0.65	0.75
CAMBRIA	0.34	*	0.00	0.18
CAMERON	*	*	*	0.14
CARBON	1.30	2.58	0.33	0.62
CENTRE	1.49	2.53	0.65	0.68
CHESTER	1.70	2.99	0.52	0.80
CLARION	0.90	2.00	0.00	0.40
CLEARFIELD	0.93	2.06	0.00	0.42
CLINTON	0.88	2.21	0.00	0.45
COLUMBIA	1.14	2.25	0.30	0.54
CRAWFORD	0.89	1.96	0.03	0.42
CUMBERLAND	1.53	2.55	0.74	0.69
DAUPHIN	1.31	*	0.41	0.63
DELAWARE	0.93	*	0.00	*
ELK	*	*	0.00	0.29
ERIE	0.95	2.14	0.00	0.43
FAYETTE	0.77	*	0.00	0.38
FOREST	*	*	*	0.65
FRANKLIN	1.31	2.54	0.47	0.65
FULTON	*	2.10	*	0.50
GREENE	1.19	2.62	0.00	0.56
HUNTINGDON	*	1.91	0.00	0.37
INDIANA	1.17	*	0.11	0.52
JEFFERSON	*	2.11	0.00	0.42
JUNIATA	*	*	*	0.55
LACKAWANNA	0.78	2.27	0.00	0.42
LANCASTER	1.74	2.64	1.08	0.78
LAWRENCE	0.74	2.05	0.00	0.35
LEBANON	*	2.44	0.39	0.61
LEHIGH	1.54	2.86	0.43	0.73
LUZERNE	0.71	2.14	0.00	0.39
LYCOMING	0.96	2.16	0.00	0.45
MCKEAN	0.60	*	0.00	0.33
MERCER	0.63	1.96	0.00	0.33
MIFFLIN	0.73	*	0.00	0.37
MONROE	1.40	2.46	0.68	0.67
MONTGOMERY	1.17	*	0.28	0.57
MONTOUR	1.48	2.61	0.28	0.65
NORTHAMPTON	1.28	2.53	0.41	0.63
NORTHUMBERLAND	0.75	2.04	0.00	0.39
PERRY	*	*	0.92	0.63
PHILADELPHIA	0.69	*	0.00	*
PIKE	2.14	2.79	1.59	0.96
POTTER	*	*	*	0.46
SCHUYLKILL	0.58	1.89	0.00	0.33
SNYDER	1.15	*	0.35	0.55
SOMERSET	0.59	1.72	0.00	0.32
SULLIVAN	*	*	*	0.42
SUSQUEHANNA	1.11	2.23	0.27	0.53
TIOGA	*	*	*	0.48
UNION	1.52	2.42	0.82	0.69
VENANGO	*	1.67	0.00	0.28
WARREN	*	*	0.00	0.36
WASHINGTON	1.28	2.62	0.10	0.59
WAYNE	*	2.22	0.16	0.51
WESTMORELAND	0.89	2.05	0.00	0.40
WYOMING	*	*	0.00	0.43
YORK	1.34	2.53	0.54	0.66

* = Functional Class Doesn't Exist in County

Questions? Please contact Andrew O'Neill at the Bureau of Planning and Research, 717-346-3250 or andoneill@pa.gov

NOTE: The projected growth factors are derived using historical VMT (Vehicle Miles Traveled) data (1994 to 2018), as well as Woods and Poole demographic and economic data. The factors should be compounded when calculating future values. The factors should not be used to project traffic beyond a 20-year period. Please be aware that these factors are estimates, and unforeseen events (opening of shopping centers, fast food franchises, gas stations, etc) could cause growth to change over time.

Land Use: 220

Multifamily Housing (Low-Rise)

Description

Low-rise multifamily housing includes apartments, townhouses, and condominiums located within the same building with at least three other dwelling units and that have one or two levels (floors). Multifamily housing (mid-rise) (Land Use 221), multifamily housing (high-rise) (Land Use 222), and off-campus student apartment (Land Use 225) are related land uses.

Additional Data

In prior editions of *Trip Generation Manual*, the low-rise multifamily housing sites were further divided into rental and condominium categories. An investigation of vehicle trip data found no clear differences in trip making patterns between the rental and condominium sites within the ITE database. As more data are compiled for future editions, this land use classification can be reinvestigated.

For the three sites for which both the number of residents and the number of occupied dwelling units were available, there were an average of 2.72 residents per occupied dwelling unit.

For the two sites for which the numbers of both total dwelling units and occupied dwelling units were available, an average of 96.2 percent of the total dwelling units were occupied.

This land use included data from a wide variety of units with different sizes, price ranges, locations, and ages. Consequently, there was a wide variation in trips generated within this category. Other factors, such as geographic location and type of adjacent and nearby development, may also have had an effect on the site trip generation.

Time-of-day distribution data for this land use are presented in Appendix A. For the 10 general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:15 and 8:15 a.m. and 4:45 and 5:45 p.m., respectively. For the one site with Saturday data, the overall highest vehicle volume was counted between 9:45 and 10:45 a.m. For the one site with Sunday data, the overall highest vehicle volume was counted between 11:45 a.m. and 12:45 p.m.

For the one dense multi-use urban site with 24-hour count data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:00 and 8:00 a.m. and 6:15 and 7:15 p.m., respectively.

For the three sites for which data were provided for both occupied dwelling units and residents, there was an average of 2.72 residents per occupied dwelling unit.

The average numbers of person trips per vehicle trip at the five general urban/suburban sites at which both person trip and vehicle trip data were collected were as follows:

- 1.13 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.21 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in British Columbia (CAN), California, District of Columbia, Florida, Georgia, Illinois, Indiana, Maine, Maryland, Minnesota, New Jersey, New York, Ontario, Oregon, Pennsylvania, South Dakota, Tennessee, Texas, Utah, Virginia, and Washington.

It is expected that the number of bedrooms and number of residents are likely correlated to the number of trips generated by a residential site. Many of the studies included in this land use did not indicate the total number of bedrooms. To assist in the future analysis of this land use, it is important that this information be collected and included in trip generation data submissions.

Source Numbers

168, 187, 188, 204, 211, 300, 305, 306, 319, 320, 321, 357, 390, 412, 418, 525, 530, 571, 579, 583, 864, 868, 869, 870, 896, 903, 918, 946, 947, 948, 951

Multifamily Housing (Low-Rise) (220)

Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban

Number of Studies: 29

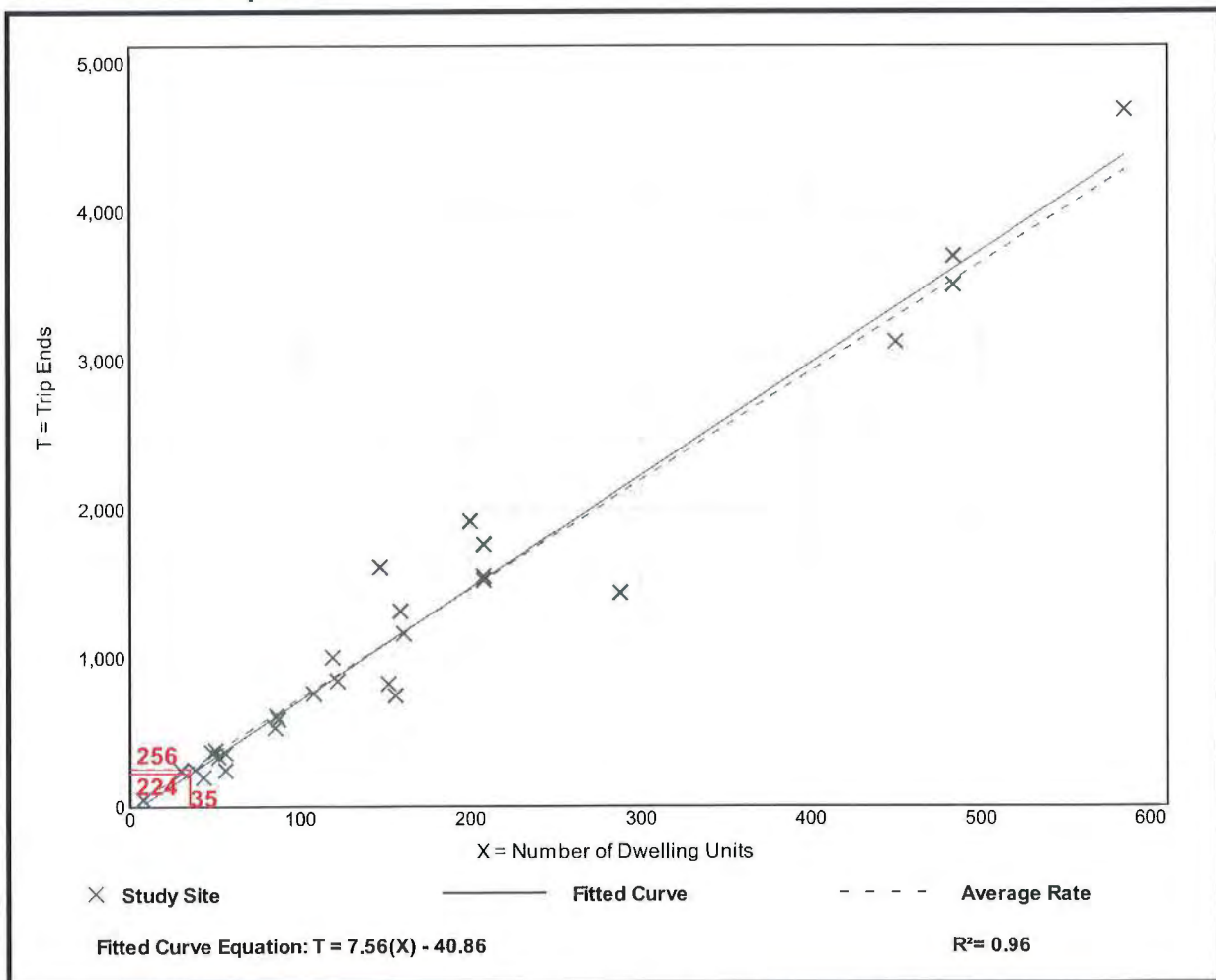
Avg. Num. of Dwelling Units: 168

Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
7.32	4.45 - 10.97	1.31

Data Plot and Equation



Multifamily Housing (Low-Rise) (220)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 42

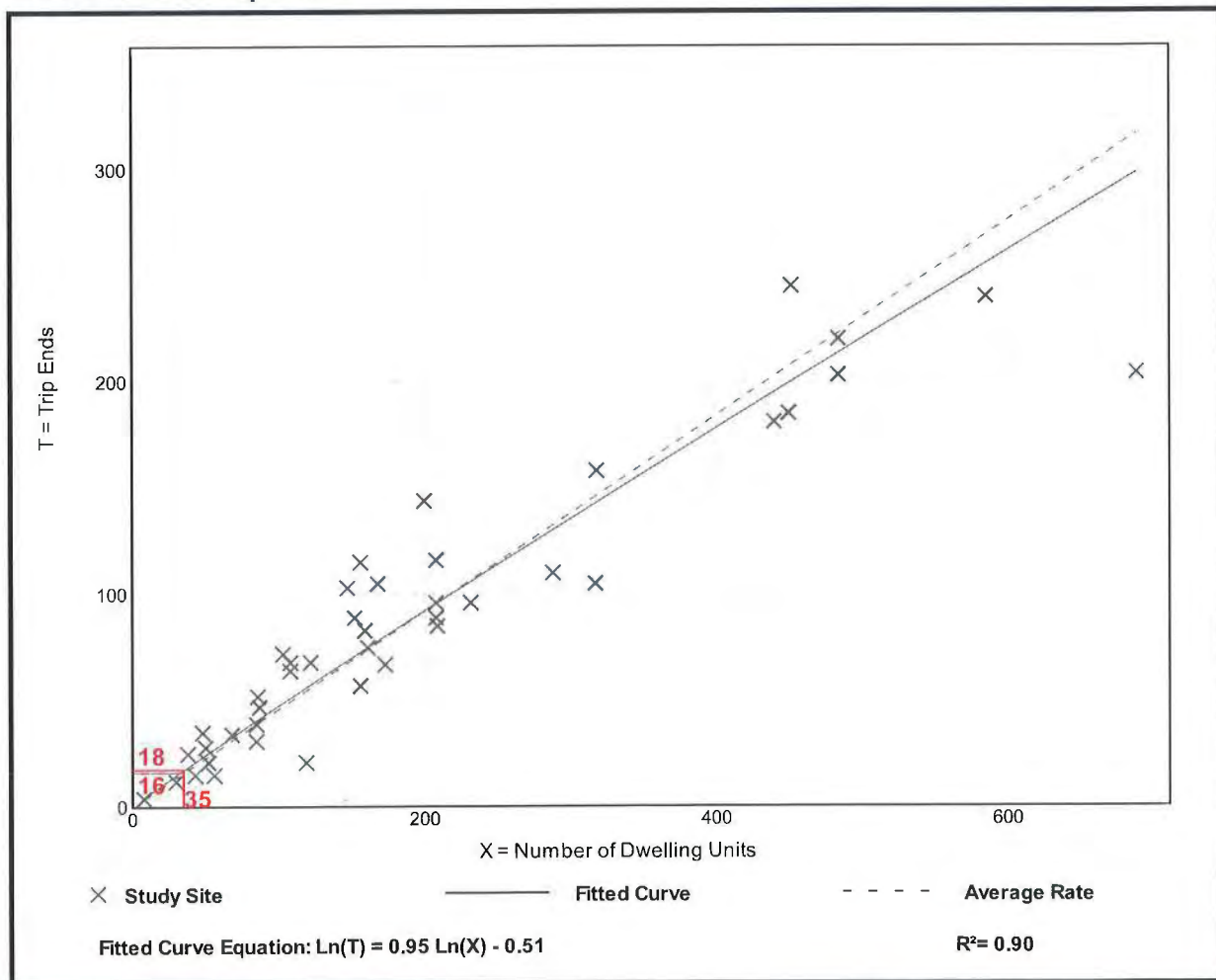
Avg. Num. of Dwelling Units: 199

Directional Distribution: 23% entering, 77% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.46	0.18 - 0.74	0.12

Data Plot and Equation



Multifamily Housing (Low-Rise) (220)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 50

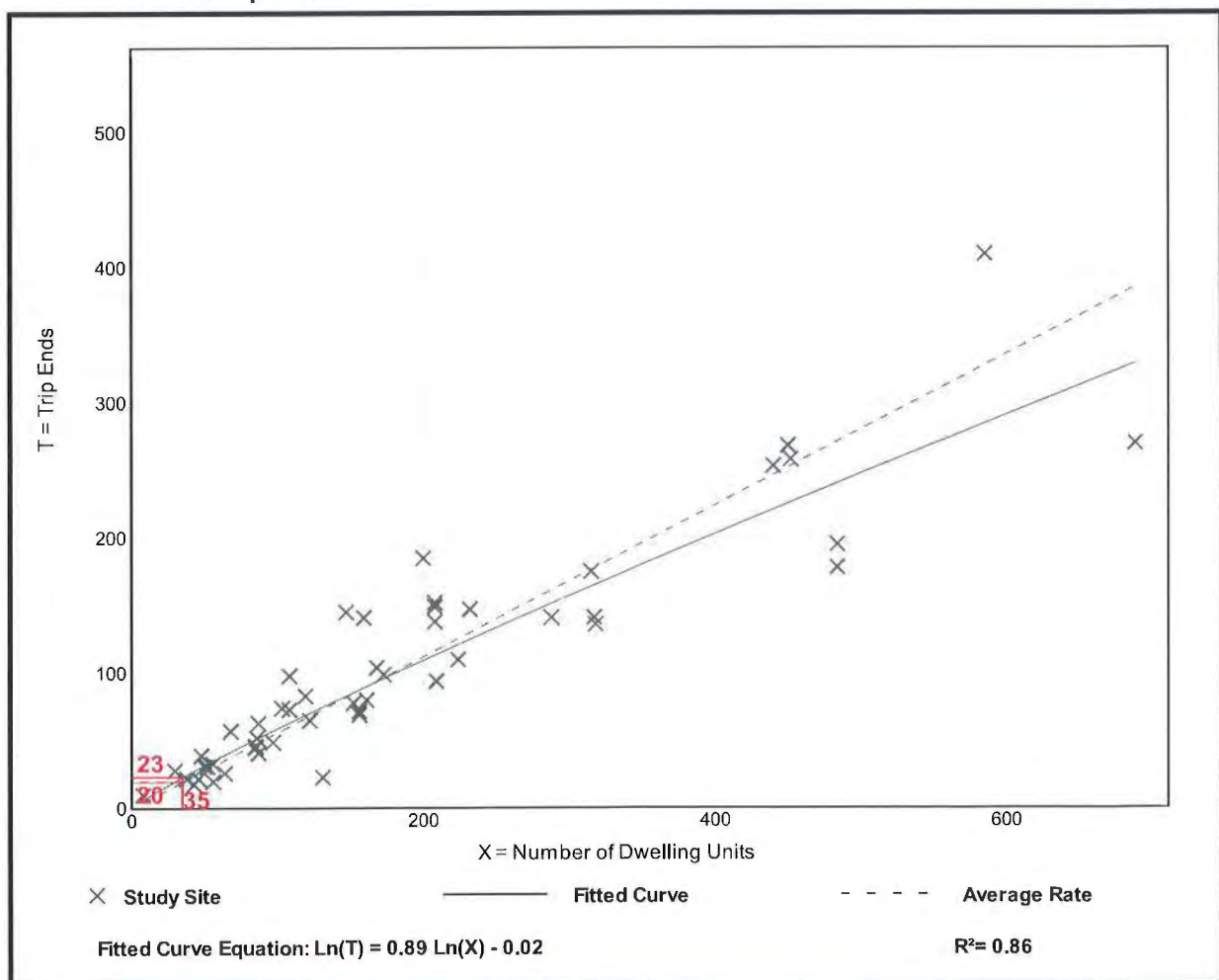
Avg. Num. of Dwelling Units: 187

Directional Distribution: 63% entering, 37% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.56	0.18 - 1.25	0.16

Data Plot and Equation



















APPENDIX F

Capacity Analyses

HCM 2010 Signalized Intersection Summary
5: N Wayne & Eagle

06/18/2019

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	24	93	20	19	75	25	14	228	33	28	324	34
Future Volume (veh/h)	24	93	20	19	75	25	14	228	33	28	324	34
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1800	1761	1800	1872	1827	1872	1872	1806	1872	1800	1756	1800
Adj Flow Rate, veh/h	26	101	22	21	82	27	15	248	36	30	352	37
Adj No. of Lanes	0	1	0	0	1	0	0	1	0	0	1	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	0	0	0	0	0	0	4	4	4	2	2	2
Cap, veh/h	119	186	38	115	180	55	99	1026	143	117	1016	102
Arrive On Green	0.15	0.15	0.15	0.15	0.15	0.15	0.68	0.68	0.68	0.68	0.68	0.68
Sat Flow, veh/h	205	1212	245	188	1178	358	30	1505	210	55	1491	150
Grp Volume(v), veh/h	149	0	0	130	0	0	299	0	0	419	0	0
Grp Sat Flow(s),veh/h/ln	1662	0	0	1725	0	0	1745	0	0	1696	0	0
Q Serve(g_s), s	0.7	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Cycle Q Clear(g_c), s	3.9	0.0	0.0	3.2	0.0	0.0	3.1	0.0	0.0	4.9	0.0	0.0
Prop In Lane	0.17		0.15	0.16		0.21	0.05		0.12	0.07		0.09
Lane Grp Cap(c), veh/h	342	0	0	350	0	0	1268	0	0	1236	0	0
V/C Ratio(X)	0.44	0.00	0.00	0.37	0.00	0.00	0.24	0.00	0.00	0.34	0.00	0.00
Avail Cap(c_a), veh/h	794	0	0	816	0	0	1268	0	0	1236	0	0
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00
Uniform Delay (d), s/veh	19.0	0.0	0.0	18.7	0.0	0.0	3.0	0.0	0.0	3.2	0.0	0.0
Incr Delay (d2), s/veh	0.9	0.0	0.0	0.7	0.0	0.0	0.4	0.0	0.0	0.7	0.0	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	3.4	0.0	0.0	3.0	0.0	0.0	3.0	0.0	0.0	4.7	0.0	0.0
LnGrp Delay(d),s/veh	19.9	0.0	0.0	19.4	0.0	0.0	3.4	0.0	0.0	4.0	0.0	0.0
LnGrp LOS	B			B			A			A		
Approach Vol, veh/h		149			130			299			419	
Approach Delay, s/veh		19.9			19.4			3.4			4.0	
Approach LOS		B			B			A			A	
Timer	1	2	3	4	5	6	7	8				
Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		37.0		11.4		37.0		11.4				
Change Period (Y+Rc), s		5.0		5.0		5.0		5.0				
Max Green Setting (Gmax), s		32.0		20.0		32.0		20.0				
Max Q Clear Time (g_c+I1), s		5.1		5.9		6.9		5.2				
Green Ext Time (p_c), s		2.0		0.6		2.9		0.6				
Intersection Summary												
HCM 2010 Ctrl Delay			8.2									
HCM 2010 LOS			A									

