

#### Excellence Delivered As Promised

Date: November 5, 2020

To: Steve Norcini, PE Township Engineer

From: Roger Phillips, PE

cc: Kevin W. Kochanski, RLA, CZO – Director of Community Development

Mary Eberle, Esq. – Grim, Biehn, and Thatcher Damon Drummond, PE – Gilmore & Associates, Inc.

Patricia Sherwin – Radnor Township Engineering Department

**RE:** Hamilton Estate Strafford Ave and Forrest Lane – Conditional Use Review

Gannett Fleming, Inc. has completed our review of the Conditional Use Application for 208 & 228 Strafford Avenue and 18 Forrest Lane Conditional Use Plans for compliance with the Radnor Township Code.

The applicant has filed a conditional use application as outlined in Article XXIII – Conditional Uses, of the Township Code, and is requesting conditional use under §280-90 (Density Modification) of the Township Zoning Code to allow the development of the property to be consolidated from 3 lots to 1 lot with the construction of 41 townhomes. This plan was reviewed only for items relating to the Density Modification portion of the Township Code. A full land development review will be completed following the Conditional Use Hearing should the Density Modification application be approved by the Board of Commissioners.

The tract presently consists of several parcels with a combined lot area of approximately 7.5 acres, containing a total of six dwelling units and related improvements. The tract has frontage on Eagle Road and Strafford Avenue, with the rear boundary adjacent to the Eagle Village Shops.

The applicant intends to develop the 7.5 acres into 41 townhomes under the Pennsylvania Uniform Planned Community Act. The plan is in conformance with the required open space under §280-91. The Plan proposed 53,685 square feet of open space (15.9%) of the total adjusted tract area in accordance with the Density Modification Provisions that will be available for the use of the residents of the new homes.

We have the following general comments regarding the plans as submitted. A detailed review of the subdivision and land development plans will be completed should the Conditional Use Application be approved by the Board of Commissioners.

- 1. Information regarding the ownership of the proposed roadways must be provided. Should the applicant wish to dedicate the roadways to the Township a 60-foot right of way must be provided.
- 2. The applicant must provide information regarding trash removal, snow removal, etc.
- 3. §255-38.H All shade trees provided must be in conformance with the approved trees listing. Upon approval of the Board of Commissioners, other species may be utilized.

If you have any questions or require any additional information, please contact me.

Very truly yours,

GANNETT FLEMING, INC.

Roger A. Phillips, P.E. Senior Project Manager



## GILMORE & ASSOCIATES, INC.

**ENGINEERING & CONSULTING SERVICES** 

#### **MEMORANDUM**

Date: November 16, 2020

**To**: Steve Norcini, P.E.

From: Damon Drummond, P.E., PTOE

cc: Leslie Salsbury, P.E.

Reference: 208 & 228 Strafford Avenue and 18 Forrest Lane

Conditional Use

Radnor Township, Delaware County

Transportation Review 1

G&A #20-11003

Our transportation department has completed a review for the Conditional Use plans prepared for above reference project. We offer the following comments for your consideration:

#### A. PROJECT DESCRIPTION

The Applicant is proposing to construct 41 townhomes on the southwest corner of Stafford Avenue and Eagle Road.

## B. <u>DOCUMENTS</u> REVIEWED

- 1. Conditional Use and Preliminary Subdivision and Land Development Plans prepared by Site Engineering Concepts, LLC, prepared for Hamilton Estate, consisting of 15 sheets and dated August 27, 2020.
- 2. Traffic Impact Assessment prepared by F. Tavani and Associates, Inc., dated August 25, 2020.
- 3. Cover Letter prepared for Radnor Township, prepared by Kaplin Stewart Meloff Reiter & Stein, PC, dated September 24, 2020.