EX am 06/17/2019

Synchro 10 Light Report
Page 1

Intersection

Intersection Delay, s/veh 9.4

Intersection LOS A

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	46	60	6	32	84	30	5	72	13	44	76	40
Future Vol, veh/h	46	60	6	32	84	30	5	72	13	44	76	40
Peak Hour Factor	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79
Heavy Vehicles, %	2	7	0	6	1	3	0	3	0	5	0	0
Mvmt Flow	58	76	8	41	106	38	6	91	16	56	96	51
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	9.3	9.6	8.9	9.7
HCM LOS	A	A	A	A

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	6%	41%	22%	28%
Vol Thru, %	80%	54%	58%	48%
Vol Right, %	14%	5%	21%	25%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	90	112	146	160
LT Vol	5	46	32	44
Through Vol	72	60	84	76
RT Vol	13	6	30	40
Lane Flow Rate	114	142	185	203
Geometry Grp	1	1	1	1
Degree of Util (X)	0.155	0.196	0.25	0.272
Departure Headway (Hd)	4.886	4.988	4.873	4.833
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	728	715	732	737
Service Time	2.956	3.058	2.938	2.897
HCM Lane V/C Ratio	0.157	0.199	0.253	0.275
HCM Control Delay	8.9	9.3	9.6	9.7
HCM Lane LOS	A	A	A	A
HCM 95th-tile Q	0.5	0.7	1	1.1

Intersection

Intersection Delay, s/veh 8.8

Intersection LOS A

















Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	0	158	1	3	144	0	3	0	2	1	0	0
Future Vol, veh/h	0	158	1	3	144	0	3	0	2	1	0	0
Peak Hour Factor	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76
Heavy Vehicles, %	0	1	0	33	3	0	33	0	0	0	0	0
Mvmt Flow	0	208	1	4	189	0	4	0	3	1	0	0
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	8.4	9.2	8.3	8
HCM LOS	A	A	A	A

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	60%	0%	2%	100%
Vol Thru, %	0%	99%	98%	0%
Vol Right, %	40%	1%	0%	0%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	5	159	147	1
LT Vol	3	0	3	1
Through Vol	0	158	144	0
RT Vol	2	1	0	0
Lane Flow Rate	7	209	193	1
Geometry Grp	1	1	1	1
Degree of Util (X)	0.01	0.237	0.249	0.002
Departure Headway (Hd)	5.259	4.071	4.635	5.023
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	685	872	772	717
Service Time	3.259	2.145	2.688	3.024
HCM Lane V/C Ratio	0.01	0.24	0.25	0.001
HCM Control Delay	8.3	8.4	9.2	8
HCM Lane LOS	A	A	A	A
HCM 95th-tile Q	0	0.9	1	0

HCM 2010 Signalized Intersection Summary 5: N Wayne & Eagle

06/18/2019

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	44	147	22	36	138	41	23	271	43	32	342	36
Future Volume (veh/h)	44	147	22	36	138	41	23	271	43	32	342	36
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1800	1779	1800	1872	1872	1872	1872	1838	1872	1800	1770	1800
Adj Flow Rate, veh/h	46	153	23	38	144	43	24	282	45	33	356	38
Adj No. of Lanes	0	1	0	0	1	0	0	1	0	0	1	0
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	1	1	1	0	0	0	2	2	2	2	2	2
Cap, veh/h	135	246	34	123	240	66	108	960	146	117	963	98
Arrive On Green	0.20	0.20	0.20	0.20	0.20	0.20	0.65	0.65	0.65	0.65	0.65	0.65
Sat Flow, veh/h	253	1257	175	207	1227	339	51	1482	225	63	1487	151
Grp Volume(v), veh/h	222	0	0	225	0	0	351	0	0	427	0	0
Grp Sat Flow(s), veh/h/ln	1685	0	0	1773	0	0	1758	0	0	1701	0	0
Q Serve(g_s), s	0.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Cycle Q Clear(g_c), s	6.0	0.0	0.0	5.7	0.0	0.0	4.4	0.0	0.0	5.8	0.0	0.0
Prop In Lane	0.21		0.10	0.17		0.19	0.07		0.13	0.08		0.09
Lane Grp Cap(c), veh/h	414	0	0	429	0	0	1214	0	0	1178	0	0
V/C Ratio(X)	0.54	0.00	0.00	0.52	0.00	0.00	0.29	0.00	0.00	0.36	0.00	0.00
Avail Cap(c_a), veh/h	759	0	0	794	0	0	1214	0	0	1178	0	0
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00
Uniform Delay (d), s/veh	18.9	0.0	0.0	18.8	0.0	0.0	3.9	0.0	0.0	4.2	0.0	0.0
Incr Delay (d2), s/veh	1.1	0.0	0.0	1.0	0.0	0.0	0.6	0.0	0.0	0.9	0.0	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.0	0.0	0.0	3.0	0.0	0.0	2.3	0.0	0.0	3.0	0.0	0.0
LnGrp Delay(d),s/veh	20.0	0.0	0.0	19.8	0.0	0.0	4.5	0.0	0.0	5.1	0.0	0.0
LnGrp LOS	B			B			A			A		
Approach Vol, veh/h		222			225			351			427	
Approach Delay, s/veh		20.0			19.8			4.5			5.1	
Approach LOS		B			B			A			A	
Timer	1	2	3	4	5	6	7	8				
Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		37.0		13.9		37.0		13.9				
Change Period (Y+Rc), s		5.0		5.0		5.0		5.0				
Max Green Setting (Gmax), s		32.0		20.0		32.0		20.0				
Max Q Clear Time (g_c+I1), s		6.4		8.0		7.8		7.7				
Green Ext Time (p_c), s		2.4		1.0		3.0		1.0				
Intersection Summary												
HCM 2010 Ctrl Delay				10.3								
HCM 2010 LOS				B								

EX pm 06/17/2019

Synchro 10 Light Report
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Intersection

Intersection Delay, s/veh 10.7

Intersection LOS B

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	56	105	7	21	142	36	18	94	59	45	72	129
Future Vol, veh/h	56	105	7	21	142	36	18	94	59	45	72	129
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Heavy Vehicles, %	4	0	0	5	1	0	6	1	2	0	1	0
Mvmt Flow	60	112	7	22	151	38	19	100	63	48	77	137
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	10.6	10.9	10.3	10.9
HCM LOS	B	B	B	B

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	11%	33%	11%	18%
Vol Thru, %	55%	62%	71%	29%
Vol Right, %	35%	4%	18%	52%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	171	168	199	246
LT Vol	18	56	21	45
Through Vol	94	105	142	72
RT Vol	59	7	36	129
Lane Flow Rate	182	179	212	262
Geometry Grp	1	1	1	1
Degree of Util (X)	0.268	0.274	0.315	0.363
Departure Headway (Hd)	5.295	5.518	5.357	4.99
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	677	651	671	720
Service Time	3.335	3.559	3.396	3.027
HCM Lane V/C Ratio	0.269	0.275	0.316	0.364
HCM Control Delay	10.3	10.6	10.9	10.9
HCM Lane LOS	B	B	B	B
HCM 95th-tile Q	1.1	1.1	1.3	1.7

Intersection

Intersection Delay, s/veh 8.7

Intersection LOS A

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	1	243	1	2	186	1	0	0	3	0	0	1
Future Vol, veh/h	1	243	1	2	186	1	0	0	3	0	0	1
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Heavy Vehicles, %	0	0	0	0	2	0	0	0	0	0	0	0
Mvmt Flow	1	264	1	2	202	1	0	0	3	0	0	1
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0




Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	8.9	8.4	7.3	7.3
HCM LOS	A	A	A	A

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	0%	0%	1%	0%
Vol Thru, %	0%	99%	98%	0%
Vol Right, %	100%	0%	1%	100%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	3	245	189	1
LT Vol	0	1	2	0
Through Vol	0	243	186	0
RT Vol	3	1	1	1
Lane Flow Rate	3	266	205	1
Geometry Grp	1	1	1	1
Degree of Util (X)	0.004	0.3	0.234	0.001
Departure Headway (Hd)	4.294	4.059	4.104	4.297
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	838	881	869	838
Service Time	2.294	2.103	2.159	2.297
HCM Lane V/C Ratio	0.004	0.302	0.236	0.001
HCM Control Delay	7.3	8.9	8.4	7.3
HCM Lane LOS	A	A	A	A
HCM 95th-tile Q	0	1.3	0.9	0

Intersection

Int Delay, s/veh 0.3

Movement EBL EBR NBL NBT SBT SBR

Lane Configurations						
Traffic Vol, veh/h	2	5	2	150	165	0
Future Vol, veh/h	2	5	2	150	165	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	79	79	79	79	79	79
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	3	6	3	190	209	0

Major/Minor Minor2 Major1 Major2

Conflicting Flow All	405	209	209	0	-	0
Stage 1	209	-	-	-	-	-
Stage 2	196	-	-	-	-	-
Critical Hdwy	6.4	6.2	4.3	-	-	-
Critical Hdwy Stg 1	5.4	-	-	-	-	-
Critical Hdwy Stg 2	5.4	-	-	-	-	-
Follow-up Hdwy	3	3.1	3	-	-	-
Pot Cap-1 Maneuver	688	885	1019	-	-	-
Stage 1	956	-	-	-	-	-
Stage 2	969	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	686	885	1019	-	-	-
Mov Cap-2 Maneuver	686	-	-	-	-	-
Stage 1	953	-	-	-	-	-
Stage 2	969	-	-	-	-	-

Approach EB NB SB

















HCM Control Delay, s	9.5	0.1	0
HCM LOS	A		

Minor Lane/Major Mvmt NBL NBT EBLn1 SBT SBR

Capacity (veh/h)	1019	-	817	-	-
HCM Lane V/C Ratio	0.002	-	0.011	-	-
HCM Control Delay (s)	8.5	0	9.5	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

HCM 2010 Signalized Intersection Summary 5: N Wayne & Eagle

04/16/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	24	97	20	19	76	25	14	228	33	28	324	34
Future Volume (veh/h)	24	97	20	19	76	25	14	228	33	28	324	34
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1800	1762	1800	1872	1827	1872	1872	1806	1872	1800	1756	1800
Adj Flow Rate, veh/h	26	105	22	21	83	27	15	248	36	30	352	37
Adj No. of Lanes	0	1	0	0	1	0	0	1	0	0	1	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	0	0	0	0	0	0	4	4	4	2	2	2
Cap, veh/h	118	191	37	115	184	55	98	1023	143	117	1013	102
Arrive On Green	0.16	0.16	0.16	0.16	0.16	0.16	0.68	0.68	0.68	0.68	0.68	0.68
Sat Flow, veh/h	198	1227	239	185	1185	356	30	1505	210	55	1491	150
Grp Volume(v), veh/h	153	0	0	131	0	0	299	0	0	419	0	0
Grp Sat Flow(s),veh/h/ln	1665	0	0	1725	0	0	1745	0	0	1696	0	0
Q Serve(g_s), s	0.8	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Cycle Q Clear(g_c), s	4.0	0.0	0.0	3.3	0.0	0.0	3.2	0.0	0.0	5.0	0.0	0.0
Prop In Lane	0.17		0.14	0.16		0.21	0.05		0.12	0.07		0.09
Lane Grp Cap(c), veh/h	346	0	0	355	0	0	1264	0	0	1232	0	0
V/C Ratio(X)	0.44	0.00	0.00	0.37	0.00	0.00	0.24	0.00	0.00	0.34	0.00	0.00
Avail Cap(c_a), veh/h	793	0	0	814	0	0	1264	0	0	1232	0	0
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00
Uniform Delay (d), s/veh	19.0	0.0	0.0	18.7	0.0	0.0	3.0	0.0	0.0	3.3	0.0	0.0
Incr Delay (d2), s/veh	0.9	0.0	0.0	0.6	0.0	0.0	0.4	0.0	0.0	0.8	0.0	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	3.5	0.0	0.0	3.0	0.0	0.0	3.0	0.0	0.0	4.7	0.0	0.0
LnGrp Delay(d),s/veh	19.9	0.0	0.0	19.3	0.0	0.0	3.4	0.0	0.0	4.0	0.0	0.0
LnGrp LOS	B			B			A			A		
Approach Vol, veh/h	153			131			299			419		
Approach Delay, s/veh	19.9			19.3			3.4			4.0		
Approach LOS	B			B			A			A		
Timer	1	2	3	4	5	6	7	8				
Assigned Phs	2		4		6		8					
Phs Duration (G+Y+Rc), s	37.0		11.6		37.0		11.6					
Change Period (Y+Rc), s	5.0		5.0		5.0		5.0					
Max Green Setting (Gmax), s	32.0		20.0		32.0		20.0					
Max Q Clear Time (g_c+11), s	5.2		6.0		7.0		5.3					
Green Ext Time (p_c), s	2.0		0.7		2.9		0.6					
Intersection Summary												
HCM 2010 Ctrl Delay	8.3											
HCM 2010 LOS	A											

Intersection

Intersection Delay, s/veh 9.6

Intersection LOS A

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	47	60	6	32	84	31	5	72	13	48	78	44
Future Vol, veh/h	47	60	6	32	84	31	5	72	13	48	78	44
Peak Hour Factor	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79
Heavy Vehicles, %	2	7	0	6	1	3	0	3	0	5	0	0
Mvmt Flow	59	76	8	41	106	39	6	91	16	61	99	56
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	9.4	9.7	8.9	9.9
HCM LOS	A	A	A	A

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	6%	42%	22%	28%
Vol Thru, %	80%	53%	57%	46%
Vol Right, %	14%	5%	21%	26%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	90	113	147	170
LT Vol	5	47	32	48
Through Vol	72	60	84	78
RT Vol	13	6	31	44
Lane Flow Rate	114	143	186	215
Geometry Grp	1	1	1	1
Degree of Util (X)	0.156	0.2	0.254	0.289
Departure Headway (Hd)	4.914	5.027	4.906	4.842
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	723	708	726	736
Service Time	2.99	3.099	2.974	2.909
HCM Lane V/C Ratio	0.158	0.202	0.256	0.292
HCM Control Delay	8.9	9.4	9.7	9.9
HCM Lane LOS	A	A	A	A
HCM 95th-tile Q	0.6	0.7	1	1.2

Intersection

Intersection Delay, s/veh 8.8

Intersection LOS A

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	0	159	1	3	148	0	3	0	2	1	0	0
Future Vol, veh/h	0	159	1	3	148	0	3	0	2	1	0	0
Peak Hour Factor	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76
Heavy Vehicles, %	0	1	0	33	3	0	33	0	0	0	0	0
Mvmt Flow	0	209	1	4	195	0	4	0	3	1	0	0
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0




Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	8.4	9.3	8.3	8
HCM LOS	A	A	A	A

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	60%	0%	2%	100%
Vol Thru, %	0%	99%	98%	0%
Vol Right, %	40%	1%	0%	0%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	5	160	151	1
LT Vol	3	0	3	1
Through Vol	0	159	148	0
RT Vol	2	1	0	0
Lane Flow Rate	7	211	199	1
Geometry Grp	1	1	1	1
Degree of Util (X)	0.01	0.238	0.256	0.002
Departure Headway (Hd)	5.273	4.075	4.636	5.038
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	683	869	771	714
Service Time	3.273	2.151	2.689	3.039
HCM Lane V/C Ratio	0.01	0.243	0.258	0.001
HCM Control Delay	8.3	8.4	9.3	8
HCM Lane LOS	A	A	A	A
HCM 95th-tile Q	0	0.9	1	0

Intersection

Int Delay, s/veh 0.3

Movement EBL EBR NBL NBT SBT SBR

Lane Configurations						
Traffic Vol, veh/h	2	5	1	149	160	1
Future Vol, veh/h	2	5	1	149	160	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	79	79	79	79	79	79
Heavy Vehicles, %	0	0	0	3	3	0
Mvmt Flow	3	6	1	189	203	1

Major/Minor Minor2 Major1 Major2

Conflicting Flow All	395	204	204	0	-	0
Stage 1	204	-	-	-	-	-
Stage 2	191	-	-	-	-	-
Critical Hdwy	6.4	6.2	4.3	-	-	-
Critical Hdwy Stg 1	5.4	-	-	-	-	-
Critical Hdwy Stg 2	5.4	-	-	-	-	-
Follow-up Hdwy	3	3.1	3	-	-	-
Pot Cap-1 Maneuver	698	891	1023	-	-	-
Stage 1	961	-	-	-	-	-
Stage 2	975	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	697	891	1023	-	-	-
Mov Cap-2 Maneuver	697	-	-	-	-	-
Stage 1	960	-	-	-	-	-
Stage 2	975	-	-	-	-	-

Approach EB NB SB

HCM Control Delay, s	9.4	0.1	0
HCM LOS	A		

Minor Lane/Major Mvmt NBL NBT EBLn1 SBT SBR

Capacity (veh/h)	1023	-	825	-	-
HCM Lane V/C Ratio	0.001	-	0.011	-	-
HCM Control Delay (s)	8.5	0	9.4	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

Intersection

Int Delay, s/veh 0.2

Movement EBL EBR NBL NBT SBT SBR

Lane Configurations	W			W	W	
Traffic Vol, veh/h	1	3	5	194	249	2
Future Vol, veh/h	1	3	5	194	249	2
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	1	3	6	216	277	2

Major/Minor Minor2 Major1 Major2

Conflicting Flow All	506	278	279	0	-	0
Stage 1	278	-	-	-	-	-
Stage 2	228	-	-	-	-	-
Critical Hdwy	6.4	6.2	4.3	-	-	-
Critical Hdwy Stg 1	5.4	-	-	-	-	-
Critical Hdwy Stg 2	5.4	-	-	-	-	-
Follow-up Hdwy	3	3.1	3	-	-	-
Pot Cap-1 Maneuver	598	809	964	-	-	-
Stage 1	886	-	-	-	-	-
Stage 2	936	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	594	809	964	-	-	-
Mov Cap-2 Maneuver	594	-	-	-	-	-
Stage 1	880	-	-	-	-	-
Stage 2	936	-	-	-	-	-

Approach EB NB SB

















HCM Control Delay, s	9.9	0.2	0
HCM LOS	A		

Minor Lane/Major Mvmt NBL NBT EBLn1 SBT SBR

Capacity (veh/h)	964	-	742	-	-
HCM Lane V/C Ratio	0.006	-	0.006	-	-
HCM Control Delay (s)	8.8	0	9.9	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

HCM 2010 Signalized Intersection Summary 5: N Wayne & Eagle

04/16/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	44	150	22	36	142	41	23	271	43	32	342	36
Future Volume (veh/h)	44	150	22	36	142	41	23	271	43	32	342	36
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1800	1779	1800	1872	1872	1872	1872	1838	1872	1800	1770	1800
Adj Flow Rate, veh/h	46	156	23	38	148	43	24	282	45	33	356	38
Adj No. of Lanes	0	1	0	0	1	0	0	1	0	0	1	0
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	1	1	1	0	0	0	2	2	2	2	2	2
Cap, veh/h	134	249	34	122	244	66	108	958	146	117	961	98
Arrive On Green	0.20	0.20	0.20	0.20	0.20	0.20	0.65	0.65	0.65	0.65	0.65	0.65
Sat Flow, veh/h	249	1265	172	203	1238	333	51	1482	225	63	1486	151
Grp Volume(v), veh/h	225	0	0	229	0	0	351	0	0	427	0	0
Grp Sat Flow(s),veh/h/ln	1686	0	0	1775	0	0	1758	0	0	1701	0	0
Q Serve(g_s), s	0.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Cycle Q Clear(g_c), s	6.1	0.0	0.0	5.9	0.0	0.0	4.4	0.0	0.0	5.9	0.0	0.0
Prop In Lane	0.20		0.10	0.17		0.19	0.07		0.13	0.08		0.09
Lane Grp Cap(c), veh/h	417	0	0	432	0	0	1212	0	0	1175	0	0
V/C Ratio(X)	0.54	0.00	0.00	0.53	0.00	0.00	0.29	0.00	0.00	0.36	0.00	0.00
Avail Cap(c_a), veh/h	758	0	0	793	0	0	1212	0	0	1175	0	0
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00
Uniform Delay (d), s/veh	18.9	0.0	0.0	18.8	0.0	0.0	4.0	0.0	0.0	4.2	0.0	0.0
Incr Delay (d2), s/veh	1.1	0.0	0.0	1.0	0.0	0.0	0.6	0.0	0.0	0.9	0.0	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.0	0.0	0.0	3.0	0.0	0.0	2.3	0.0	0.0	3.0	0.0	0.0
LnGrp Delay(d),s/veh	20.0	0.0	0.0	19.8	0.0	0.0	4.6	0.0	0.0	5.1	0.0	0.0
LnGrp LOS	B			B			A			A		
Approach Vol, veh/h	225				229				351			
Approach Delay, s/veh	20.0				19.8				4.6			
Approach LOS	B				B				A			
Timer	1	2	3	4	5	6	7	8				
Assigned Phs	2		4		6		8					
Phs Duration (G+Y+Rc), s	37.0		14.1		37.0		14.1					
Change Period (Y+Rc), s	5.0		5.0		5.0		5.0					
Max Green Setting (Gmax), s	32.0		20.0		32.0		20.0					
Max Q Clear Time (g_c+I1), s	6.4		8.1		7.9		7.9					
Green Ext Time (p_c), s	2.4		1.0		3.0		1.0					
Intersection Summary												
HCM 2010 Ctrl Delay			10.4									
HCM 2010 LOS			B									

Intersection

Intersection Delay, s/veh 10.9

Intersection LOS B

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	60	105	7	21	142	40	18	96	59	48	73	131
Future Vol, veh/h	60	105	7	21	142	40	18	96	59	48	73	131
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Heavy Vehicles, %	4	0	0	5	1	0	6	1	2	0	1	0
Mvmt Flow	64	112	7	22	151	43	19	102	63	51	78	139
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	10.8	11	10.4	11.1
HCM LOS	B	B	B	B

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	10%	35%	10%	19%
Vol Thru, %	55%	61%	70%	29%
Vol Right, %	34%	4%	20%	52%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	173	172	203	252
LT Vol	18	60	21	48
Through Vol	96	105	142	73
RT Vol	59	7	40	131
Lane Flow Rate	184	183	216	268
Geometry Grp	1	1	1	1
Degree of Util (X)	0.273	0.283	0.323	0.375
Departure Headway (Hd)	5.345	5.565	5.39	5.033
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	672	644	665	713
Service Time	3.386	3.606	3.43	3.071
HCM Lane V/C Ratio	0.274	0.284	0.325	0.376
HCM Control Delay	10.4	10.8	11	11.1
HCM Lane LOS	B	B	B	B
HCM 95th-tile Q	1.1	1.2	1.4	1.7

Intersection

Intersection Delay, s/veh 8.7

Intersection LOS A




Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	1	247	1	2	189	1	0	0	3	0	0	1
Future Vol, veh/h	1	247	1	2	189	1	0	0	3	0	0	1
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Heavy Vehicles, %	0	0	0	0	2	0	0	0	0	0	0	0
Mvmt Flow	1	268	1	2	205	1	0	0	3	0	0	1
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	1	1
Conflicting Approach Left SB		NB	EB	WB
Conflicting Lanes Left	1	1	1	1
Conflicting Approach Right NB		SB	WB	EB
Conflicting Lanes Right	1	1	1	1
HCM Control Delay	8.9	8.5	7.3	7.3
HCM LOS	A	A	A	A

Lane	NBLn1	EBLn1	WBLn1	SBLn1
Vol Left, %	0%	0%	1%	0%
Vol Thru, %	0%	99%	98%	0%
Vol Right, %	100%	0%	1%	100%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	3	249	192	1
LT Vol	0	1	2	0
Through Vol	0	247	189	0
RT Vol	3	1	1	1
Lane Flow Rate	3	271	209	1
Geometry Grp	1	1	1	1
Degree of Util (X)	0.004	0.305	0.238	0.001
Departure Headway (Hd)	4.31	4.061	4.108	4.313
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	835	880	868	835
Service Time	2.31	2.107	2.163	2.313
HCM Lane V/C Ratio	0.004	0.308	0.241	0.001
HCM Control Delay	7.3	8.9	8.5	7.3
HCM Lane LOS	A	A	A	A
HCM 95th-tile Q	0	1.3	0.9	0

Intersection

Int Delay, s/veh 0.2

Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	2	3	5	190	248	3
Future Vol, veh/h	2	3	5	190	248	3
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	0	0	0	2	3	0
Mvmt Flow	2	3	6	211	276	3

Major/Minor	Minor2	Major1	Major2
Conflicting Flow All	501	278	279
Stage 1	278	-	-
Stage 2	223	-	-
Critical Hdwy	6.4	6.2	4.3
Critical Hdwy Stg 1	5.4	-	-
Critical Hdwy Stg 2	5.4	-	-
Follow-up Hdwy	3	3.1	3
Pot Cap-1 Maneuver	602	809	964
Stage 1	886	-	-
Stage 2	941	-	-
Platoon blocked, %			
Mov Cap-1 Maneuver	598	809	964
Mov Cap-2 Maneuver	598	-	-
Stage 1	880	-	-
Stage 2	941	-	-

Approach	EB	NB	SB
HCM Control Delay, s	10.1	0.2	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	964	-	709	-	-
HCM Lane V/C Ratio	0.006	-	0.008	-	-
HCM Control Delay (s)	8.8	0	10.1	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

APPENDIX G

Auxiliary Turn Lane Warrants

Turn Lane Warrant and Length Analysis Workbook

STUDY LOCATION AND ANALYSIS INFORMATION

Municipality:	Radnor	Analysis Date:	4/15/2020
County:	Delaware County	Conducted By:	FT
PennDOT Engineering District:	6	Checked By:	NB Approach
		Agency/Company Name:	FTA
Intersection & Approach Description: Stafford Ave & Site Driveway (assumes combined volumes into one hypothetical driveway)			
Analysis Period:	2024	Number of Approach Lanes:	1
Design Hour:	PM Peak Hour	Undivided or Divided Highway:	Undivided
Intersection Control:	Unsignalized	Type of Analysis	Left Turn Lane
Posted Speed Limit (MPH):	25		
Type of Terrain:	Level		
		Left or Right-Turn Lane Analysis?:	

VOLUME CALCULATIONS

Left Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	10	0.0%	10
	Through	-	194	5.0%	199
	Right	No	0	0.0%	N/A
Opposing	Left	No	0	0.0%	N/A
	Through	-	249	5.0%	256
	Right	Yes	4	0.0%	4

Advancing Volume:

209

Opposing Volume:

260

Left Turn Volume:

10

% Left Turns in Advancing Volume:

4.78%

Right Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	No	0	0.0%	N/A
	Through	-	0	0.0%	N/A
	Right	-	0	0.0%	N/A

Advancing Volume:

N/A

Right Turn Volume:

N/A

TURN LANE WARRANT FINDINGS

Left Turn Lane Warrant Findings	Right Turn Lane Warrant Findings
Applicable Warrant Figure: Figure 1	Applicable Warrant Figure: N/A
Warrant Met?: No	Warrant Met?: N/A

TURN LANE LENGTH CALCULATIONS

Intersection Control:	Unsignalized	
Design Hour Volume of Turning Lane:	10	
Cycles Per Hour (Assumed):	60	
Cycles Per Hour (If Known):		Average # of Vehicles/Cycle: N/A

PennDOT Publication 46, Exhibit 11-6

Type of Traffic Control	Speed (MPH)					
	25-35		40-45		50-60	
	Turn Demand Volume					
	High	Low	High	Low	High	Low
Signalized	A	A	B or C	B or C	B or C	B or C
Unsignalized	A	A	C	B	B or C	B

Left Turn Lane Storage Length, Condition A:

N/A

Feet

Condition B:

N/A

Feet

Condition C:

N/A

Feet

Required Left Turn Lane Storage Length:

N/A

Feet

Additional Findings:

N/A

Additional Comments / Justifications:

hypothetical analysis of combined volumes is maximal conservative approach

Turn Lane Warrant and Length Analysis Workbook

STUDY LOCATION AND ANALYSIS INFORMATION

Municipality:	Radnor	Analysis Date:	4/15/2020
County:	Delaware County	Conducted By:	FT
PennDOT Engineering District:	6	Checked By:	SB Approach
		Agency/Company Name:	FTA
Intersection & Approach Description: Stafford Ave & Site Driveway			
Analysis Period:	2024	Number of Approach Lanes:	1
Design Hour:	PM Peak Hour	Undivided or Divided Highway:	Undivided
Intersection Control:	Unsignalized	Type of Analysis:	Right Turn Lane
Posted Speed Limit (MPH):	25	Left or Right-Turn Lane Analysis?:	Right Turn Lane
Type of Terrain:	Level		

VOLUME CALCULATIONS

Left Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	Yes	0	0.0%	N/A
	Through	-	0	0.0%	N/A
	Right	No	0	0.0%	N/A
Opposing	Left	No	0	0.0%	N/A
	Through	-	0	0.0%	N/A
	Right	Yes	0	0.0%	N/A

Advancing Volume: N/A
Opposing Volume: N/A
Left Turn Volume: N/A
% Left Turns in Advancing Volume: N/A

Right Turn Lane Volume Calculations					
Movement		Include?	Volume	% Trucks	PCEV
Advancing	Left	No	0	0.0%	N/A
	Through	-	249	5.0%	256
	Right	-	4	0.0%	4

Advancing Volume: 260
Right Turn Volume: 4

TURN LANE WARRANT FINDINGS

Left Turn Lane Warrant Findings	Right Turn Lane Warrant Findings
Applicable Warrant Figure: N/A	Applicable Warrant Figure: Figure 9
Warrant Met?: N/A	Warrant Met?: No

TURN LANE LENGTH CALCULATIONS

Intersection Control:	Unsignalized
Design Hour Volume of Turning Lane:	4
Cycles Per Hour (Assumed):	60
Cycles Per Hour (If Known):	
Average # of Vehicles/Cycle:	N/A

PennDOT Publication 46, Exhibit 11-6

Type of Traffic Control	Speed (MPH)					
	25-35		40-45		50-60	
	Turn Demand Volume					
	High	Low	High	Low	High	Low
Signalized	A	A	B or C	B or C	B or C	B or C
Unsignalized	A	A	C	B	B or C	B

Right Turn Lane Storage Length, Condition A: N/A Feet
Condition B: N/A Feet
Condition C: N/A Feet
Required Right Turn Lane Storage Length: N/A Feet

Additional Findings: N/A

Additional Comments / Justifications:

George W. Broseman
Direct Dial: (610) 941-2459
Direct Fax: (610) 684-2005
Email: gbroseman@kaplaw.com

December 10, 2020

VIA FIRST CLASS MAIL

Stephen Norcini
Township Engineer
Township of Radnor
301 Iven Avenue
Wayne, PA 19087

**RE: Preliminary Land Development Application #2020-LD-08 and #2020-LD-09 –
Eagle Road Hamilton Estate; Conditional Use Application – 208 & 228 Strafford
Avenue and 18 Forrest Lane – Hamilton Estate
Our Reference: 15709-1**

Dear Mr. Norcini:

In accordance with Radnor Township Code provisions, I am enclosing the following documents on behalf of the Dorrance H. Hamilton 3/15/1996 Trust, applicant in the above matter, in advance of the upcoming public meetings:

1. A copy of the notice sent by this office to property owners in the vicinity of the property utilizing the addresses provided to us by Radnor Township; and
2. Original United States Postal Service 3877 forms, (Blue Bell Post Office) date-stamped December 9, 2020.

Should you have any questions, please do not hesitate to contact me.

Sincerely,



George W Broseman

GWB:sl
Enclosure

December 10, 2020

VIA FIRST CLASS MAIL (PROOF OF MAILING)

RE: Preliminary Land Development - Plan Land Development Application #2020-LD-08 and #2020-LD-09 - Eagle Road Hamilton Estate; Conditional Use Application – 208 & 228 Strafford Avenue and 18 Forrest Lane - Hamilton Estate

Dear Neighbor,

We have submitted an application to Radnor Township for the approval of a preliminary land development plan to subdivide the above property into 9 single family lots and construct 9 single family homes along Eagle Road.

We have also submitted a Conditional Use Application for 208 & 228 Strafford Avenue, and 18 Forrest Lane, to construct 41 Townhomes under the Density Modification provisions in §280.90 of the Radnor Township Zoning Code.

These plans are available for public viewing in the Engineering Department. The Land Development Plans for Eagle Road and the Conditional Use Application for Strafford Road will be reviewed by the Planning Commission at a scheduled meeting on **Monday December 21, 2020**:

This Planning Commission meeting will begin at 6:00 P.M. Currently, these meetings are being held via Zoom.

Sincerely,



George W. Broseman

GWB:sl



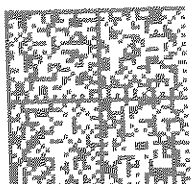
Firm Mailing Book For Accountable Mail

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Name and Address of Sender

- Check type of mail or service
- | | |
|--|---|
| <input type="checkbox"/> Adult Signature Required | <input type="checkbox"/> Priority Mail Express |
| <input type="checkbox"/> Adult Signature Restricted Delivery | <input type="checkbox"/> Registered Mail |
| <input type="checkbox"/> Certified Mail | <input type="checkbox"/> Return Receipt for Merchandise |
| <input type="checkbox"/> Certified Mail Restricted Delivery | <input type="checkbox"/> Signature Confirmation |
| <input type="checkbox"/> Collect on Delivery (COD) | <input type="checkbox"/> Signature Confirmation |

Affix Stamp Here
(for additional copies of this
Postmark with Date of Rec)



US POSTAGE
\$249.18
CORRECTION
ZIP 19422
12/09/2020
034A 0081800912

USPS Tracking/Article Number

1.

YOUNG LESLIE L
632 W BEECHTREE LN
WAYNE, PA 19087

.50 1.50

2.

YOST MICHELE M & JONATHAN P
526 W BEECHTREE LA
WAYNE, PA 19087

.50 .43

3.

BOSTWICK WENDY A
520 W BEECH TREE LN
WAYNE, PA 19087

.50 .43

4.

PERROT PAUL J
514 W BEECHTREE LN
WAYNE, PA 19087

.50 .43

5.

HODGEMAN MARK
527 W BEECH TREE LANE
WAYNE, PA 19087

.50 .43

6.

WILLIS MICHAEL G & SAWYER J KANAN
25 FOREST RD
WAYNE, PA 19087

.50 .43

7.

TOEBE JOHN W & PATRICIA
19 FOREST RD
WAYNE, PA 19087

.50 .43

8.

GEAGAN MICHAEL J & DONYA
15 FOREST RD
WAYNE, PA 19087

.50 .43

Total Number of Pieces
Listed by Sender 8

Total Number of Pieces
Received at Post Office

4.00 4.51

249.18

1



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation
☐ Insured Mail

Affix Stamp Here
(for additional copies of this receipt,
Postmark with Date of Receipt.)

USPS Tracking/Article Number

LEE BRIAN E & ELIZABETH W
11 FOREST RD
WAYNE, PA 19087

Postage (Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
-----------------------------	-----------------	----------------------------	---------------	-------------------	---------	----------	--------	--------	--------	----------	--------

1.

BROWNE DENNIS E
10 FOREST RD
WAYNE, PA 19087

.50	.43										
-----	-----	--	--	--	--	--	--	--	--	--	--

2.

KRIEDEL DAVID
530 W BEECHTREE LA
WAYNE, PA 19087

.50	.43										
-----	-----	--	--	--	--	--	--	--	--	--	--

3.

KOZUL MARGARET A
524 W BEECHTREE LANE
WAYNE, PA 19087

.50	.43										
-----	-----	--	--	--	--	--	--	--	--	--	--

4.

EDKINS THOMAS J
518 W BEECH TREE LANE
WAYNE, PA 19087

.50	.43										
-----	-----	--	--	--	--	--	--	--	--	--	--

5.

BROWN RUTH A
510 W BEECHTREE LN
WAYNE, PA 19087

.50	.43										
-----	-----	--	--	--	--	--	--	--	--	--	--

6.

PAHOMOV GEORGE S ETAL
525 W BEECHTREE LN
WAYNE, PA 19087

.50	.43										
-----	-----	--	--	--	--	--	--	--	--	--	--

7.

WALTERS LINDA
23 FOREST RD
WAYNE, PA 19087

.50	.43										
-----	-----	--	--	--	--	--	--	--	--	--	--

8.

Total Number of Pieces Listed by Sender
8

Total Number of Pieces Received at Post Office

4.00	3.44										
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Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation Restricted Delivery
☐ Insured Mail ☐ Restricted Delivery
☐ Priority Mail

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

Addressee (Name, Street, City, State, & ZIP Code™)

Postage (Extra Service) Fee Handling Charge Actual Value if Registered Insured Value Due Sender if COD ASR Fee ASRD Fee RD Fee RR Fee SC Fee SCRD Fee SH Fee

1.

FULTON NEAL
6 FOREST RD
WAYNE, PA 19087

121 INVESTMENTS LLC

705 PONDVIEW WAY
DOWNTOWN, PA 19335

2.

MELTZER ARTHUR H & DEBRA V

528 W BEECHTREE LN
WAYNE, PA 19087

3.

MARTIN JAMES W & EKATERINI K

522 W BEECHTREE LN
WAYNE, PA 19087

4.

RHINE MICHAEL S & ANNA M

516 W BEECHTREE LN
WAYNE, PA 19087

5.

BROWN MICHAEL J & DONNA S

523 W BEECHTREE LANE
RADNOR, PA 19087

6.

MCCLYMONT ALFRED & MCCLYMONT DONNA

21 FOREST ROAD
WAYNE, PA 19087

7.

HALLAM TREVOR J & JACQUELINE A

17 FOREST RD
WAYNE, PA 19087

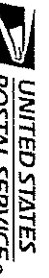
8.

Total Number of Pieces Listed by Sender 8

Total Number of Pieces Received at Post Office

4.00 3.44

3



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Insured Mail
☐ Priority Mail Express
☐ Registered Mail
☐ Return Receipt for Merchandise
☐ Signature Confirmation
☐ Signature Confirmation

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

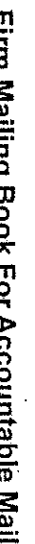
USPS Tracking/Article Number	Postage (Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
1. CELLUCCI BERNADETTE 13 FOREST RD WAYNE, PA 19087	.50	.43										
2. DELONG C FEDERICK & PAULA T 14 FOREST ROAD WAYNE, PA 19087	.50	.43										
3. JUNIOR JAY L 16 FOREST ROAD WAYNE, PA 19087	.50	.43										
4. SMITH JENNIFER D 515 W BEECHTREE LA WAYNE, PA 19087	.50	.43										
5. ALFONSO PEDRO & ALICIA 507 W BEECH TREE LN WAYNE, PA 19087	.50	.43										
6. LOFTUS ANTHONY P & MARY J 502 W BEECHTREE LANE WAYNE, PA 19087	.50	.43										
7. LIND ROGER B & MONICA 344 S DEVON AVENUE WAYNE, PA 19087	.50	.43										
8. Total Number of Pieces Listed by Sender: 8	4.00	3.44										

DEC - 9 2020

PA 19422

OKW

4



Check type of mail or service

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

Check type of mail or service

<input type="checkbox"/> Adult Signature Required	<input type="checkbox"/> Priority Mail Express
<input type="checkbox"/> Adult Signature Restricted Delivery	<input type="checkbox"/> Registered Mail
<input type="checkbox"/> Certified Mail	<input type="checkbox"/> Return Receipt for Merchandise
<input type="checkbox"/> Certified Mail Restricted Delivery	<input type="checkbox"/> Signature Confirmation
<input type="checkbox"/> Collect on Delivery (COD)	<input type="checkbox"/> Signature Confirmation Restricted Delivery
<input type="checkbox"/> Insured Mail	
<input type="checkbox"/> Restricted Mail	

USPS Tracking/Article Number

BEACOM DANIEL ROGER & JENNIFER HINZ
332 S DEVON AVE
WAYNE, PA 19382

SPOSATO RICHARD S & VINCENT P &
STEPHANIE
219 SUGARTOWN RD
WAYNE, PA 19087

CARNUNTUM ASSOCIATES LP
550 AMERICAN AVE #1
KING OF PRUSSIA, PA 19406

FORSTER ROBERT S JR AND
BERNADETTE C
18 FOREST ROAD
WAYNE, PA 19087

BOWER JEFFREY E & DEBORAH E
513 W BEECHTREE LANE
WAYNE, PA 19087

KERPIUS EDWARD D & LORI B
509 W BEECHTREE LN
WAYNE, PA 19087

HILLMAN MARK H & ABIGAIL N
505 W BEECHTREE LN
WAYNE, PA 19087

MATTIONI MICHAEL & JILLIAN T
200 MARSHALL ST
KENNETT SQUARE, PA 19348

Total Number of Pieces Listed by Sender	Total Number of Pieces Received at Post Office
1	1
2	2
3	3
4	4
5	5
6	6
7	7
8	8
9	9
10	10
11	11
12	12
13	13
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100	100



4.00 3.44

Handling Charge - if Registered and over \$50,000 in value	
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Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery			
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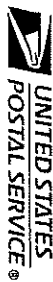
Special Handling

9077 180100Z JAN 17 /BAGG 1 AF 21

PSN 7530-02-000-9098

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5



KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

- Check type of mail or service
- | | |
|--|---|
| <input type="checkbox"/> Adult Signature Required | <input type="checkbox"/> Priority Mail Express |
| <input type="checkbox"/> Adult Signature Restricted Delivery | <input type="checkbox"/> Registered Mail |
| <input type="checkbox"/> Certified Mail | <input type="checkbox"/> Return Receipt for Merchandise |
| <input type="checkbox"/> Certified Mail Restricted Delivery | <input type="checkbox"/> Signature Confirmation |
| <input type="checkbox"/> Collect on Delivery (COD) | <input type="checkbox"/> Signature Confirmation Restricted Delivery |
| <input type="checkbox"/> Insured Mail | |

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

DALLAS ROBERT L & SUSAN A
340 S DEVON AVE
WAYNE, PA 19086

LOTT ANDREW PETER & NANCY WISE
328 S DEVON AVE
WAYNE, PA 19087

ZAHM NATHAN C & ASHLEY J
619 W BEECHTREE LA
WAYNE, PA 19087

CONDELLO MARK W & ANNE E
511 W BEECHTREE LN
WAYNE, PA 19087

ALFONSO PEDRO & ALICIA
507 W BEECH TREE LN
WAYNE, PA 19087

LOFTUS ANTHONY P & MARY J
502 W BEECHTREE LANE
WAYNE, PA 19087

FLETCHER FREDERICK B & KATHLEEN
4 FOREST RD
WAYNE, PA 19087

GANZ ADI & GLICK MIRIAM B
2 FOREST RD
WAYNE, PA 19087

Total Number of Pieces
Listed by Sender

8

Total Number of Pieces
Received at Post Office

PS Form 3877, January 2017 (Page 1 of 2)

PSN 7530-02-000-9098

Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
.50	.43	Handling Charge - if Registered and over \$50,000 in value										
.50	.43					Adult Signature Required						
.50	.43					Adult Signature Restricted Delivery						
.50	.43					Restricted Delivery						
.50	.43					Return Receipt						
.50	.43					Signature Confirmation						
.50	.43					Signature Confirmation Restricted Delivery						
.50	.43					Special Handling						
4.00	3.44											

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6



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

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☐ Adult Signature Restricted Delivery ☐ Registered Mail
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☐ Collect on Delivery (COD) ☐ Signature Confirmation Restricted Delivery
☐ Insured Mail

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Postmark with Date of Receipt.