## C. ZONING ORDINANCE COMMENTS

- 1. §280-135 F. (1) In addition to demonstrating compliance with all standards applicable to the conditional use being requested, the generalized site plan shall show the applicant's intentions with regard to site access, interior circulation and parking.
  - a. In accordance with SADLO §255-27A.(5) Dead-end streets shall be prohibited, except as stubs to permit future street extension into adjoining tracts or when designed as cul-de-sacs. In accordance with SADLO §255-27A.(6) Stub streets greater in length than one lot width shall be provided with a turnaround designed

to meet the standards required for a cul-de-sac and shall be provided with sufficient rights-of-way to permit the future extension of the street into the adjacent property.

Unit1 and Unit 2 access a dead-end section of Road A. Access to these two units should be further evaluated.

- b. SADLO §255-27C(1) Sidewalks should be provided on both sides of Road A and Road B for pedestrian circulation.
- 2. §280-135 G. (1) Traffic Impact Study Comments
  - a. The site trips to/and from the site accesses are to be based on 41 units. We understand there are existing units and the applicant is assuming credit for these trips in the existing network; however, the traffic volumes at the access should reflect the proposed site trips based on 41 units to evaluate its operation.
  - b. Revise the Trip Generation Table to reflect site trips for 41 units. An additional line should be added indicating credit for existing site trips.
  - c. Provide a figure indicating the existing site trips.
  - d. Update the analysis worksheets accordingly.
  - e. Attach the turning movement traffic counts in the appendix of the Traffic Impact Assessment for verification.



George W. Broseman

Direct Dial: (610) 941-2459 Direct Fax: (610) 684-2005 Email: gbroseman@kaplaw.com www.kaplaw.com

September 24, 2020

#### VIA HAND DELIVERY

Stephen Norcini Township Engineer Township of Radnor 301 Iven Avenue Wayne, PA 19087

RE: 208 & 228 Strafford Ave, 18 Forrest Lane – Wayne, Radnor Township

**Conditional Use and Preliminary Land Development Applications** 

Our Reference: 15709.1

Dear Mr. Norcini:

I represent the Trustees of the Dorrance Hamilton 3/15/1996 Revocable Agreement of Trust ("Applicant"), the owner of the properties at 208 & 228 Strafford Avenue and 18 Forrest Lane in Radnor Township (collectively "Property") located in the R-4 Residence District. The Applicant is proposing to construct forty-one townhomes and related improvements on the Property as generally shown on the plans included with this application ("Project"). On behalf of the Applicant, I am filing the following:

## I. Conditional Use Application

- 1. Ten (10) copies of the Conditional Use Application seeking a conditional use under Radnor Township Code ("Code") Section 280-29.B.(1) for Density Modification Development in the R-4 Residence District;
- 2. Ten (10) copies of the plan set entitled "208 & 228 Strafford Avenue 18 Forrest Lane Preliminary Land Development Plan Set," containing 15 Sheets dated August 27, 2020 (sheets 1-11 prepared by SITE Engineering Concepts, LLC, sheets 12-15 prepared by Glackin Thomas Panzak);
- 3. Ten (10) copies of a stormwater management report titled "Post Construction Stormwater Management Analysis," prepared by SITE Engineering Concepts, LLC, dated August 27, 2020;

- 4. Ten (10) copies of the narrative for the planning module for land development dated August 24, 2020, prepared by HILBEC Engineering & Geosciences, LLC;
- 5. Ten (10) copies of a traffic impact study prepared by F. Tavani & Associates, dated August 25, 2020;
- 6. Ten (10) copies of a Fiscal Impact Analysis prepared by Erik W. Hetzel, AICP/PP, dated September 17, 2020;
- 7. Ten (10) copies of a development impact study compiled by Kaplin Stewart Meloff Reiter & Stein, dated September 24, 2020;
- 8. Ten (10) copies of the Conceptual Architectural Elevations;
- 9. Ten (10) copies of a draft Planned Community Declaration;
- 10. A \$1,500.00 check made payable to the Township of Radnor, the applicable Conditional Use application fee;
- 11. An electronic copy of all submitted materials;