USPS Tracking/Article Number

RICHTER WALLACE E F ETUX
336 S DEVON AVE
WAYNE, PA 19087

Postage (Extra Service) Fee

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

.50 .43

SPOSATO RICHARD S & VINCENT P & STEPHANIE

219 SUGARTOWN RD

WAYNE, PA 19087

.50 .43

CARNUNTUM ASSOCIATES LP

650 AMERICAN AVE #1

KING OF PRUSSIA, PA 19406

.50 .43

WU JING

389 MORRIS RD

WAYNE, PA 19087

.50 .43

HENDERSON KATHERINE

4 CLOVER LA

WAYNE, PA 19087

.50 .43

FALCONE VINCENTO

375 MORRIS RD

WAYNE, PA 19087

.50 .43

CARMICHAEL SARA & SILVERMAN JEREMY

10 CLOVER LA

WAYNE, PA 19087

.50 .43

MCGARRIGLE WILLIAM M & ELIZABETH M

100 MONUMENT AVE

MALVERN, PA 19355

.50 .43

Postage (Extra Service) Fee

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery

Special Handling

4.00 3.44

Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

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Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

.50 .43

Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

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SCRD Fee

SH Fee

.50 .43

Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

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.50 .43

Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

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Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

Insured Value

Due Sender if COD

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Handling Charge - if Registered and over \$50,000 in value

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SCRD Fee

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Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

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Due Sender if COD

ASR Fee

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Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

Insured Value

Due Sender if COD

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Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

Insured Value

Due Sender if COD

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SC Fee

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Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

Insured Value

Due Sender if COD

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Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

Insured Value

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Handling Charge - if Registered and over \$50,000 in value

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Insured Value

Due Sender if COD

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Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

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Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

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Due Sender if COD

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Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

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Due Sender if COD

ASR Fee

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Handling Charge - if Registered and over \$50,000 in value

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ASR Fee

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Handling Charge - if Registered and over \$50,000 in value

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Due Sender if COD

ASR Fee

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Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

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Handling Charge - if Registered and over \$50,000 in value

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Insured Value

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SC Fee

SCRD Fee

SH Fee

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Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

.50 .43

Handling Charge - if Registered and over \$50,000 in value

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee</



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation
☐ Insured Mail

Affix Stamp Here
(for additional copies of this receipt)
Postmark with Date of Receipt.

USPS Tracking/Article Number

SCOTT ROSEMARY H
16 CLOVER LN
WAYNE, PA 19087

Postage (Extra Service) Fee

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

CARBINE MARIANNE

357 MORRIS RD
WAYNE, PA 19087

.50 .43

NASS CHRISTINE M & PHILLIPS ROBERT M
22 CLOVER LA
WAYNE, PA 19087

.50 .43

SPOSATO RICHARD S & VINCENT P &
STEPHANIE
219 SUGARTOWN RD
WAYNE, PA 19087

.50 .43

U LING & SHUM ADRIAN Y

383 MORRIS RD
WAYNE, PA 19087

.50 .43

LORINE DAVID J

381 MORRIS RD
RADNOR, PA 19087

.50 .43

MOOSEHEAD PROPERTIES LLC
P.O. BOX 814
WAYNE, PA 19087

.50 .43

RICHTER MARK C & JESSICA POLAND
367 MORRIS RD
WAYNE, PA 19087

.50 .43

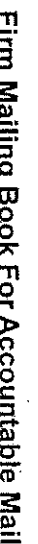
Total Number of Pieces Listed by Sender
8

Total Number of Pieces Received at Post Office

4.00 3.44

Complete in Ink

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Check type of mail or service

<input type="checkbox"/> Adult Signature Required	<input type="checkbox"/> Priority Mail Express
<input type="checkbox"/> Adult Signature Restricted Delivery	<input type="checkbox"/> Registered Mail
<input type="checkbox"/> Certified Mail	<input type="checkbox"/> Return Receipt for Merchandise
<input type="checkbox"/> Certified Mail Restricted Delivery	<input type="checkbox"/> Signature Confirmation
<input type="checkbox"/> Collect on Delivery (COD)	<input type="checkbox"/> Signature Confirmation
<input type="checkbox"/> Insured Mail	<input type="checkbox"/> Signature Confirmation

Affix Stamp Here
(for additional copies of this receipt)
Postmark with Date of Receipt.

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Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD)

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

COMPLETE INTERIORS INC
400 SAINT DAVIDS ROAD
WAYNE, PA 19087

Postage (Extra Service) Fee

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

1.

LAMAGRO ROSS RICHARD
302 SOUTH DEVON AVE
WAYNE, PA 19087

.50

.43

2.

MATTHEWS MICHAEL T & OMALLEY
AIDEEN M
337 S DEVON AVE
WAYNE, PA 19087

.50

.43

3.

MACERA JOHN A & MICHAEL J
325 S DEVON AVE
WAYNE, PA 19087

.50

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4.

MULLIN JOHN B & CAROLE A
317 S DEVON RD
WAYNE, PA 19087

.50

.43

5.

EGGER SHERMAN G
953 CLOVERLY RD
BERWYN, PA 19312

.50

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6.

FINKEL LAWRENCE H & FRANCES A
312 ROCKLAND ROAD
WAYNE, PA 19087

.50

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7.

MAGNAMARA DAVID M & LUCIANA L
26 CLOVER LN
WAYNE, PA 19087

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8.

[Handwritten signature]

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Total Number of Pieces Listed by Sender
8

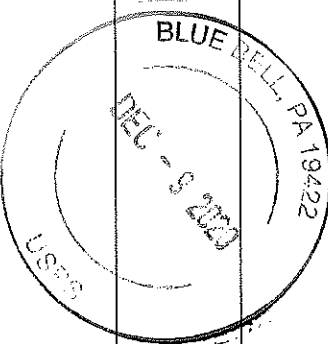
Total Number of Pieces Received at Post Office

Postmaster, Per (Name of receiving employee)

4.00 3.44

Complete in Ink

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Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation Restricted Delivery
☐ Insured Mail ☐ Priority Mail

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number	Postage (Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
1. SULKOWSKI THEODORE S 316 ROCKLAND RD WAYNE, PA 19087	.50	.43										
2. CANIGLIA RAYMOND ETUX 335 MORRIS RD WAYNE, PA 19087	.50	.43										
3. DANTONIO NICHOLAS & DONNA M 30 CLOVER LANE WAYNE, PA 19087	.50	.43										
4. BLACK ELIZABETH A 316 S DEVON AVE WAYNE, PA 19087	.50	.43										
5. GRIFFIN DEVIN P & GRACE CAHILL 304 S DEVON AVE WAYNE, PA 19087	.50	.43										
6. MAGRATH LUCY J 341 S DEVON AVE WAYNE, PA 19087	.50	.43										
7. BRENNICK MICHAEL J 2937 HAVERFORD RD ARDMORE, PA 19003	.50	.43										
8. MULLIN JOHN B & CAROLE A 317 S DEVON RD WAYNE, PA 19087	.50	.43										
Total Number of Pieces Listed by Sender	8											
Total Number of Pieces Received at Post Office												

Handling Charge - if Registered and over \$50,000 in value

Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery

Special Handling

4.00 3.44

Complete in ink

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910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

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☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation Restricted Delivery
☐ Insured Mail

Affix Stamp Here
(for additional copies of this receipt).
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USPS Tracking/Article Number

ZHOU WEIDONG DAN & JIANG HAIYAN
1427 WYNNEMOOR WAY
FT WASHINGTON, PA 19034

Postage (Extra Service) Fee

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

Postage (Extra Service) Fee

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

Postage (Extra Service) Fee

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

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SCRD Fee

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Postage (Extra Service) Fee

Handling Charge

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Insured Value

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Postage (Extra Service) Fee

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SC Fee

SCRD Fee

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Postage (Extra Service) Fee

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

Postage (Extra Service) Fee

Handling Charge



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- | | |
|--|---|
| <input type="checkbox"/> Adult Signature Required | <input type="checkbox"/> Priority Mail Express |
| <input type="checkbox"/> Adult Signature Restricted Delivery | <input type="checkbox"/> Registered Mail |
| <input type="checkbox"/> Certified Mail | <input type="checkbox"/> Return Receipt for Merchandise |
| <input type="checkbox"/> Certified Mail Restricted Delivery | <input type="checkbox"/> Signature Confirmation |
| <input type="checkbox"/> Collect on Delivery (COD) | <input type="checkbox"/> Signature Confirmation |
| <input type="checkbox"/> Insured Mail | |

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number	Postage (Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
1.												
MARTIN DAVID J 20 MORRIS CIR WAYNE, PA 19087	.50	.43										
2.												
RUSSO THOMAS P & CANFAROTTA MARCELLA 8 MORRIS CIR WAYNE, PA 19087	.50	.43										
3.												
CALVARESE VINCENT L & JOANNE C 121 CALVARESE LA WAYNE, PA 19087	.50	.43										
4.												
BUCCI BARBARA A 311 LIBERTY LN WAYNE, PA 19087	.50	.43										
5.												
MORRISSEY WILLIAM L SR 317 LIBERTY LANE WAYNE, PA 19087	.50	.43										
6.												
HORVATH DAVID A 328 ROCKLAND RD WAYNE, PA 19087	.50	.43										
7.												
COX GREGORY 4 DONNA LA WAYNE, PA 19087	.50	.43										
8.												
BRENNAN EDWARD G & JENNIFER B 9 MORRIS CIR WAYNE, PA 19087	.50	.43										

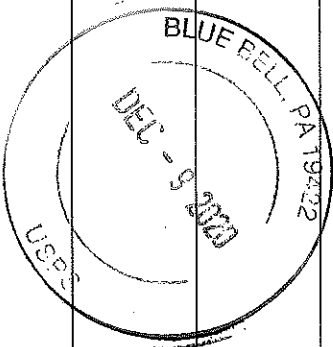
Total Number of Pieces Listed by Sender

8

Total Number of Pieces Received at Post Office

4.00 3.44

Postmaster, Per (Name of receiving employee)

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15



Name and Address of Sender

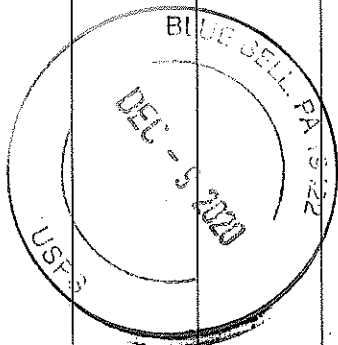
KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation
☐ Insured Mail

Affix Stamp Here
(for additional copies of this receipt,
Postmark with Date of Receipt.)

USPS Tracking/Article Number	Postage (Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
1. JOHNSTONE DUNCAN & MERRILEE 13 MORRIS CIR WAYNE, PA 19087	.50	.43										
2. MITCHELL JAMES F 21 MORRIS CIR WAYNE, PA 19087	.50	.43										
3. CURLEY DONALD E & CAROL 16 MORRIS CIR WAYNE, PA 19087	.50	.43										
4. RUSSELL LOIS ANN 4 MORRIS CIRCLE WAYNE, PA 19087	.50	.43										
5. YANG SOYOUNG 307 LIBERTY LN WAYNE, PA 19087	.50	.43										
6. GARMAN WILLIAM F & KATHLEEN 313 LIBERTY LN RADNOR, PA 19087	.50	.43										
7. CLEARY SUSAN 319 LIBERTY LN WAYNE, PA 19087	.50	.43										
8. PACKER KEVIN 332 ROCKLAND RD WAYNE, PA 19087	.50	.43										



Total Number of Pieces Listed by Sender 8

Total Number of Pieces Received at Post Office

Postmaster, Per (Name of receiving employee)

4.00 3.44

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16



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD)

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(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number		Postage (Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
1.	KLOSS DANIEL C 13 ROCKLAND CIR WAYNE, PA 19087	.50	.43										
2.	BORTNER THOMAS & SINEAD 11 ROCKLAND CIR WAYNE, PA 19087	.50	.43										
3.	DORNEY MATTHEW F & COMLY MEREDITH L 7 ROCKLAND CIR WAYNE, PA 19087	.50	.43										
4.	PARETCHAN ROBERT ETUX 24 MORRIS CIRCLE WAYNE, PA 19087	.50	.43										
5.	HAMILTON RUSSELL S & JUNE M 12 MORRIS CIR WAYNE, PA 19087	.50	.43										
6.	BASSIRI HAMID & ASHLYN EATON 303 LIBERTY LA WAYNE, PA 19087	.50	.43										
7.	CARR MARGARET M 309 LIBERTY LN WAYNE, PA 19087	.50	.43										
8.	HEMWELL JAMES 315 LIBERTY LA WAYNE, PA 19087	.50	.43										
Total Number of Pieces Listed by Sender		4.00	3.44										
Total Number of Pieces Received at Post Office		8											

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Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Insured Mail
☐ Signature Confirmation

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

PANELLI ELIZABETH H TRUSTEES
331 EDGEHILL RD
WAYNE, PA 19087

MIGHTY TWIN OAKS LTD
354 MORRIS RD
WAYNE, PA 19087

PUGH SARAH E
362 MORRIS RD
WAYNE, PA 19087

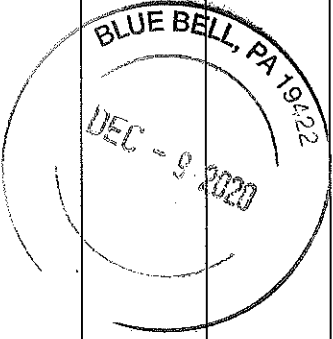
COMPLETE INTERIORS INC
400 SAINT DAVIDS ROAD
WAYNE, PA 19087

FIELDEN GEORGE & NANCY
284 S DEVON AVE
WAYNE, PA 19087

JIANG GRACE E & XIONG DIANE D
302 MORRIS RD
WAYNE, PA 19087

DIGNAZIO SANDORO E
328 MORRIS RD
WAYNE, PA 19087

DIGNAZIO FLORENCE D
333 MORRIS RD
WAYNE, PA 19087



Total Number of Pieces Listed by Sender

8

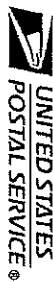
Total Number of Pieces Received at Post Office

4.00 3.44

Complete in Ink

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Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD)

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(for additional copies of this receipt)
Postmark with Date of Receipt.

USPS Tracking/Article Number

BRODERICK REBECCA E
344 MORRIS RD
WAYNE, PA 19087

Postage (Extra Service) Fee

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

1.

STICKEL TERRIL
865 BABB CIRCLE
WAYNE, PA 19087

.50 .43

2.

PAOLANTONIO DAVID & MARGUERITE
360 MORRIS RD
WAYNE, PA 19087

.50 .43

3.

ROSE RYAN & ANMARIE L
364 MORRIS RD
WAYNE, PA 19087

.50 .43

4.

UNIATOWSKI SUE E TRUST
280 S DEVON AVE
WAYNE, PA 19087

.50 .43

5.

SWITUCHA STEPHEN CHRISTOPHER
288 S DEVON AVE
WAYNE, PA 19087

.50 .43

6.

PALANIAPPAN SARAVANAN MUTHUSAMY
304 MORRIS RD
WAYNE, PA 19087

.50 .43

7.

PEPE DENNIS J & MARY E
332 MORRIS RD
WAYNE, PA 19087

.50 .43

8.

Total Number of Pieces Listed by Sender

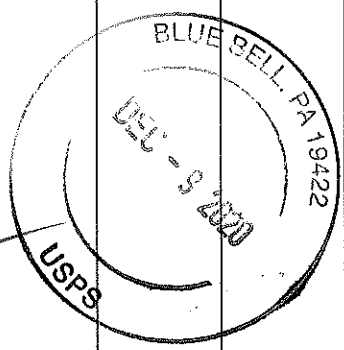
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Postmaster, Per (Name of receiving employee)

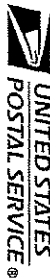
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Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation
☐ Insured Mail

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Postmark with Date of Receipt.

USPS Tracking/Article Number

VOEGELE JOSEPH T & COLLEEN W
342 MORRIS RD
WAYNE, PA 19087

Postage (Extra Service) Fee

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

1.

FINSDORE WENDY H

346 MORRIS ROAD

WAYNE, PA 19087

.50 .43

2.

MIGHTY TWIN OAKS LTD

364 MORRIS RD

WAYNE, PA 19087

.50 .43

3.

FRANCESCHINI SHERYL

368 MORRIS RD

WAYNE, PA 19087

.50 .43

4.

MCCARTHY JOHN

372 MORRIS RD

WAYNE, PA 19087

.50 .43

5.

ROCHE BARBARA

384 MORRIS RD

WAYNE, PA 19087

.50 .43

6.

7.

KARNIK SHANTANU

410 MORRIS RD

WAYNE, PA 19087

.50 .43

8.

LEVIN JAMES G & RAYMOND M

PO BOX 196

WAYNE, PA 19087

.50 .43

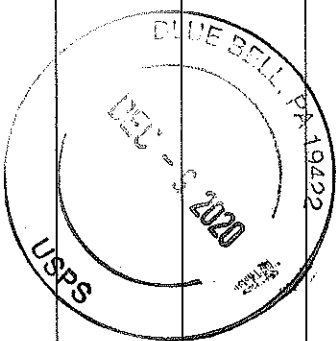
Total Number of Pieces Listed by Sender
8

Total Number of Pieces Received at Post Office

4.00 3.44

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Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation Restricted Delivery
☐ Insured Mail ☐ Restricted Mail

Affix Stamp Here
(for additional copies of this receipt)
Postmark with Date of Receipt.

USPS Tracking/Article Number

1.

WAYNEFORD ASSO C/O PESKIN REALTY
CO
P O BOX 176
HAVERFORD, PA 19041

2.

BRAXTON FAMILY LTD PARTNERSHIP
620 W LANCASTER AV
WAYNE, PA 19087

3.

KODY JAMES
530 W LANCASTER AVE
WAYNE, PA 19087

4.

GALVIN DONNA TRUSTEE - ERIN
IRREVOCABLE TRUST
620 RIGHTERS FERRY RD
BALA CYNWYD, PA 19004
KARAKELIAN GARABET & CONSTANCE
1747 SPRING HOUSE RD
CHESTER SPRINGS, PA 19425

5.

RIO MARCOS
376 MORRIS RD
WAYNE, PA 19087

7.

PIETRINI FRANCIS A
111 E CHURCH RD
KING OF PRUSSIA, PA 19406

8.

PIETRINI FRANCIS A
111 E CHURCH ROAD
KING OF PRUSSIA, PA 19406

Total Number of Pieces
Listed by Sender

8

Total Number of Pieces
Received at Post Office

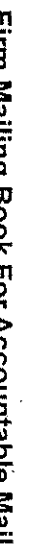
PS Form 3877, January 2017 (Page 1 of 2)

Complete in Ink

Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
.50	.43	Handling Charge - if Registered and over \$50,000 in value										
.50	.43					Adult Signature Required						
.50	.43					Adult Signature Restricted Delivery						
.50	.43					Restricted Delivery						
.50	.43					Return Receipt						
.50	.43					Signature Confirmation						
.50	.43					Signature Confirmation Restricted Delivery						
.50	.43					Special Handling						
4.00	3.44											

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Chickadee

Order Type	Order Name	Service
<input type="checkbox"/>	Adult Signature Required	<input type="checkbox"/> Priority Mail Express
<input type="checkbox"/>	Adult Signature Restricted Delivery	<input type="checkbox"/> Registered Mail
<input type="checkbox"/>	Certified Mail	<input type="checkbox"/> Return Receipt for Merchandise
<input type="checkbox"/>	Certified Mail Restricted Delivery	<input type="checkbox"/> Signature Confirmation
<input type="checkbox"/>	Collect on Delivery (COD)	<input type="checkbox"/> Signature Confirmation
<input type="checkbox"/>	Insured Mail	<input type="checkbox"/> Signature Confirmation

Affix Stamp Here
(for additional copies of this receipt)
Postmark with Date of Receipt

WAWA FOOD MARKET #291
250 BALTIMORE PK
WAWA, PA 19063

- ☐ Priority Mail Express
- ☐ Registered Mail
- ☐ Return Receipt for Merchandise
- ☐ Signature Confirmation
- ☐ Signature Confirmation Restricted

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PS Form **3877**, January 2017 (Page 1 of 2)
PSN 7530-02-000-9098

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Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation
☐ Insured Mail

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(for additional copies of this receipt)
Postmark with Date of Receipt

USPS Tracking/Article Number

1. WAYNEFORD ASSOC c/o BASCO
333 E LANCASTER AVE
WYNNWOOD, PA 19096

2. BRAXTON FAMILY LIMITED PARTNERSHIP
620 W LANCASTER AV
WAYNE, PA 19087

3. FERRILL ANTIMO A & ANTIMO
P.O. Box 44
Sea Isle City, NJ 08243

4. WAYNE CORNER LLC
110 BRITTANY WAY
BLUE BELL, PA 19422

5. DIORIO STEPHEN J & SUSAN
29 FOX RIDGE DR
MALVERN, PA 19355

6. NRP REALTY LLC
1125 KAOLIN RD
KENNETT SQUARE, PA 19348

7. DIORIO STEPHEN J & SUSAN L
512 W LANCASTER AVE
WAYNE, PA 19087

8. GARTNER BRENT S DDS & KARENA A
512 W LANCASTER AVE UNIT D-1
WAYNE, PA 19087

Total Number of Pieces
Listed by Sender

Total Number of Pieces
Received at Post Office

8

4.00 3.44

Handling Charge - if Registered and over \$50,000 in value

Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

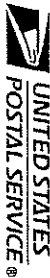
Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery

Special Handling

24



Name and Address of Sender

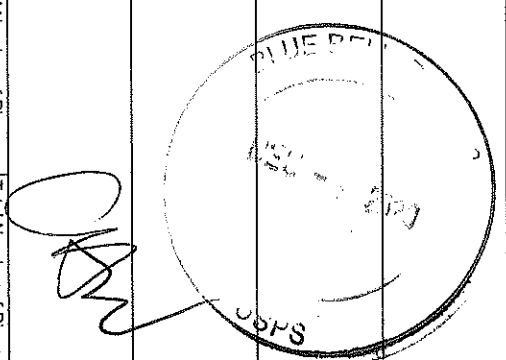
KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Insured Mail
☐ Signature Confirmation

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Postmark with Date of Receipt.

USPS Tracking/Article Number	Postage (Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
1. GALLGHER TRAIL HOLDING CO LP 110 GALLAGHER ROAD WAYNE, PA 19087	.50	.43										
2. EVANS DOUGLAS R & DIANE P 115 GALLAGHER RD WAYNE, PA 19087	.50	.43										
3. EVANS KATHRYN R 117 GALLAGHER RD WAYNE, PA 19087	.50	.43										
4. ROSCIOLI GABRIEL 119 GALLAGHER RD WAYNE, PA 19087	.50	.43										
5. GIBBONS PATRICK V JR 96 CONESTOGA RD WAYNE, PA 19087	.50	.43										
6. ANDERSON THOMAS 106 CONESTOGA RD WAYNE, PA 19087	.50	.43										
7. BUI TAN KIM & NGUYEN PHUONG THI 108 CONESTOGA RD WAYNE, PA 19087	.50	.43										
8. NRP REALTY LLC 1125 KAOLIN RD KENNETT SQUARE, PA 19348	.50	.43										
Total Number of Pieces Listed by Sender: 8	4.00	3.44										



25



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation
☐ Insured Mail

Affix Stamp Here
(for additional copies of this receipt)
Postmark with Date of Receipt.

USPS Tracking/Article Number

1.

PHILA AUTH FOR IND DEVELOP
2845 E WESTMORELAND ST
PHILADELPHIA, PA 19134

2.

M R A INC (PA CORPORATION)
500 W LANCASTER AVE
WAYNE, PA 19087

3.

MACHHERSON KATELYN K & RODRIGUEZ
JORDAN M
100 CONESTOGA RD
WAYNE, PA 19087

4.

CONNOR KERRY
119 GALLAGHER RD
WAYNE, PA 19087

5.

KLAPP JOSEPH W
1401 BAYVIEW DR #1
FT LAUDERDALE, FL 333041659

6.

123 125 GALLAGHER RD LP
PO BOX 321
WAYNE, PA 19087

7.

FORMAN STANLEY A
110 CONESTOGA RD
WAYNE, PA 19087

8.

DIORIO STEPHEN J & SUSAN L
29 FOX RIDGE DR
MALVERN, PA 19355

Total Number of Pieces
Listed by Sender

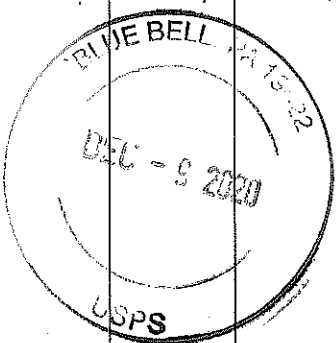
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Total Number of Pieces
Received at Post Office

Postmaster, Per (Name of receiving employee)

4.00 3.44

Complete in Ink

Privacy Notice: For more information on USPS privacy policies, visit usps.com/privacypolicy.

Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
.50	.43	Handling Charge - if Registered and over \$50,000 in value										
.50	.43					Adult Signature Required						
.50	.43					Adult Signature Restricted Delivery						
.50	.43					Restricted Delivery						
.50	.43					Return Receipt						
.50	.43					Signature Confirmation						
.50	.43					Signature Confirmation Restricted Delivery						
.50	.43					Special Handling						

24



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery

Affix Stamp Here
(For additional copies of this receipt,
Postmark with Date of Receipt.)

USPS Tracking/Article Number

OLUMITOUHE KARIM & GHANIA
120 CONESTOGA RD
WAYNE, PA 19087

1.

SHERTZ MICHAEL
126 CONESTOGA RD-C
WAYNE, PA 19087

2.

RICHARDSON JOSEPH ETUX
268 S DEVON AVE
WAYNE, PA 19087

3.

STATTS JOHN G & CATHLEEN M
850 SPRING BANK LA
WAYNE, PA 19087

4.

SPARANGO BENEDICT
P O BOX 428
VILLANOVA, PA 19085

CALFO LEE & DEGRAFF LAURA
127 CONESTOGA RD
WAYNE, PA 19087

7.

DAVIDSON LINDA A
114 CONESTOGA RD
WAYNE, PA 19087

8.

SMITH LYNN
118 CONESTOGA ROAD
WAYNE, PA 19087

Total Number of Pieces
Listed by Sender

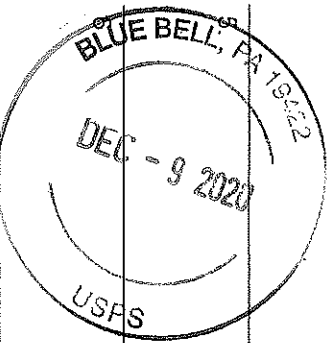
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Total Number of Pieces
Received at Post Office

4.00 3.44

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Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
.50	.43	Handling Charge - if Registered and over \$50,000 in value										
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.50	.43					Adult Signature Restricted Delivery						
.50	.43					Restricted Delivery						
.50	.43					Return Receipt						
.50	.43					Signature Confirmation						
.50	.43					Signature Confirmation Restricted Delivery						
.50	.43					Special Handling						



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD)

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

BREWINGTON DAVID VANCE & MARION J
468 SHARON DR
WAYNE, PA 19087

Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
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1. FISHER CARTER JR & NANCY L
122 CONESTOGA RD
WAYNE, PA 19087

.50 .43

2. LEMMA FRANK A & MARY D
145 GALLAGHER RD
WAYNE, PA 19087

.50 .43

3. HEDGECOCK BRIANA
266 S DEVON RD
WAYNE, PA 19087

.50 .43

4. MATTIOLI EDWARD K & SUZANNE
262 S DEVON AVE
WAYNE, PA 19087

.50 .43

5. FERRILLI ANTIMO JR
133 SUGARTOWN RD
WAYNE, PA 19087

.50 .43

6. MORRIS ROBERT L
126 CONESTOGA RD
WAYNE, PA 19087

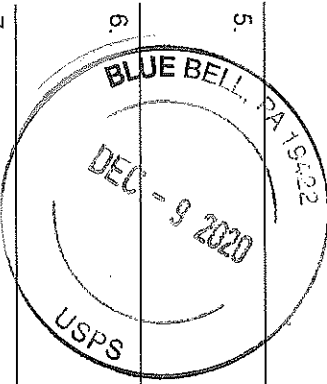
.50 .43

7. KENNY FRANCIS
864 MARTIN AVE
BRYN MAWR, PA 19010

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8. Postmaster, Per (Name of receiving employee)

4.00 3.44



Total Number of Pieces Listed by Sender

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Total Number of Pieces Received at Post Office

Postmaster, Per (Name of receiving employee)

4.00 3.44

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KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

<input type="checkbox"/> Adult Signature Required	<input type="checkbox"/> Priority Mail Express
<input type="checkbox"/> Adult Signature Restricted Delivery	<input type="checkbox"/> Registered Mail
<input type="checkbox"/> Certified Mail	<input type="checkbox"/> Return Receipt for Merchandise
<input type="checkbox"/> Certified Mail Restricted Delivery	
<input type="checkbox"/> Collect on Delivery (COD)	<input type="checkbox"/> Signature Confirmation

Affix Stamp Here
(for additional copies of this receipt)
Postmark with Date of Receipt.

MELMARK HOME INC
2600 WAYLAND RD
BERWYN, PA 19312

BERWYN, PA 19312

MACH 133 GALL LLC
210 WINDERMERE AVE
WAYNE, PA 19087

WAYNE, PA 19087

CARR MARGARET M & CHARLES H
10 CHRISTINE LN
MEDIA, PA 19063

MEDIA, PA 19062

LEIGHTON WAYNE D & SUSAN C
425 BOXWOOD RD
ROSEMONT, PA 19010

ROSEMONT, PA 19010

DETWILER GEO S & ETUX
270 S DEVON AVE
WAYNE, PA 19087

WAYNE, PA 13087

BRUNO ELIZABETH & LOUIS REVOCABLE
TRUST
340 ROCKLAND RD
WAYNE, PA 19087

WAYNE, PA 19087

HOLTMAN MATTHEW CHARLES & BLAIR
ESTER
260 S DEVON AVE
WAYNE, PA 19087

WAYNE, PA 19087

HANSELL GREGORY R & SAMANTHA S
129 CONESTOGA RD
WAYNE, PA 19087

WAYNE, PA 19087

Total Number of Pieces Listed by Sender	Total Number of Pieces Received at Post Office
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PS Form 3877, January 2017 (Page 1 of 2)

PSN 7530-02-000-9098

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4.00 3.44

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Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation
☐ Insured Mail

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Postmark with Date of Receipt.

USPS Tracking/Article Number

3D BODY WORKS SPRING CITY LLC
300 W BOOT RD
WEST CHESTER, PA 19380

DENHAM PATRICIA K & ISAAC
121 CONESTOGA RD
WAYNE, PA 19087

STRAUSS ISADORE ETAL & BESCO CO
631 W GERMANTOWN PIKE #103
PLYMOUTH MEETING, PA 19462

FUCHS O JOHN
200 EAGLE RD SUITE 218
WAYNE, PA 19087

VILLAGE ASSOCIATES
555 CROTON RD #120
KING OF PRUSSIA, PA 19406
SHERBY AARON
P.O. BOX 242
DEVON, PA 19333

GDK REAL ESTATE LP
119 CONESTOGA RD
WAYNE, PA 19087

LUKOIL NORTH AMERICA LLC
505 5th AVENUE - 9th FLOOR
NEW YORK, NY 10017

Total Number of Pieces Listed by Sender

8

Total Number of Pieces Received at Post Office

Postmaster, Per (Name of receiving employee)

4.00 3.44

Complete in Ink

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Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation Restricted Delivery
☐ Insured Mail
☐ Priority Mail

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Postmark with Date of Receipt.

USPS Tracking/Article Number

605 W LANCASTER TP
4427 SPRUCE ST
PHILADELPHIA, PA 19104

Postage (Extra Service) Fee

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

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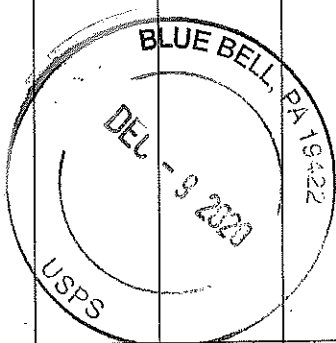
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5.

6.

7.

8.



K KIMMEL - GDK REAL ESTATE LP
P.O. BOX 155
Souderton, PA 18964

NEFF ANDREW S & DAVID S
222 LANCASTER AVE
LANCASTER, PA 17602

ALBED SHIRLEY A
1176 THOMAS RD
WAYNE, PA 19087
523 RESTAURANT LP
620 RIGHTERS FERRY RD
BALA CYNWYD, PA 19004

EAGLE GREEN LP
620 RIGHTERS FERRY RD
BALA CYNWYD, PA 19004

DOMINICK GALVANI DARLENE
HOROCOFKY
1146 MADISON AVE APT 2B
PROSPECT PARK, PA 19076

SELLERS HEDWIG
118 FAIRFIELD LN
STRAFFORD, PA 19087

Total Number of Pieces Listed by Sender

8

Total Number of Pieces Received at Post Office

8

4.00 3.44

Complete in Ink

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Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery

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(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

HILLIARD JEFF
308 ECHO VALLEY LANE
NEWTOWN SQUARE, PA 19073

KATHY ZHANG

909 Carroll Road
Wynnewood, PA 19096

CAPPELLI JOHN A JR & CARMIA M

3095 MARK TERR
BROOMALL, PA 19008

TORUNYAN ARAKSI

130 FAIRFIELD LA
WAYNE, PA 19087

CAPPELLI JOHN A & CARMIA M

3095 MARK TERR
BROOMALL, PA 19008

THROUP JOHN

1753 AQUEETONG ROAD
NEW HOPE, PA 18938

REESE ALAN J

817-A NORTH TANNEY STREET
PHILADELPHIA, PA 19130

OUR LADY OF ASSUMPTION CHURCH

135 FAIRFIELD LANE
WAYNE, PA 19087

Postmaster, Per (Name of receiving employee)

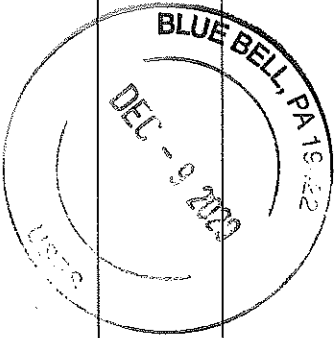
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Total Number of Pieces
Listed by Sender

Total Number of Pieces
Received at Post Office

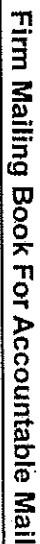
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Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
.50	.43	Handling Charge - if Registered and over \$50,000 in value										
.50	.43	Adult Signature Required										
.50	.43	Adult Signature Restricted Delivery										
.50	.43	Restricted Delivery										
.50	.43	Return Receipt										
.50	.43	Signature Confirmation										
.50	.43	Signature Confirmation Restricted Delivery										
.50	.43	Special Handling										

93



Check type of mail or service

<input type="checkbox"/> Adult Signature Required	<input type="checkbox"/> Priority Mail Express
<input type="checkbox"/> Adult Signature Restricted Delivery	<input type="checkbox"/> Registered Mail
<input type="checkbox"/> Certified Mail	<input type="checkbox"/> Return Receipt for Merchandise
<input type="checkbox"/> Certified Mail Restricted Delivery	<input type="checkbox"/> Signature Confirmation
<input type="checkbox"/> Collect on Delivery (COD)	

Affix Stamp Here
(for additional copies of this receipt)
Postmark with Date of Receipt.

136 FAIRFIELD LN
WAYNE, PA 19087

KURANDA FINANCIAL MTG INC
617 W LANCASTER AVE
WAYNE, PA 19087

ZWEIER JOHN C & LORENZO JOHN A
57 S PROVIDENCE RD
WALLINGFORD, PA 19086

HILLIARD JEFFREY
308 ECHO VALLEY LA
NEWTOWN SQUARE, PA 19073

CIANCI ANNA M
135 HEARTSIDE DR
WINSTON SALEM, NC 27104

CAPPELLI JOHN A JR & CARMIA
102 FAIRFIELD LN
STRAFFORD, PA 19087

OULOUHOJIAN JUDITH & MCCHALE MARY
203 HASTINGS BLVD
BROOMALL, PA 19008

KIM TAE H & JEONG SEUNG HEH
140 FAIRFIELD LA
WAYNE, PA 19087

Total Number of Pieces Listed by Sender	Total Number of Pieces Received at Post Office
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Handling Charge - if Registered and over \$50,000 in value

Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery

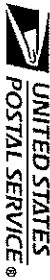
Special Handling

PS Form 3877, January 2017 (Page 1 of 2)
PSN 7530-02-000-9098

Complete in Ink

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34



Name and Address of Sender
KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service
☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Priority Mail Express
☐ Registered Mail
☐ Return Receipt for Merchandise

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

OCNNOR CONSTANCE M & IZZO PETER J
H/H
15 FORREST LN
STRAFFORD, PA 19087

CHAWLA GAGAN & SANNU
21 GRANT LN
WAYNE, PA 19087

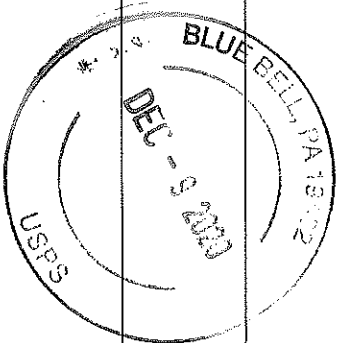
SCHERI STEVEN W & MEGAN G
9 GRANT LN
WAYNE, PA 19087

HAMILTON FORRANCE H TRUSTEE
200 EAGLE ROAD, SUITE 316
WAYNE, PA 19087

GRAY JEREMY P & CONTRERAS TERESA
PAOLA ZARATE
231 STRAFFORD AVE
WAYNE, PA 19087

WHITMORE CHRISTOPHER M
428 WEST AVE
WAYNE, PA 19087
GDK REAL ESTATE LP c/o KEVIN KIMMEL
P O BOX 544
FREDERICK, PA 19435

HAMILTON S MATTHEWS V JR
200 EAGLE RD #316
WAYNE, PA 19087



Total Number of Pieces Listed By Sender
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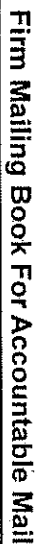
Total Number of Pieces Received at Post Office

Postmaster, Per (Name of receiving employee)

Complete in Ink

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KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

<input type="checkbox"/> Adult Signature Required	<input type="checkbox"/> Priority Mail Express
<input type="checkbox"/> Adult Signature Restricted Delivery	<input type="checkbox"/> Registered Mail
<input type="checkbox"/> Certified Mail	<input type="checkbox"/> Return Receipt for Merchandise
<input type="checkbox"/> Certified Mail Restricted Delivery	<input type="checkbox"/> Signature Confirmation
<input type="checkbox"/> Collect on Delivery (COD)	

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(for additional copies of this receipt)
Postmark with Date of Receipt.