## II. Preliminary Land Development Application

- 1. Eighteen (18) copies of the Preliminary Land Development application;
- 2. Eighteen (18) copies of the Delaware County Planning Commission review application;
- 3. Eighteen (18) copies of the plan set entitled "208 & 228 Strafford Avenue 18 Forrest Lane Preliminary Land Development Plan Set," containing 15 Sheets dated August 27, 2020 (sheets 1-11 prepared by SITE Engineering Concepts, LLC, sheets 12-15 prepared by Glackin Thomas Panzak);
- 4. Six (6) copies of a stormwater management report titled "Post Construction Stormwater Management Analysis," prepared by SITE Engineering Concepts, LLC, dated August 27, 2020;
- 5. Eighteen (18) copies of a title report prepared by First American Title Insurance Company and identified as PAFA19-0709D/M that includes the portions of the Properties currently known as Delaware Folio Numbers: 36-01-00538-00; 36-01-00539-00; 36-01-00540-00, together with attachments. It is noted that this title report also pertains to other lands owned by Applicant that are not part of this Application. The Properties subject to this these applications are identified in this

title report as "Premises A" and "Premises B". This title report also contains the deed for these parcels;

- 6. Eighteen (18) copies of a title report prepared by First American Title Insurance Company and identified as Commitment Number PAFA20-1391D/M that includes the portion of the Properties currently known as 18 Forrest Lane, Delaware County Folio Number: 36-01-00236-02, together with attachments. It is noted that the title report also pertains to other lands owned by Applicant that are not part of this Application. The Property is identified as "Premises C" in the title report. The title report contains the deed for this portion of this parcel;
- 7. Eighteen (18) copies of a traffic impact study prepared by F. Tavani & Associates, dated August 25, 2020;
- 8. Eighteen (18) copies of a letter from Aqua Pennsylvania to SITE Engineering Concepts, LLC regarding the availability of public water for the Project, dated March 25, 2020;
- 9. An electronic copy of all submitted materials;
- 10. Two (2) checks as follows:
  - a. A \$50.00 check made payable to the Township of Radnor, the applicable Preliminary Land Development application fee;
  - b. A \$16,950.00 check made payable to the Township of Radnor, the applicable Professional Services Account fee.

Please submit the applications and plans to the appropriate Township staff, consultants and bodies for review, public meetings, and a public hearing for the Conditional Use application. Please provide us with copies of all reviews, communications, notices and other documentation relating to these applications, or related to the Property, as soon as they are generated and/or received.

If you have any questions or require any further information, please contact me. Thank you for your attention to this matter.

Sincerely,

George W. Broseman

**GWB:DPR** 

Stephen Norcini September 24, 2020 Page 4

## Enclosures

cc via email: Haverford Properties, Inc.
SITE Engineering Concepts, LLC
Glackin Thomas Panzak

**Attorneys for Applicant** 

George W. Broseman, Esquire

Identification No.: 62649 Daniel P. Rowley, Esquire Identification No.: 322680

**Union Meeting Corporate Center** 

910 Harvest Drive Post Office Box 3037 Blue Bell, PA 19422 (610) 941-2459

gbroseman@kaplaw.com drowley@kaplaw.com www.kaplaw.com

# BEFORE THE BOARD OF COMMISSIONERS OF RADNOR TOWNSHIP, DELAWARE COUNTY, PENNSYLVANIA

In the Matter of: Premises:

Trustees of the Dorrance Hamilton 3/15/1996 Revocable Agreement of Trust, Applicant 208 & 228 Strafford Avenue 18 Forrest Lane Wayne, PA 19087

## **CONDITIONAL USE APPLICATION**

The Trustees of the Dorrance Hamilton 3/15/1996 Revocable Agreement of Trust, through its attorneys, George W. Broseman and Daniel P. Rowley, hereby files this Conditional Use application seeking approval to redevelop the properties at 208 Strafford Avenue, 228 Strafford Avenue, and 18 Forrest Lane in Wayne under the Density Modification Development provisions of the Township Zoning Ordinance and in support thereof states as follows:

## 1. NAME/ADDRESS OF APPLICANT.

The Applicant is the Trustees of the Dorrance Hamilton 3/15/1996 Revocable Agreement of Trust ("Applicant"), c/o Mr. D. Charles Houder, Haverford Properties, Inc., 551 W. Lancaster Avenue, Suite 307, Haverford, PA 19041. Applicant is the owner of the parcels currently known as 208 Strafford Avenue, 228 Strafford Avenue, and 18 Forrest Lane (collectively the "Tract"). The Tract consists of Delaware County Folio Numbers: 36-01-00538-00; 36-01-00539-00; 36-

01-00540-00; 36-01-00236-02. A copy of the deeds to the parcels that comprise the Tract are attached as **Exhibit A-1** and **Exhibit A-2**. **Exhibit A-1** is a copy of the deed that includes the parcels known as 208 Strafford Avenue and 228 Strafford Avenue, also known as Delaware County Folio Numbers: 36-01-00538-00; 36-01-00539-00; 36-01-00540-00. **Exhibit A-2** is a copy of the deed for the parcel known as 18 Forrest Lane, also known as Delaware County Folio Number 36-01-00236-02. It is noted that both deeds also include other lands owned by the Applicant that are not part of this Application.

#### 2. **DESCRIPTION OF PROPERTY.**

The Tract presently consists of several parcels with a combined lot area of approximately 7.5 acres that contain a total of six dwelling units and related improvements. The Tract has frontage on Eagle Road and Strafford Avenue, and has an extensive boundary along the rear of the Eagle Village Shops, a large, multi-use shopping center that fronts on Lancaster Avenue and Eagle Road<sup>1</sup>. The Tract is served by four driveways, three of which are interconnected. Three driveways connect to Strafford Avenue and one connects to Eagle Road<sup>2</sup>. There are no sidewalks along the frontages of the Tract. The Tract and the general area around the Tract are developed suburban areas and are served by both public water and sanitary sewer service. Like many of the other properties in the area, including those in close proximity to the Tract that were developed before today's strict stormwater management requirements, the parcels within the Tract have no stormwater management facilities.

The existing conditions of the Tract are shown on the plan sheet entitled "Existing Conditions Plan," sheet 2 of 15 of the plan set entitled "208 & 228 Strafford Avenue, 18 Forrest

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<sup>&</sup>lt;sup>1</sup> One parcel, 18 Forrest Lane also abuts the Eagle Village Shops and presently contains a single-family detached dwelling that fronts on Forrest Lane.

<sup>&</sup>lt;sup>2</sup> The parcel at 18 Forrest Lane presently has its own driveway to Forrest Lane.

Lane Preliminary Land Development Plan Set" containing 15 sheets dated August 27, 2020 (sheets 1-11 prepared by SITE Engineering Concepts, LLC, sheets 12-15 prepared by Glackin Thomas Panzak) (collectively referred to as "Plans"). The Plans are attached as Exhibit "B.".

Pursuant to the Radnor Township Code ("Code"), the Tract is located in the R-4 Residence District ("R-4 District"), and is one property removed from the Township's primary commercial artery, Lancaster Avenue/Route 30. The Tract serves as a transitional area from the commercial area along Lancaster Avenue to the residential areas to the north and northeast. The Tract either borders or is in close proximity to a number of commercial zoning districts, including: the C-2 General Commercial District, the C-3 Service Commercial District, the CO Commercial-Office District, and the PB Planned Business District. Some of the commercial uses that border or are in close proximity to the Tract include the Eagle Village Shops (described above), the Strafford Shopping Center with frontage on Lancaster Avenue and Eagle Road, and the Executive Commons of the Main Line office park with extensive frontage on both Eagle Road and Strafford Avenue. Only one property line, and the side lot lines of the 18 Forrest Lane parcel abut residentially zoned lots.