SCHUDA JOSEPH M & FRANCES E
14 FORREST LN
WAYNE, PA 19087

2. JOHNSTON ALFRED J III & GLORIA
ROMEIKA
17 GRANT LANE
WAYNE, PA 19087

MAHONEY MARY A
5 GRANT LANE
WAYNE, PA 19087

4. THOMAS ROBERT I & TRAUDI
344 STRAECHD AVE

HAMILTON DORRANCE H
200 EAGLE RD
WAYNE, PA 19087

6.  SANTUCCI, RAYMOND L
390 W LANCASTER AV
WAYNE, PA 19087

MORRISSEY CAREN E
13 GRANT LA
WAYNE, PA 19087

SAREEN ANUJEET & TARA
205 STRAFFORD AVE
WAYNE, PA 19087

Total Number of Pieces Listed by Sender	Total Number of Pieces Received at Post Office
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PS Form 3877, January 2017 (Page 1 of 2)

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4.00 3.44

Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
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.50	.43					Adult Signature Required						
.50	.43					Adult Signature Restricted Delivery						
.50	.43					Restricted Delivery						
.50	.43					Return Receipt						
.50	.43					Signature Confirmation						
.50	.43					Signature Confirmation Restricted Delivery						
.50	.43					Special Handling						



Name and Address of Sender

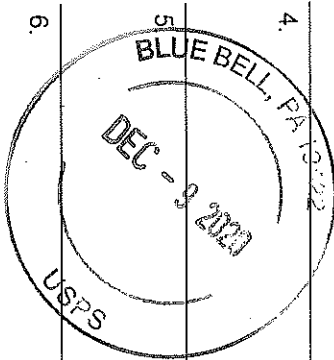
KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation Restricted Delivery
☐ Insured Mail

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number	Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
1. BROOKS JOHN 227 STRAFFORD AVE WAYNE, PA 19087	.50	.43											
2. WHITMORE CHRISTOPHER M 428 WEST AVE WAYNE, PA 19087	.50	.43											
3. GDK REAL ESTATE 392 W LANCASTER AVE WAYNE, PA 19087	.50	.43											
4. INFOFINDERS INC 1157 PUGH RD WAYNE, PA 19087	.50	.43											
5. EPSTEIN NEIL & AMY 431 WEST AVE WAYNE, PA 19087	.50	.43											
6. BELL BROOKE B & CHRISTOPHER W 420 WEST AVE WAYNE, PA 19087	.50	.43											
7. CHEN HONG JEN & ALICE 425 WEST AVE WAYNE, PA 19087	.50	.43											
8. CAREY DAVID 408 WEST AVE WAYNE, PA 19087	.50	.43											
Total Number of Pieces Listed by Sender	4.00	3.44											
Total Number of Pieces Received at Post Office	8												



[Handwritten signature]



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

MENSER MARK S & JANET W
417 WEST AVE
WAYNE, PA 19087

tion

CRAVEN ROBERT H III
317 EAGLE RD
WAYNE, PA 19087

PECK DAVID C & LYNN BRIDGET
307 EAGLE RD
WAYNE, PA 19087

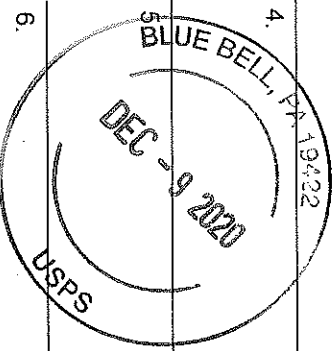
GRAHAM WILLARD & KRISTIN
508 W BEECH TREE LN
WAYNE, PA 19087

MCCALLION WM R & JEAN
426 BEECHTREE LN
WAYNE, PA 19087

PLOTKIN MOLLE
429 W BEECHTREE LA
WAYNE, PA 19087

MORGAN STACEY P & THEODORE S
424 WEST AVE
WAYNE, PA 19087

ANDERSON MARK R
717 HOMESTEAD AVENUE
HAVERTOWN, PA 19083



Total Number of Pieces
Listed by Sender 8

Total Number of Pieces
Received at Post Office

Postmaster, Per (Name of receiving employee)

4.00 3.44

Complete in Ink

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Name and Address of Sender
KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service
☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Priority Mail Express
☐ Registered Mail
☐ Return Receipt for Merchandise
☐ Signature Confirmation

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

MAXFIELD HONOR C
7 F LEXINGTON LANE EST
WEST PALM BEACH, FL 33418

MCCARTHY ARACELI U
421 WEST AVE
WAYNE, PA 19087

HUCKELBRIDGE ARTHUR A JR TR
2629 WELLINGTON RD
CLEVELAND HTS, OH 44118

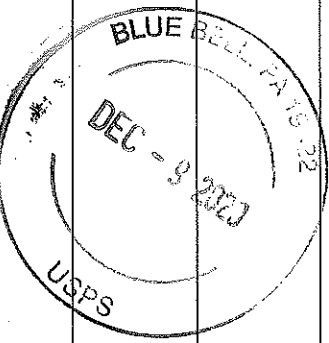
MAHONEY TARA
311 EAGLE RD
WAYNE, PA 19087

COOPER PAUL J
305 EAGLE RD
WAYNE, PA 19087

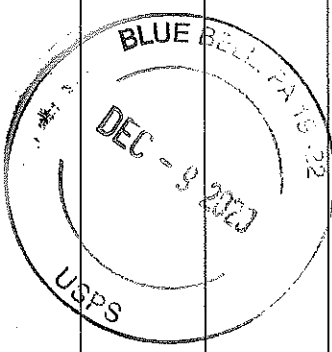
HARRIS MARGARETE
428 W BEECHTREE LA
WAYNE, PA 19087

NUTTALL BECKY A
427 W BEECH TREE LN
WAYNE, PA 19087

MUNTZ CHRISTINA & TIMOTHY
429 WEST AVE
WAYNE, PA 19087



USPS Tracking/Article Number		MAXFIELD HONOR C 7 F LEXINGTON LANE EST WEST PALM BEACH, FL 33418														
1.		MCCARTHY ARACELI U 421 WEST AVE WAYNE, PA 19087														
2.		HUCKELBRIDGE ARTHUR A JR TR 2629 WELLINGTON RD CLEVELAND HTS, OH 44118														
3.		MAHONEY TARA 311 EAGLE RD WAYNE, PA 19087														
4.		COOPER PAUL J 305 EAGLE RD WAYNE, PA 19087														
5.		HARRIS MARGARETE 428 W BEECHTREE LA WAYNE, PA 19087														
6.		NUTTALL BECKY A 427 W BEECH TREE LN WAYNE, PA 19087														
7.		MUNTZ CHRISTINA & TIMOTHY 429 WEST AVE WAYNE, PA 19087														
8.																
Total Number of Pieces Listed by Sender		Total Number of Pieces Received at Post Office		Postmaster, Per (Name of receiving employee)												
8				4.00 3.44												



Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
.50	.43	Handling Charge - if Registered and over \$50,000 in value										
.50	.43											
.50	.43											
.50	.43											
.50	.43											
Adult Signature Required												
Adult Signature Restricted Delivery												
Restricted Delivery												
Return Receipt												
Signature Confirmation												
Signature Confirmation Restricted Delivery												
Special Handling												

Total Number of Pieces Listed by Sender: 8

Total Number of Pieces Received at Post Office

Postmaster, Per (Name of receiving employee)

Complete in Ink

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Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation Restricted Delivery
☐ Insured Mail

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Postmark with Date of Receipt.

USPS Tracking/Article Number

1. BONVETTI JOHN
416 WEST AVE
WAYNE, PA 19087

2. HORNER STEPHEN R & CAROLYN N
423 WEST AVE
RADNOR, PA 19087

3. TORUNIAN VAHE & FISCHER MARY BETH
404 WEST AVE
WAYNE, PA 19087

4. SZELA WOJCIECH & CECILE
319 EAGLE RD
WAYNE, PA 19087

5. HUSSON-CHARLET VALERIE
309 EAGLE RD
WAYNE, PA 19087

6. RICHARDSON STEVEN & GRACE
301 EAGLE RD
WAYNE, PA 19087

7. OGRADY CHRISTOPHER & ANNE WALKER
504 W BEECHTREE LA
WAYNE, PA 19087

8. O'NEILL BRENDAN & KIMBERLY
122 E BEECHTREE LA
WAYNE, PA 19087

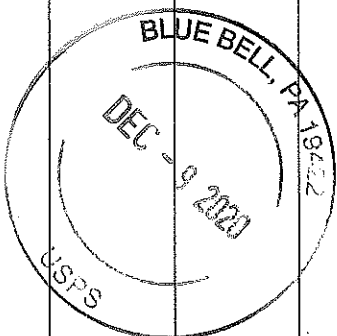
Total Number of Pieces
Listed by Sender

8

Total Number of Pieces
Received at Post Office

4.00 3.44

Complete in Ink

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Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
.50	.43	Handling Charge - if Registered and over \$50,000 in value										
.50	.43					Adult Signature Required						
.50	.43					Adult Signature Restricted Delivery						
.50	.43					Restricted Delivery						
.50	.43					Return Receipt						
.50	.43					Signature Confirmation						
.50	.43					Signature Confirmation Restricted Delivery						
.50	.43					Special Handling						
4.00	3.44											

410



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- | | |
|--|---|
| <input type="checkbox"/> Adult Signature Required | <input type="checkbox"/> Priority Mail Express |
| <input type="checkbox"/> Adult Signature Restricted Delivery | <input type="checkbox"/> Registered Mail |
| <input type="checkbox"/> Certified Mail | <input type="checkbox"/> Return Receipt for Merchandise |
| <input type="checkbox"/> Certified Mail Restricted Delivery | <input type="checkbox"/> Signature Confirmation |
| <input type="checkbox"/> Collect on Delivery (COD) | <input type="checkbox"/> Signature Confirmation Restricted Delivery |
| <input type="checkbox"/> Insured Mail | |
| <input type="checkbox"/> Priority Mail | |

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USPS Tracking/Article Number

ROSATO ROBERT T
425 W BEECHTREE LN
WAYNE, PA 19087

Postage (Extra Service) Fee

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

1.

HARPER PATRICK D & KATHERINE A

.50 .43

423 W BEECHTREE LN

.50 .43

WAYNE, PA 19087

.50 .43

MCPHERSON FRANK H JR & MARGARET

.50 .43

233 FRETZ ROAD

.50 .43

TELFORD, PA 18969

.50 .43

HOOVER REED A & KATHLEEN C

.50 .43

418 W BEECHTREE RD

.50 .43

WAYNE, PA 19087

.50 .43

HIRSCHLER MARC D & ANDREAL

.50 .43

415 W BEECHTREE LN

.50 .43

WAYNE, PA 19087

.50 .43

RENZI MARIE J

.50 .43

409 W BEECH TREE LANE

.50 .43

WAYNE, PA 19087

.50 .43

BURGMAYER PAUL R & SHARON J N

.50 .43

405 W BEECH TREE LN

.50 .43

WAYNE, PA 19087

.50 .43

CREATO HEATHER A

.50 .43

203 WEST AVE

.50 .43

WAYNE, PA 19087

.50 .43

WAYNE, PA 19087

.50 .43

Total Number of Pieces
Listed by Sender

8

Total Number of Pieces
Received at Post Office

4.00 3.44

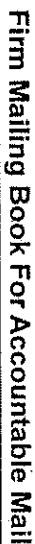
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PS Form 3877, January 2017 (Page 1 of 2)

PSN 7530-02-000-9098

41



Check type of mail or service

<input type="checkbox"/> Adult Signature Required	<input type="checkbox"/> Priority Mail Express
<input type="checkbox"/> Adult Signature Restricted Delivery	<input type="checkbox"/> Registered Mail
<input type="checkbox"/> Certified Mail	<input type="checkbox"/> Return Receipt for Merchandise
<input type="checkbox"/> Certified Mail Restricted Delivery	<input type="checkbox"/> Signature Confirmation
<input type="checkbox"/> Collect on Delivery (COD)	<input type="checkbox"/> Signature Confirmation Restricted Delivery
<input type="checkbox"/> Insured Mail	
<input type="checkbox"/> Priority Mail	

Affix Stamp Here
(for additional copies of this receipt)
Postmark with Date of Receipt.

CHRISTIANSEN LAUREN
211 WEST AVE
WAYNE, PA 19087

THOROUGHGOOD PRISCILLA

116 SPRUCE LA
PAOLI, PA 19301

DUDA LISA A
6 MARLYN CIR
WAYNE, PA 19087

YOSUA MICHAEL J & EMMA
422 BEECHTREE LN
WAYNE, PA 19087

GURIAN PATRICK L & GLEASON
KATHARINE A
419 W BEECHTREE LA
WAYNE, PA 19087

DOUGHERTY GERARD F & KATHLEEN M
416 W BEECHTREE LA
WAYNE, PA 19087

RONGIONE NICHOLAS M & MALONE
CONSTANCE A
413 W BEECHTREE LA
WAYNE, PA 19087

GIARDINELLI CHAD & SAMANTHA L
407 W BEECHTREE LA
WAYNE, PA 19087

4.00 3.44

Handling Charge - if Registered and over \$50,000 in value	
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Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

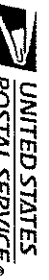
Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery

Special Handling

412



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD)

tion

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(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number	Postage (Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
1.	.50	.43										
2.	.50	.43										
3.	.50	.43										
4.	.50	.43										
5.	.50	.43										
6.	.50	.43										
7.	.50	.43										
8.	.50	.43										
Handling Charge - if Registered and over \$50,000 in value												
Adult Signature Required												
Adult Signature Restricted Delivery												
Restricted Delivery												
Return Receipt												
Signature Confirmation												
Signature Confirmation Restricted Delivery												
Special Handling												

Total Number of Pieces Listed by Sender 8

Total Number of Pieces Received at Post Office

8

4.00 3.44

Complete in Ink



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD)

Affix Stamp Here
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Postmark with Date of Receipt.

USPS Tracking/Article Number

BURGMAYER PAUL R & SHARON J N
405 W BEECH TREE LN
WAYNE, PA 19087

1.

BAKES SETH A & STROGEN KELLY A
201 WEST AVE
WAYNE, PA 19087

2.

TARLECKY ROZANNE T
209 WEST AVE
WAYNE, PA 19087

3.

SACKSTEDER BRIAN J & CARROLL
CHARLOTTE E
2 MARLYN CI
WAYNE, PA 19087

4.

FIRENG LAYTON
5 MARLYN CIR
WAYNE, PA 19087

5.

NOLAN PATRICK
217 WEST AVE
WAYNE, PA 19087

6.

GRUENER JAY L & ELAINE M
309 OVERHILL RD
WAYNE, PA 19087

7.

COOKE BRUCE R & ELIZABETH L
THOMPSON
315 OVERHILL RD
WAYNE, PA 19087

8.

Total Number of Pieces Listed by Sender 8

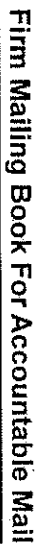
Postmaster, Per (Name of receiving employee)

4.00 3.44

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44



Check type of mail or service

<input type="checkbox"/> Adult Signature Required	<input type="checkbox"/> Priority Mail Express
<input type="checkbox"/> Adult Signature Restricted Delivery	<input type="checkbox"/> Registered Mail
<input type="checkbox"/> Certified Mail	<input type="checkbox"/> Return Receipt for Merchandise
<input type="checkbox"/> Certified Mail Restricted Delivery	<input type="checkbox"/> Signature Confirmation
<input type="checkbox"/> Collect on Delivery (COD)	<input type="checkbox"/> Signature Confirmation
<input type="checkbox"/> Insured Mail	

Affix Stamp Here
(for additional copies of this receipt)
Postmark with Date of Receipt.

ZIEGENFUSE BRYAN & ELIZABETH
303 WEST AVE
WAYNE, PA 19087

WAYNE, PA 19087

GOGLIA ASHLEY MAHADY
319 OVERHILL RD
WAYNE, PA 19087

BALITSARIS MICHAEL
326 FAIRHILL ROAD
WYNNNEWOOD, PA 19096

LAMSBACK ROBERT BRIAN & ANGELA
MARIE
321 OVERHILL RD
WAYNE, PA 19087

BEITER, JEFFREY
4 PRINCETON RD
WAYNE, PA 19087

MATUCH PAUL & CHRISTIANE
323 WEST AVE
WAYNE, PA 19087

TRAINOR THOMAS J JR & RUTH A
331 OVERHILL RD
WAYNE, PA 19087

THOROUGHGOOD PRISCILLA
116 SPRUCE LANE
PAOLI, PA 19301

Postmaster, Per (Name of receiving employee),

PS Form 3877, January 2017 (Page 1 of 2)

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4.00 3.44

4.00 3.44

Handling Charge - if Registered and over \$50,000 in value	
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Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery

Special Handling

45



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD)

tion

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(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

1.

BOSTON MARC & MARIAH
311 OVERHILL RD
WAYNE, PA 19087

2.

MCGRATH JOHN W & KATHLEEN E
318 OVERHILL RD
WAYNE, PA 19087

3.

SEVERANCE ALEXANDER & SYLVIA M
320 OVERHILL RD
WAYNE, PA 19087

4.

SAVINI MATTHEW T
322 OVERHILL RD
WAYNE, PA 19087

5.

SCHNEIDER SANDRA M
324 OVERHILL RD
WAYNE, PA 19087

6.

STAPLES WILLIAM
323 OVERHILL RD
WAYNE, PA 19087

7.

DAMBLY KEITH J
326 OVERHILL ROAD
WAYNE, PA 19087

8.

CONNOLLY BRIAN J & ARIMOND DOROTHY M
328 OVERHILL RD
WAYNE, PA 19087

Total Number of Pieces Listed by Sender
8

Total Number of Pieces Received at Post Office

4.00 3.44

Handling Charge - if Registered and over \$50,000 in value

Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery

Special Handling

46



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation
☐ Signature Confirmation

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Postmark with Date of Receipt.

USPS Tracking/Article Number

BURNS CAROL A
325 WEST AVE
WAYNE, PA 19087

Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
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1.

HENRY JUSTIN & LAUREN BAUS
307 OVERHILL RD
WAYNE, PA 19087

.50 .43

2.

BROWN PHILIP & HOLLY
301 WEST AVE
WAYNE, PA 19087

.50 .43

3.

FACENDIA PAUL M & JOYCE E
317 OVERHILL RD
WAYNE, PA 19087

.50 .43

4.

SCHRAMM HENRY O & PATRICIA
309 WEST AVE
WAYNE, PA 19087

.50 .43

5.

MORRISSEY BRIAN & BRIGID
325 OVERHILL ROAD
WAYNE, PA 19087

.50 .43

6.

NEAGOE LEONARD & GINA
307 MIDWAY RD
WAYNE, PA 19087

.50 .43

7.

OKLEWICZ ROSS MICHAEL
329 OVERHILL RD
WAYNE, PA 19087

.50 .43

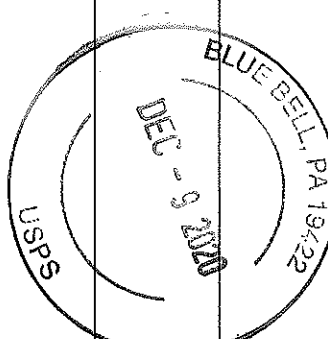
8.

Total Number of Pieces Listed by Sender
8

Total Number of Pieces Received at Post Office

4.00 3.44

Complete in Ink

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Handwritten signature: [Signature]

Handling Charge - if Registered and over \$50,000 in value

Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery

Special Handling

47



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

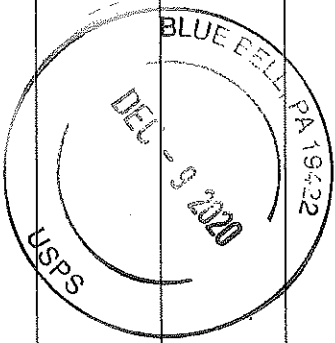
Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD)

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(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number	Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
1.													
2.													
3.													
4.													
5.													
6.													
7.													
8.													
Total Number of Pieces Listed by Sender 8													
Total Number of Pieces Received at Post Office 8													



[Handwritten signature]

Handling Charge - if Registered and over \$50,000 in value

Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery

Special Handling

48



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation Restricted Delivery
☐ Insured Mail

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Postmark with Date of Receipt.

USPS Tracking/Article Number

PARILLA JONATHAN M & DENISE
328 WEST AVE
WAYNE, PA 19087

Postage (Extra Service) Fee

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

1.

MASSARO CHRISTOPHER J & DEBORAH A
320 WEST AVE
WAYNE, PA 19087

2.

HENRY LAWRENCE E
316 WEST AVE
WAYNE, PA 19087

3.

PERTSCHUK MICHAEL & GRAVES JANET
15 CHESTNUT LANE
WAYNE, PA 19087

4.

KOO YUNMEE K
333 OVERHILL RD
WAYNE, PA 19087

5.

DELIZIA BRADLEY
337 WEST AVE
WAYNE, PA 19087

6.

GRAVES ERIC J
340 WEST AVE
WAYNE, PA 19087

7.

CONN JEFFREY & LINDA E
348 EAGLE ROAD
WAYNE, PA 19087

8.

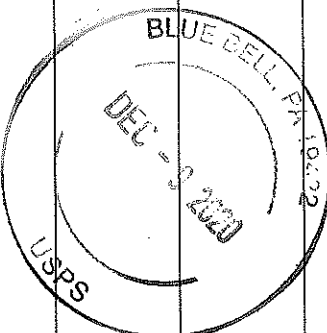
Total Number of Pieces Listed by Sender
8

Total Number of Pieces Received at Post Office

4.00 3.44

Complete in Ink

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Handwritten signature/initials.

50



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation Restricted Delivery
☐ Insured Mail ☐ Signature Confirmation Restricted Delivery
☐ Restricted Mail

Affix Stamp Here
(for additional copies of this receipt)
Postmark with Date of Receipt.

USPS Tracking/Article Number

1. GIROD PAULETTE M
405 WINDSOR AVE
WAYNE, PA 19087

2. DONCHES GREGORY & KIMBERLY
327 WINDSOR AVE
WAYNE, PA 19087

3. HARTMAN PETER A & BROWN JUDITH A
323 WINDSOR AVE
WAYNE, PA 19087

4. FREDERICKS JO ANNE T
319 WINDSOR AVE
WAYNE, PA 19087

5. ZULLINGER ROBERT L III & HEATHER K
312 WEST AVE
WAYNE, PA 19087

6. HUNT HOLBROOK PASSMORE & DIANE
MARIE
116 BANBURY WAY
WAYNE, PA 19087

7. HUNT HOLBROOK PASSMORE & DIANE
MARIE
116 BANBURY WAY
WAYNE, PA 19087

8. BARNES JAMES R & ELIZABETH H
112 BANBURY WAY
WAYNE, PA 19087

Total Number of Pieces Listed by Sender 8
Total Number of Pieces Received at Post Office

4.00 3.44

Handling Charge - if Registered and over \$50,000 in value

Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

Return Receipt

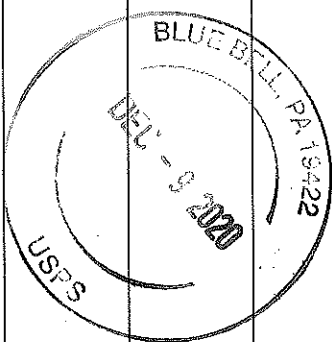
Signature Confirmation

Signature Confirmation Restricted Delivery

Special Handling

Complete in Ink

Privacy Notice: For more information on USPS privacy policies, visit usps.com/privacypolicy.



Handwritten signature



Name and Address of Sender

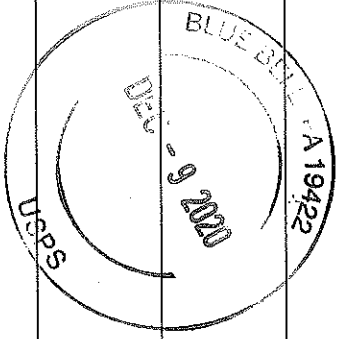
KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD)

Affix Stamp Here
(for additional copies of this receipt,
Postmark with Date of Receipt.)

USPS Tracking/Article Number	Postage (Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
1. NEW NAME LLC c/o MICHAEL McDONOUGH P O BOX 6091 CHARLOTTEVILLE, VA 22906	.50	.43										
2. MACRONE JOSEPH M & TRISHA R 107 BANBURY WAY WAYNE, PA 19087	.50	.43										
3. DENT JOHN G 113 BANBURY WAY WAYNE, PA 19087	.50	.43										
4. MORRIS DAVID W 295 E SWEDESFORD RD #170 WAYNE, PA 19087	.50	.43										
5. LOPEZ TOBIOLA TROSINI & BERNARD L 115 GROTON RD WAYNE, PA 19087	.50	.43										
6. MACGILLIVRAY ANGUS R & MCCARROLL KATHLEEN A 120 BELLEVUE AVE WAYNE, PA 19087	.50	.43										
7. GILMOUR MELANIE 111 BRYAN LA STOCKTON, NJ 08559	.50	.43										
8. ELISIO NICHOLAS C & SHEILA W 111 FRANCIS AVENUE WAYNE, PA 19087	.50	.43										
Total Number of Pieces Listed by Sender: 8	4.00	3.44										



Signature of Sender

52



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD)

ion

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

1.

METZGER RICHARD G & DEMORROW
DARLA L
307 WINDSOR AVE
WAYNE, PA 19087

2.

HINO MICHAEL S & MARGARET
109 BANBURY WAY
WAYNE, PA 19087

3.

LIEB THOMAS J & REBECCA A
124 S BELLEVUE AVE
WAYNE, PA 19087

4.

MCGRATH JOHN D & LAURIE E
119 FRANCIS AVE
WAYNE, PA 19087

5.

REID ELIZABETH
117 FRANCIS AVE
WAYNE, PA 19087

6.

AIKEN JOHN G
116 S BELLEVUE AVE
WAYNE, PA 19087

7.

NAZARSKI CAESAR L & DEBRAH I
114 S BELLEVUE AVE
WAYNE, PA 19087

8.

KELLY MATTHEW C & JULIE G
303 WINDSOR AVE
WAYNE, PA 19087

Total Number of Pieces
Listed by Sender

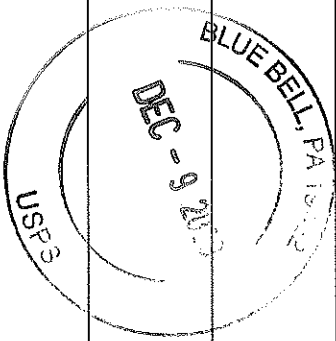
8

Total Number of Pieces
Received at Post Office

Postmaster, Per (Name of receiving employee)

4.00 3.44

Complete in Ink

Privacy Notice: For more information on USPS privacy policies, visit usps.com/privacypolicy.

Handling Charge - if Registered and over \$50,000 in value

Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery

Special Handling

53



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD)

Affix Stamp Here
(for additional copies of this receipt)
Postmark with Date of Receipt

USPS Tracking/Article Number

WEST NATHAN ARTHUR & LI HAOPING
105 BANBURY WAY
WAYNE, PA 19087

Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
---------	---------------------	-----------------	----------------------------	---------------	-------------------	---------	----------	--------	--------	--------	----------	--------

1.

DORN ERIC W & KATIE L
111 BANBURY WAY
WAYNE, PA 19087

.50 .43

2.

SPARANGO BENEDICT
P O BOX 428
VILLANOVA, PA 19085

.50 .43

3.

PRIMA DANIEL L & BAUER SANDRA
122 S BELLEVUE AVE
WAYNE, PA 19087

.50 .43

4.

DAVIDSON JENINE
115 FRANCIS AVE
WAYNE, PA 19087

.50 .43

5.

HUFF JASON
113 FRANCIS AVE
WAYNE, PA 19087

.50 .43

6.

201 WEST LP
1118 LAFAYETTE RD
WAYNE, PA 19087

.50 .43

7.

NOWAY ASSOCIATES
696 E LANCASTER AVE #303
ST DAVIDS, PA 19087

.50 .43

8.

Total Number of Pieces Listed by Sender
8

Total Number of Pieces Received at Post Office

4.00 3.44

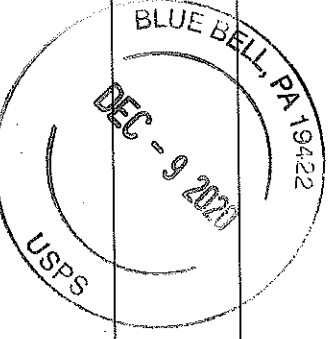
Postmaster, Per (Name of receiving employee)

PS Form 3877, January 2017 (Page 1 of 2)

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PSN 7530-02-000-9098



[Handwritten signature]

54



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD)

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

KARAKELIAN GARABET
1747 SPRING HOUSE RD
CHESTER SPRING, PA 19425

Postage	(Extra Service) Fee	Handling Charge	Actual Value If Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
---------	---------------------	-----------------	----------------------------	---------------	-------------------	---------	----------	--------	--------	--------	----------	--------

1. 365 INC A PA CORP
5 HARVEST LA
HAVERFORD, PA 19041

2. SAVAR SAEED & MITRA MANSOORY
306 WINDSOR AVE
WAYNE, PA 19087

3. RADNOR PROPERTIES CORP
571 CREEK RD
HARTSVILLE, PA 18974

4. LOCKYER ELLEN S & NIGEL S
308 WINDSOR AVE
WAYNE, PA 19087

5. HANAWAY WILLIAM L JR & LORRAINE
19 FARM RD
WAYNE, PA 19087

6. SCHRIEBER ERIC M & AMY W
20 FARM ROAD
WAYNE, PA 19087

7. LMP 353 LLC
555 E LANCASTER AVE STE 620
RADNOR, PA 19087

8.

Total Number of Pieces Listed by Sender 8
Total Number of Pieces Received at Post Office

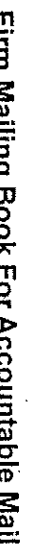
4.00 3.44

PS Form 3877, January 2017 (Page 1 of 2)

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55



Check type of mail or service

Adult Signature Required

☐ Adult Signature Required

☐ Adult Signature Restricted Delivered

☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail

☐ Adult Signature Required

☐ Adult Signature Restricted Delivery

☐ Certified Mail

☐ Certified Mail Restricted Delivery

☐ Collect on Delivery (COD)

☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Insured Mail

LEVINNE ROBERT G &
 11 LINCOLN AVE

☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Insured Mail

DELIVERY MAIL
LEVINE ROBERT G &
1 LINCOLN LA
WAYNE, PA 19087

☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Insured Mail

☒ **Delivery Method**
LEVINNE ROBERT G &
1 LINCOLN LA
WAYNE, PA 19087

209 WAYNE IPG ASSOC
70 PORTLAND RD
CONSHOHOCKEN, PA

☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Insured Mail
☐ Registered Mail

LEVINNE ROBERT G &
 1 LINCOLN LA
 WAYNE, PA 19087

209 WAYNE IPG ASSOC
 70 PORTLAND RD
 CONSHOHOCKEN, PA

301 WEST PARTNERS
 28 S WATERLOO RD

☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Insured Mail
☒ Delivery Mail
LEVINE ROBERT G &
1 LINCOLN LA
WAYNE, PA 19087

☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Insured Mail
☒ Priority Mail
 LEVINE ROBERT G &
 1 LINCOLN LA
 WAYNE, PA 19087

209 WAYNE IPG ASSOC
 70 PORTLAND RD
 CONSHOHOCKEN, PA
 301 WEST PARTNERS
 28 S WATERLOO RD
 DEVON, PA 19333

FIRST RESOURCE BAA
 1 E UWCHLAN AVE #4
 EXTON, PA 19341

☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Insured Mail
☒ ^{Priority Mail} LEVINE ROBERT G &
 1 LINCOLN LA
 WAYNE, PA 19087
 209 WAYNE IPG ASSOC
 70 PORTLAND RD
 CONSHOHOCKEN, PA
 301 WEST PARTNERS
 28 S WATERLOO RD
 DEVON, PA 19333
 FIRST RESOURCE BAY
 1 E UWCHLAN AVE #4
 EXTON, PA 19341
 BOUCHER NORMAND
 1-138 MONTROSE AVE
 ROSEMONT, PA 19011

☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Insured Mail
☒ ^{Priority Mail} LEVINE ROBERT G &
 1 LINCOLN LA
 WAYNE, PA 19087

209 WAYNE IPG ASSOC
 70 PORTLAND RD
 CONSHOHOCKEN, PA

301 WEST PARTNERS
 28 S WATERLOO RD
 DEVON, PA 19333

FIRST RESOURCE BANC
 1 E UWCHLAN AVE #4
 EXTON, PA 19341

BOUCHER NORMAND
 1-138 MONTROSE AVE
 ROSEMONT, PA 19011

ZELEZNIK SCOTT W
 7 FARM RD
 WAYNE, PA 19087

☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Insured Mail
☐ Priority Mail
 LEVINNE ROBERT G &
 1 LINCOLN LA
 WAYNE, PA 19087

209 WAYNE IPG ASSOC
 70 PORTLAND RD
 CONSHOHOCKEN, PA

301 WEST PARTNERS
 28 S WATERLOO RD
 DEVON, PA 19333

FIRST RESOURCE BANC
 1 E UWCHLAN AVE #4
 EXTON, PA 19341

BOUCHER NORMAND
 1-138 MONTROSE AVE
 ROSEMONT, PA 19011

ZELEZNIK SCOTT W
 7 FARM RD
 WAYNE, PA 19087

ZELTEN ROBERT A & S
 310 WINDSOR AVE
 WAYNE, PA 19087

☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Insured Mail
☒ Priority Mail
 LEVINNE ROBERT G &
 1 LINCOLN LA
 WAYNE, PA 19087
 209 WAYNE I PG ASSOC
 70 PORTLAND RD
 CONSHOHOCKEN, PA
 301 WEST PARTNERS
 28 S WATERLOO RD
 DEVON, PA 19333
 FIRST RESOURCE BAN
 1 E UWCHLAN AVE #4
 EXTON, PA 19341
 BOUCHER NORMAND
 1-138 MONTROSE AVE/RO
 ROSEMONT, PA 19011
 ZELEZNIK SCOTT W
 7 FARM RD
 WAYNE, PA 19087
 ZELTEN ROBERT A & S
 310 WINDSOR AVE
 WAYNE, PA 19087
 GALLAGHER PAUL F &
 312 WINDSOR AVE
 WAYNE, PA 19087

☐ Check type of mail or service
☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Insured Mail
☐ ~~Priority Mail~~
 LEVINE ROBERT G &
 1 LINCOLN LA
 WAYNE, PA 19087
 209 WAYNE I PG ASSOC
 70 PORTLAND RD
 CONSHOHOCKEN, PA
 301 WEST PARTNERS
 28 S WATERLOO RD
 DEVON, PA 19333
 FIRST RESOURCE BAN
 1 E UWCHLAN AVE #4
 EXTON, PA 19341
 BOUCHER NORMAND
 1-138 MONTROSE AVE/
 ROSEMONT, PA 19011
 ZELEZNIK SCOTT W
 7 FARM RD
 WAYNE, PA 19087
 ZELTEN ROBERT A & S
 310 WINDSOR AVE
 WAYNE, PA 19087
 GALLAGHER PAUL F &
 312 WINDSOR AVE
 WAYNE, PA 19087

☐ Other type of mail or service
☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Insured Mail

Delivery Mail
 LEVINE ROBERT G &
 1 LINCOLN LA
 WAYNE, PA 19087

209 WAYNE I/PG ASSO
 70 PORTLAND RD
 CONSHOHOCKEN, PA

301 WEST PARTNERS
 28 S WATERLOO RD
 DEVON, PA 19333

FIRST RESOURCE BA
 1 E UWCHLAN AVE #4
 EXTON, PA 19341

BOUCHER NORMAND
 1-138 MONTROSE AVE
 ROSEMONT, PA 19011

ZELEZNIK SCOTT W
 7 FARM RD
 WAYNE, PA 19087

ZELTEN ROBERT A & S
 310 WINDSOR AVE
 WAYNE, PA 19087

GALLAGHER PAUL F &
 342 WINDSOR AVE
 WAYNE, PA 19087

Complete in

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Postage	(Extra Service) Fee	Handling Charge	Actual Value If Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
.50	.43	Handling Charge - if Registered and over \$50,000 in value										
.50	.43											
.50	.43											
.50	.43											
.50	.43											
.50	.43											
.50	.43											
.50	.43											
.50	.43											
.50	.43											
Adult Signature Required												
Adult Signature Restricted Delivery												
Restricted Delivery												
Return Receipt												
Signature Confirmation												
Signature Confirmation Restricted Delivery												
Special Handling												

001



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation Restricted Delivery
☐ Collect on Delivery (COD) ☐ Signature Confirmation Restricted Delivery
☐ Insured Mail

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

1. DERSIMONIAN NISHAN & NANCY C
16 FARM ROAD
WAYNE, PA 19087

2. 115 STRAFFORD AVENUE LLC
110 N PHOENIXVILLE PIKE
MALVERN, PA 19355

3. HAMACH HOLDINGS
P.O. BOX 125
DEVON, PA 19333

4. ALBANY CHRISTOPHER W & JENNIFER M
304 WINDSOR AVE
WAYNE, PA 19087

5. SPIRE R/E DEVELOPMENT LLC
28 S WAERLOO RD #101
DEVON, PA 19333

6. HETH MOLLY M
9 FARM RD
WAYNE, PA 19087

7. MACKENZIE JONATHAN B & GRETCHEN M
15 FARM ROAD
WAYNE, PA 19087

8. HOLLAND TIMOTHY & MEREDITH
314 WINDSOR AVE
WAYNE, PA 19087

Total Number of Pieces
Listed by Sender

8

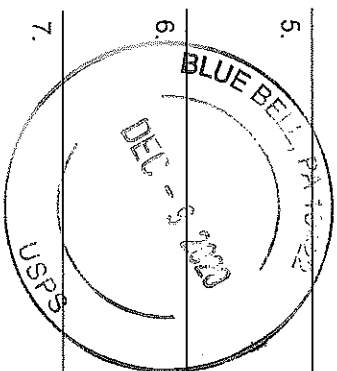
Total Number of Pieces
Received at Post Office

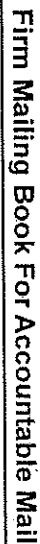
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Complete in Ink

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Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
.50	.43	Handling Charge - if Registered and over \$50,000 in value										
.50	.43					Adult Signature Required						
.50	.43					Adult Signature Restricted Delivery						
.50	.43					Restricted Delivery						
.50	.43					Return Receipt						
.50	.43					Signature Confirmation						
.50	.43					Signature Confirmation Restricted Delivery						
.50	.43					Special Handling						
4.00	3.44											





KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

<input type="checkbox"/> Adult Signature Required	<input type="checkbox"/> Priority Mail Express
<input type="checkbox"/> Adult Signature Restricted Delivery	<input type="checkbox"/> Registered Mail
<input type="checkbox"/> Certified Mail	<input type="checkbox"/> Return Receipt for Merchandise
<input type="checkbox"/> Certified Mail Restricted Delivery	<input type="checkbox"/> Signature Confirmation
<input type="checkbox"/> Collect on Delivery (COD)	<input type="checkbox"/> Signature Confirmation
<input type="checkbox"/> Insured Mail	

Affix Stamp Here
(for additional copies of this receipt)
Postmark with Date of Receipt.

MORGAN LESLIE F
12 FARM RD
WAYNE, PA 19087

ARC CBWNEPA001 LLC
200 DRYDEN RD #1100
DRESHER, PA 19025

FUCHS JR JOHN O
200 EAGLE RD SUITE 118
WAYNE, PA 19087

THREE SIXTY FIVE INC
331 S RADNOR CHESTER RD
VILLANOVA, PA 19085

GIBBONS PATRICK V & FRANCIS
137 LOWRYS LN
ROSEMONT, PA 19010

SHOCKET L YSA
143 CONESTOGA RD
WAYNE, PA 19087

WALTMAN DOUGLAS M & JESSICA
10 DOYLE ROAD
WAYNE, PA 19087

DANTONIO DOLORES & ANTHONY
500 E LANCASTER AVE #121A
ST DAVIDS, PA 19087

Total Number of Pieces Listed by Sender	Total Number of Pieces Received at Post Office
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PS Form 3877, January 2017 (Page 1 of 2)

Complete in Ink

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4.00 3.44

Handling Charge - if Registered and over \$50,000 in value	
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Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery

Special Handling

PSN 7530-02-000-9098

58



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation Restricted Delivery
☐ Insured Mail

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

LUKAS JOSEPH T & CONNOLLY SUSAN F
714 MOONRAKER CT
GALLOWAY, NJ 08205

Postage (Extra Service) Fee

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

1.

SONG KOOK & BANG KYUNG YUL
3533 DARBY RD
HAVERFORD, PA 19041

2.

WIERNAN ROBERTA M
101 LANTOGA RD
WAYNE, PA 19087

3.

DAMICANTONIO ROBERT
103 LANTOGA RD
WAYNE, PA 19087

4.

STRAUSS ISADORE c/o EQUITY RETAIL
BROKERS
101 W LLM ST #370
CONSHOHOCKEN, PA 19428

5.

WALKER FAMILY PROPERTIES LLP
314 CHAMOUNIX RD
WAYNE, PA 19087

6.

DEJOSEPH CHARLES L & TIEN THI
145 CONESTOGA RD
WAYNE, PA 19087

7.

KLEIN MARK
12 DOYLE ROAD
WAYNE, PA 19087

8.

Total Number of Pieces Listed by Sender 8

Total Number of Pieces Received at Post Office

4.00 3.44

Complete in Ink

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59



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Insured Mail
☐ Priority Mail Express
☐ Registered Mail
☐ Return Receipt for Merchandise
☐ Signature Confirmation
☐ Signature Confirmation Restricted Delivery

Affix Stamp Here
(for additional copies of this receipt)
Postmark with Date of Receipt

USPS Tracking/Article Number

GUMMEL MEGAN
4 DOYLE RD
WAYNE, PA 19087

Postage (Extra Service) Fee .43

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

1.

LUKAS JOSEPH T
89 BLACKMORE POND CIR
WEST WAREHAM, MA 02676

.50 .43

2.

BANG KOOK SONG & KYUNG YUL
346 W LANCASTER AVE
WAYNE, PA 19087

.50 .43

3.

WEISBERG MELISSA A
7 DOYLE RD
WAYNE, PA 19087

.50 .43

4.