## 3. <u>CONDITIONAL USE FOR DENSITY MODIFICATION DEVELOPMENT</u>

## A. <u>Density Modification Development – Townhomes</u> <u>a Permitted Use & Minimum Tract Size</u>

The Applicant requests conditional use approval from the Township Board of Commissioners under Code § 280-29.B.(1) for Density Modification Development in the R-4 District to redevelop the Tract with forty-one townhomes, common open space, significant stormwater management facilities, and related improvements (collectively "**Project**"). (*See, Code § 280-93 permitting townhomes in the R-4 District*). Density Modification Development is

permitted in many of the Township's residential zoning districts (*i.e.* AC, R-1, R-1A, R-2, R-3 and R-4). Code § 280-92.A. In the R-4 District, Density Modification Development is permitted on tracts of at least five acres in size. Code § 280-92.A.(4). The Tract, with over 7.5 acres exceeds this requirement. The Project will include common open space along the existing road frontages, in a centralized green to be created, and in other areas.

The Project is shown on the plan sheet entitled "Record Plan," sheet 4 of 15 of the Plans. As described below, the Plans comply with applicable provisions of the Zoning Ordinance for Density Modification Development in the R-4 District. The Applicant has also filed with the Township a preliminary land development application for the Project which will be reviewed by the Township engineering consultants, the Radnor Township Planning Commission, and the Board of Commissioners.

## B. <u>Density Modification Development Requirements</u>.

#### 1. Townhome regulations.

Article XIX of Chapter 280 of the Code contains the requirements for Density Modification Development. Code § 280-93 sets forth the specific regulations applicable to townhomes in Density Modification Developments. As required, the townhomes will be owned and operated under the applicable provisions of the Pennsylvania Uniform Planned Community Act. See, Code § 280-93.A. The proposed townhomes are arranged in groups of four, five or six units respectively, in compliance with Code § 280-93.B, which allow up to eight townhomes to be attached in any one group. Each townhouse group is separated by at least 20 feet. Code § 280-93.B. The townhomes also meet the requirements for wall plane design. Id.

## 2. Permitted Density, Dimensional Standards etc.

The Project also complies with the other applicable standards for Density Modification Development. As noted above, the area of the Tract is over 7.5 acres, which exceeds the

minimum requirement of five acres in the R-4 District under Code §280-92.A(4). The proposed 41 townhome units on the Tract are in accordance with the density requirements of 5.5 units per acre. *Id.* No buildings are within 40 feet of an existing street right-of-way line or within 25 feet from any adjacent property line. *Code § 280-92.A(4)*. The Plans also comply with the height, building coverage, and impervious surface requirements of Code 280-94, as shown in the Zoning Table on sheet 4 of 15 of the Plans.

Code § 280-97.D provides for yard setback requirements for townhomes developed under the Planned Community Act in lieu of the yard requirements of Code § 280-97.B. In the R-4 District, Code § 280-97.D requires a distance of at least 30 feet between the sides of groups of townhomes. The Plans satisfy these requirements. No accessory buildings or structures are proposed at this time, so Code § 280-97.E is not at issue. The Plans provide for ample parking in the proposed garages, the driveways, and in designated areas along the internal roadways that exceed Code parking requirements. *Code §§ 280-98.C; 280-103.B(1)*. Pedestrian connectivity will be enhanced by the addition of sidewalks along the Tract frontage on Strafford Avenue and Eagle Road, and along the internal roadways. *Id.* As noted above, the Tract and general area are presently served by public sanitary sewer and public water, and the new dwelling units will likewise be served by these public facilities. *Code §280-96*.

## 3. Common Open Space & Buffers.

Code § 280-91 provides standards for common open space and for buffers where the Tract adjoins residentially zoned districts. As required, the Plans designate at least 15% of the Tract area as common open space that will be available for use by or enjoyment of the residents of the new homes. *Code §§ 280-91.A & F.* The proposed common open space is appropriate and suitable for recreation and open space purposes. *Code § 280-91.B*. The proposed common

open space is part of the development and is not separated from the Tract by existing roads.  $Code \S 280-91.C$ . The proposed common open space includes a centralized green area that will be easily accessible to the proposed dwellings and help to, along with other portions of the common open space, preserve the character of the streetscape along Strafford Avenue and Eagle Road.  $Code \S 280-91.D$ . No structures, parking areas, streets, or public facilities are proposed within the common open space.  $Code \S 280-91.D$  & F. The common open space will be restricted as required by the Code.  $Code \S 280-91.E$ . Along the Tract's property lines that border other lots that are residentially zoned, the required 25 feet wide buffer area will be provided.  $Code \S 280-91.G$ .