AMARANT STEVEN L & LISA A
105 LANTOGA RD
WAYNE, PA 19087

.50 .43

5.

SMATLAK MARISSA
131 CONESTOGA RD
WAYNE, PA 19087

.50 .43

6.

MEEHAN CHARLES & ALICE R
139 CONESTOGA RD
WAYNE, PA 19087

.50 .43

7.

147 CONESTOGA LLC
211 POPLAR AVE
WAYNE, PA 19087

.50 .43

Handling Charge - if Registered and over \$50,000 in value

Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery

Special Handling

Postmaster, Per (Name of receiving employee)

4.00 3.44

Total Number of Pieces Listed by Sender 8

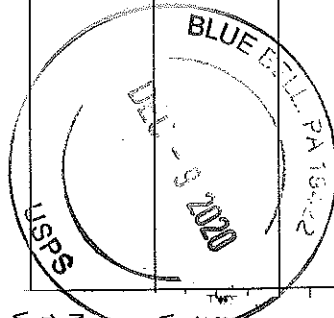
Total Number of Pieces Received at Post Office

PS Form 3877, January 2017 (Page 1 of 2)

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PSN 7530-02-000-9098



Handwritten signature

Handwritten number 60



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Insured Mail

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

1. MCGURL RICHARD J & MICHELLE ANN
8 DOYLE RD
WAYNE, PA 19087

2. ONEILL EDWARD & CARLA
378 W LANCASTER AVE
WAYNE, PA 19087

3. 100 W PARTNERS
595 E LANCASTER AVE #303
ST DAVIDS, PA 19087

4. SALVAGGIO PAUL J
9 DOYLE RD
WAYNE, PA 19087

5. STEFANOWICZ JANUS
11 DOYLE RD
WAYNE, PA 19087

6. POPLAR PROPERTY GROUP LLC
406 MANTOLOKING RD
BRICK, NJ 08723

7. WHITEHEAD KENNETH A & ELISABETH M
111 LANTOGA RD
WAYNE, PA 19087

8. CROWLEY JOHN G & PAULAL
40 LOUELLA CT - A1
WAYNE, PA 19087

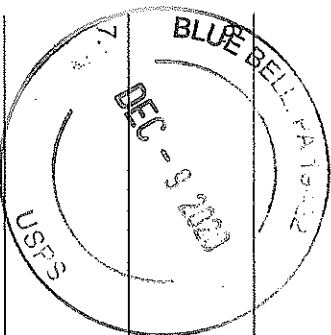
Total Number of Pieces
Listed by Sender 8

Total Number of Pieces
Received at Post Office

4.00 3.44

Complete in Ink

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Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
		Handling Charge - if Registered and over \$50,000 in value										
						Adult Signature Required						
						Adult Signature Restricted Delivery						
						Restricted Delivery						
						Return Receipt						
						Signature Confirmation						
						Signature Confirmation Restricted Delivery						
						Special Handling						

21



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation
☐ Insured Mail ☐ Signature Confirmation

Affix Stamp Here
(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

JEHOVA WITNESS CHURCH
1203 MYRTLEWOOD AVENUE
WAYNE, PA 19087

PATTERSON JESSE D
241 LENOIR AVE
WAYNE, PA 19087

LUCE CHERYL L
110/112 LANTOGA RD UNIT D
WAYNE, PA 19087

SCHUMACHER JOSEPH
108 LANTOGA RD
WAYNE, PA 19087

COCCAGNA GUIDO J & YOLANDA
671 CONESTOGA ROAD
VILLANOVA, PA 19085

TOBIAS GORDON L
109 LANTOGA RD
WAYNE, PA 19087

WHITEHEAD ROBERT A
113 LANTOGA RD
WAYNE, PA 19087

ETM Partners LLC
157 CONESTOGA RD
WAYNE, PA 19087

Total Number of Pieces
Listed by Sender

8

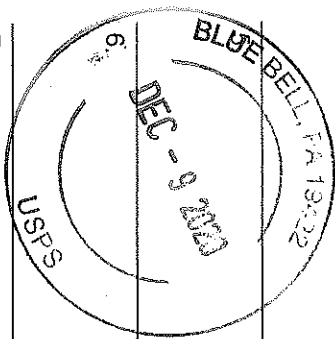
Total Number of Pieces
Received at Post Office

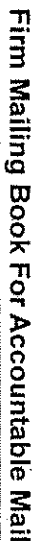
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Postmaster, Per (Name of receiving employee)

4.00 3.44

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KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

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Postmark with Date of Receipt.

ROBERTS JOSEPH D & LUTA ARLETTE M
243 LENOIR AVENUE
WAYNE, PA 19087

2

FRUMER RICHARD
110A LANTOGA RD
WAYNE, PA 19087

HOLMES SUSAN M
110 C LANTOGA RD
WAYNE, PA 19087

MARCANTONIO FREDERICK
104 LANTOGA RD
WAYNE, PA 19087

PILOTTI SAM & GABRIEL JOS INC C/O
METRIC REALTY
1800 E LANCASTER AVE
PAOLI, PA 19301

FG & S ASSOCIATES c/o METRIC REALTY
1800 E LANCASTER AVE
PAOLI, PA 19301

HACKETT KAREN B
104A LANTOGA RD #3
WAYNE, PA 19087

MAROPIS DONNA
104A LANTOGA RD - B3
WAYNE, PA 19087

PS Form 3877, January 2017 (Page 1 of 2)

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PSN 7530-02-000-909B

4.00 3.44

4.00 3.44

Handling Charge - if Registered and over \$50,000 in value

Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

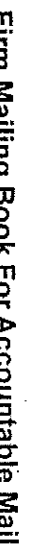
Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery

Special Handling

44



Check type of mail or service

Check type of mail to service

<input type="checkbox"/> Adult Signature Required	<input type="checkbox"/> Priority Mail Express
<input type="checkbox"/> Adult Signature Restricted Delivery	<input type="checkbox"/> Registered Mail
<input type="checkbox"/> Certified Mail	<input type="checkbox"/> Return Receipt for Merchandise
<input type="checkbox"/> Certified Mail Restricted Delivery	<input type="checkbox"/> Signature Confirmation
<input type="checkbox"/> Collected on Delivery (COD)	<input type="checkbox"/> Signature Confirmation
<input type="checkbox"/> Insured Mail	

Affix Stamp Here
(for additional copies of this receipt)
Postmark with Date of Receipt.

USPS Tracking/Article Number

LEE LINDA
104A LANATOGA RD - A1
WAYNE, PA 19087

SHERIDAN MAUREEN E & DANIEL F
233 LENOIR AVE
WAYNE, PA 19087

MAY FRANK E JR & AMY E
227 LENOIR AVE
WAYNE, PA 19087

STRUEBING WILLIAM B & DIVYA
219 LENOX AVE
WAYNE, PA 19087

J B HOLDINGS LLC
7 BERRYWOOD RD
MALVERN, PA 19355

HUMMEL CAROLINE E
217 LENOIR AVE
WAYNE, PA 19087

PILOTTI SAM & GABRIEL JOS INC C/O
METRIC REALTY
1800 E LANCASTER AVE
PAOLI, PA 19301

MARTIN KEELY S
104A LANTOGA RD C1
WAYNE, PA 19087

Total Number of Pieces Listed by Sender	Total Number of Pieces Received at Post Office
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50

4.00 3.44

Handling Charge - if Registered and over \$50,000 in value	
--	--

Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery

Special Handling

PS Form 3877 January 2017 (Page 1 of 2)

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PSN 7530-02-000-9098



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD)

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Postmark with Date of Receipt.

USPS Tracking/Article Number

STIFF SPEAKER BERTRAM III & MARGARET
104A LANTOGA RD
WAYNE, PA 19087

Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
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1.

TAGUE JAMES A & MARY J
235 LENOIR AVE
WAYNE, PA 19087

.50	.43											
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2.

MOSER JEANNETTE C
231 LENOIR AVE
WAYNE, PA 19087

.50	.43											
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3.

GARZIO JOSEPH M & MARY ANN M
225 LENOIR AVE
WAYNE, PA 19087

.50	.43											
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4.

SAMSON PETER & COUGHLIN LYNN
215 LENOIR AVE
WAYNE, PA 19087

.50	.43											
-----	-----	--	--	--	--	--	--	--	--	--	--	--

5.

GARCIA JOSE M & ANA I
104A LANTOGA RD - A5
WAYNE, PA 19087

.50	.43											
-----	-----	--	--	--	--	--	--	--	--	--	--	--

6.

MUELLER JEANNETTE SEIGEL
104A LANTOGA RD - A2
WAYNE, PA 19087

.50	.43											
-----	-----	--	--	--	--	--	--	--	--	--	--	--

7.

KAUFMAN BARBARA M
125 DRAKES DRUM DR
BRYN MAWR, PA 19010

.50	.43											
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8.

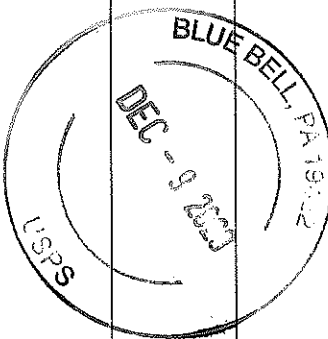
Total Number of Pieces Listed by Sender
8

Total Number of Pieces Received at Post Office

4.00 3.44

Complete in Ink

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Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation Restricted Delivery
☐ Insured Mail

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Postmark with Date of Receipt.

USPS Tracking/Article Number

1.

SWISHER CHRISTOPHER C AND SWISHER
MARGARET A
237 LENOIR AVE
WAYNE, PA 19087

2.

SULLIVAN MARK A & PATRICIA H
229 LENOIR AVE
WAYNE, PA 19087

3.

DALE JOHN & MARTHA
221 LENOIR AVE
WAYNE, PA 19087

4.

DUFRESNE KYLE L & KELSEY F
1911-3 NAVY WHARF CT
TORONTO, ON, M5V3V1, CANADA

5.

CROWLEY JOHN G & PAULA L
40 LOUELLA CT - A1
WAYNE, PA 19087

6.

PENN CREST PROPERTIES LLC
406 MANTOLOKING RD
BRICK, NJ 08723

7.

CARROLL J DOUGLAS
218 LENOIR AVE
WAYNE, PA 19087

8.

WALLACE GERRY H
224 LENOIR AVE
WAYNE, PA 19087

Total Number of Pieces
Listed by Sender

8

Total Number of Pieces
Received at Post Office

Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
.50	.43	Handling Charge - if Registered and over \$50,000 in value										
.50	.43					Adult Signature Required						
.50	.43					Adult Signature Restricted Delivery						
.50	.43					Restricted Delivery						
.50	.43					Return Receipt						
.50	.43					Signature Confirmation						
.50	.43					Signature Confirmation Restricted Delivery						
.50	.43					Special Handling						
4.00	3.44											

67



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD) ☐ Signature Confirmation

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Postmark with Date of Receipt

USPS Tracking/Article Number

MORABITO DAMIEN R & MCKENNA LAUREN
P
230 LENOIR AVE
WAYNE, PA 19087

Postage (Extra Service) Fee

Handling Charge

Actual Value if Registered

Insured Value

Due Sender if COD

ASR Fee

ASRD Fee

RD Fee

RR Fee

SC Fee

SCRD Fee

SH Fee

1.

WRIGHT KATHLEEN L
236 LENOIR AVE
WAYNE, PA 19087

.50 .43

2.

ERCOLE ANTHONY M
187 CONESTOGA RD
WAYNE, PA 19087

.50 .43

3.

KINGSWAY BUILDING ASSOC
595 E LANCASTER AVE #203
ST DAVIDS, PA 19087

.50 .43

4.

EADEN FAMILY LTD PARTNERSHIP
511 OLD LANCASTER RD STE 8
BERWYN, PA 19312

.50 .43

5.

7 EAST STATE STREET LLC
120 CARNOUSTIE WAY
MEDIA, PA 19063

.50 .43

6.

BELL ATLANTIC PA
P O BOX 2749
ADDISON, TX 75001

.50 .43

7.

HENRY PATRICIA J
210 LENOIR AVENUE
WAYNE, PA 19087

.50 .43

8.

Total Number of Pieces Listed by Sender 8

Total Number of Pieces Received at Post Office

Postmaster, Per (Name of receiving employee)

4.00

3.44

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Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD)

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(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number		Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
1.	PAOLETTI THOMAS J & ELIZABETH A HMW 226 LENOIR AVE WAYNE, PA 19087	.50	.43	Handling Charge - if Registered and over \$50,000 in value										
2.	MULLER FRANCIS T & MARY P 232 LENOIR AVE WAYNE, PA 19087	.50	.43											
3.	HULL CAROLYN 238 LENOIR AVE WAYNE, PA 19087	.50	.43											
4.	JARDIN229 LLC 110 GALLAGHER RD WAYNE, PA 19087	.50	.43											
5.	BATES CARLA B 315 W WAYNE AVE WAYNE, PA 19087	.50	.43											
6.	YATES JONATHAN P & ELIZABETH MAXFIELD 309 W WAYNE AVE WAYNE, PA 19087	.50	.43											
7.	SOCIETY ITALIAN AMERICAN CLUB 301 W WAYNE AVE WAYNE, PA 19087	.50	.43											
8.	PENN CREST PROPERTIES LLC 406 MANTOLOKING RD BRICK, NJ 08723	.50	.43											
Total Number of Pieces Listed by Sender		8	Total Number of Pieces Received at Post Office											
		8												
		4.00	3.44											

PAOLETTI THOMAS J & ELIZABETH A HMW

226 LENOIR AVE

WAYNE, PA 19087

MULLER FRANCIS T & MARY P

232 LENOIR AVE

WAYNE, PA 19087

HULL CAROLYN

238 LENOIR AVE

WAYNE, PA 19087

JARDIN229 LLC

110 GALLAGHER RD

WAYNE, PA 19087

BATES CARLA B

315 W WAYNE AVE

WAYNE, PA 19087

YATES JONATHAN P & ELIZABETH MAXFIELD

309 W WAYNE AVE

WAYNE, PA 19087

SOCIETY ITALIAN AMERICAN CLUB

301 W WAYNE AVE

WAYNE, PA 19087

PENN CREST PROPERTIES LLC

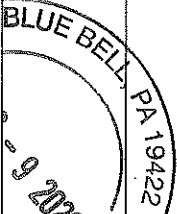
406 MANTOLOKING RD

BRICK, NJ 08723

USPS

DEC - 9 2020

BLUE BELL, PA 19422



69



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery

Postage

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Postmark with Date of Receipt.

USPS Tracking/Article Number

HOLMAN TIMOTHY J & JENNIFER REID
216 LENOIR AVE
WAYNE, PA 19087

MISKA DAVID & SUSAN P
220 LENOIR AVE
WAYNE, PA 19087

BOSNIACK LESLEY R
228 LENOIR AVE
WAYNE, PA 19087

TAGUE VINCENT J III
234 LENOIR AVE
WAYNE, PA 19087

KINGSWAY BUILDING ASSOCIATES
595 E LANCASTER AVE #303
ST DAVIDS, PA 19087

7 E STATE ST LLC
120 CARNOUSTIE WAY
MEDIA, PA 19063

FORTEBUONO JOSEPH & LUCIA F
190 CONESTOGA RD
WAYNE, PA 19087

LEIGHTON PAUL L
188 CONESTOGA ROAD APT. A
WAYNE, PA 19087

Total Number of Pieces
Listed by Sender

8

Total Number of Pieces
Received at Post Office

4.00 3.44

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70



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required ☐ Priority Mail Express
☐ Adult Signature Restricted Delivery ☐ Registered Mail
☐ Certified Mail ☐ Return Receipt for Merchandise
☐ Certified Mail Restricted Delivery ☐ Signature Confirmation
☐ Collect on Delivery (COD)

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(for additional copies of this receipt)
Postmark with Date of Receipt.

USPS Tracking/Article Number

HALLIGAN ROBERT M
3227 SAW MILL RD
NEWTOWN SQUARE, PA 19073

1.

RADNOR TOWNSHIP
301 IVEN AVE
WAYNE, PA 19087

2.

MANCUSO PETE F & RUTH
136 RIDGEFIELD RD
NEWTOWN SQUARE, PA 19073

3.

MAZIS LAWRENCE D & MINDY
327 LIBERTY LN
WAYNE, PA 19087

4.

MARDINLY JASON
333 LIBERTY LA
WAYNE, PA 19087

5.

MORGAN JONATHAN P
210 FAIRWAY RD
PAOLI, PA 19301

6.

HEERY FRANCIS W & KRISTINE
186 CONESTOGA RD
WAYNE, PA 19087

7.

LOGIOIA ANTONIO & PAMELA J
166 CONESTOGA RD
WAYNE, PA 19087

8.

Total Number of Pieces
Listed by Sender

8

Total Number of Pieces
Received at Post Office

4.00 3.44

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71



Name and Address of Sender

KAPLIN STEWART
910 Harvest Drive
Blue Bell, PA 19422

Check type of mail or service

- ☐ Adult Signature Required
☐ Adult Signature Restricted Delivery
☐ Certified Mail
☐ Certified Mail Restricted Delivery
☐ Collect on Delivery (COD)
☐ Insured Mail
☐ Priority Mail Express
☐ Registered Mail
☐ Return Receipt for Merchandise
☐ Signature Confirmation
☐ Signature Confirmation Restricted Delivery

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(for additional copies of this receipt).
Postmark with Date of Receipt.

USPS Tracking/Article Number

1. GIANFORCARO MICHELE
8 DARA CIR
BROOMALL, PA 19008

2. CONNOLLY JACQUELINE
329 LIBERTY LN
WAYNE, PA 19087

3. JBS CONCEPTS LLC
930 ACADEMY LA
BRYN MAWR, PA 19010

4. CONSTANTINE ALBERT
184 CONESTOGA RD
WAYNE, PA 19087

5. FORTUNE SHEILA A
164 CONESTOGA RD
WAYNE, PA 19087

6. AMATO PIETRO
409 W WAYNE AVE
WAYNE, PA 19087

7. DOODY PATRICK J
325 LIBERTY LA
WAYNE, PA 19087

8. WILSON WILLIAM R & GERILYN
331 LIBERTY LN
WAYNE, PA 19087

Total Number of Pieces
Listed by Sender

8

Total Number of Pieces
Received at Post Office

8

PS Form 3877, January 2017 (Page 1 of 2)
PSN 7530-02-000-9098

Postage (Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
.50	.43	Handling Charge - if Registered and over \$50,000 in value									
.50	.43				Adult Signature Required						
.50	.43				Adult Signature Restricted Delivery						
.50	.43				Restricted Delivery						
.50	.43				Return Receipt						
.50	.43				Signature Confirmation						
.50	.43				Signature Confirmation Restricted Delivery						
.50	.43				Special Handling						
4.00	3.44										

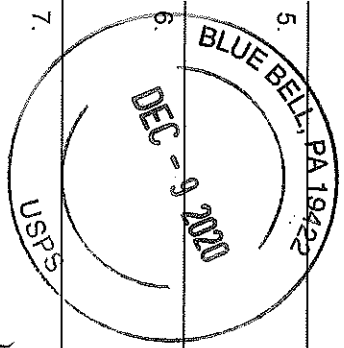
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Firm Mailing Book For Accountable Mail

Name and Address of Sender		Check type of mail or service		Affix Stamp Here (for additional copies of this receipt). Postmark with Date of Receipt.												
		<input type="checkbox"/> Adult Signature Required	<input type="checkbox"/> Priority Mail Express													
		<input type="checkbox"/> Adult Signature Restricted Delivery	<input type="checkbox"/> Registered Mail													
		<input type="checkbox"/> Certified Mail	<input type="checkbox"/> Return Receipt for Merchandise													
		<input type="checkbox"/> Certified Mail Restricted Delivery	<input type="checkbox"/> Signature Confirmation													
		<input type="checkbox"/> Collect on Delivery (COD)	<input type="checkbox"/> Signature Confirmation Restricted Delivery													
		<input type="checkbox"/> Insured Mail														
		<input type="checkbox"/> Priority Mail														
USPS Tracking/Article Number		Addressee (Name, Street, City, State, & ZIP Code™)		Postage	(Extra Service) Fee	Handling Charge	Actual Value if Registered	Insured Value	Due Sender if COD	ASR Fee	ASRD Fee	RD Fee	RR Fee	SC Fee	SCRD Fee	SH Fee
1.	SLAIM DANIEL A & PUPAT NATTASORN 28 MORRIS CIR WAYNE, PA 19087			1.50	.46											
2.																
3.																
4.																
5.																
6.																
7.																
8.																
Total Number of Pieces Listed by Sender		Total Number of Pieces Received at Post Office		Postmaster, Per (Name of receiving employee)												



Handwritten signature

Handling Charge - if Registered and over \$50,000 in value

Adult Signature Required

Adult Signature Restricted Delivery

Restricted Delivery

Return Receipt

Signature Confirmation

Signature Confirmation Restricted Delivery

Special Handling

50.46

Complete in Ink

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