The ownership and maintenance of the proposed common open space will be provided by a homeowners association that will be formed in compliance with the requirements of Code \$280-99, and the applicable provisions of the Planned Community Act.

## 4. Application Requirements for a Density Modification Development

Code § 280-100.B sets forth specific information to be provided as part of the Conditional Use application. This information has been provided and is summarized as follows:

- (1) The nature of the landowners' interest in the land to be developed.

  As noted above, Applicant is the owner of the Tract.
- (2) The density of land use to be allocated to the site to be developed.

In accordance with Code § 280-92.A(4), the permitted density is 5.5 units/per acre. The proposed density of forty-one units on 7.523 acres (+/-) satisfies that requirement.

(3) Location and size of common open space and the form of the organization proposed to own and maintain the common open space.

The common open space is shown on the Plans. The Tract will be subjected to the Pennsylvania Uniform Planned Community Act through a Declaration of Planned Community, which will be executed and recorded at the appropriate time. The

homeowners' association will own and maintain the common open space on behalf of the unit owners within the planned community. A draft of the Planned Community Declaration has been submitted to the Township and may be refined at a later stage.

(4) The use and the approximate height, bulk and location of dwellings and other structures.

The Plans depict the bulk and location of the proposed townhomes. Compliance with these provisions is described above and on the Plans. The height of the buildings will not exceed 35 feet per applicable Code requirements for Density Modification Development. It is noted that the permissible height of 35 feet is the same as allowed for conventional development in the R-4 District, and in many of the Township's other residential zoning districts.

(5) The feasibility of proposals for the disposition of sanitary waste and stormwater and provision of public water supply.

Public water and sanitary sewer are proposed. The Tract and the surrounding area are already served by public water and sanitary sewer.

(6) The substance of covenants, grants of easements or other restrictions proposed to be imposed upon the use of the land, buildings and structures, including proposed easements or grants for public utilities.

As noted above, the Tract will be subject to the Pennsylvania Uniform Planned Community Act. A draft of the Planned Community Declaration has been submitted to the Township as part of the filing of the Conditional Use Application. The Planned Community Declaration will be finalized at a later stage.

(7) A provision for parking of vehicles and the location and width of proposed streets and public ways.

The Plans depict the parking areas and internal drives. No public streets or ways are proposed. The Plans show four parking spaces for each dwelling unit, consisting of two garage parking spaces and two driveway parking spaces. There are also eleven additional parking spaces located on the internal drives. The proposed level of parking exceeds the two parking spaces per dwelling unit required by Code §280-103.B(1) and the one additional parking space per four dwelling units required by Code §255-29.A(20).

(8) A statement which will show the ecological and economic impact of the development on the Township and especially as to the surrounding areas.

The Tract and the surrounding area is within a fully-developed suburban area. The Tract will be redeveloped and will include common open space and stormwater management facilities, neither of which presently exist at the Tract. The Project will comply with applicable stormwater management requirements, and the requirements associated with the required outside agency permits, including a National Pollutant Discharge Elimination System ("NPDES") permit which includes a further review of during-construction and post-construction stormwater management. The substantial investment in the redevelopment of the Tract will generate revenue for both the Township and the Radnor Township School District through increased property values and through the realty transfer taxes on the sale of each townhouse unit. A Fiscal Impact Study and a Development Impact Study further addressing these issues are included with this Conditional Use application.

## (9) The results of traffic studies taken on the surrounding and nearby roads.

The Conditional Use Application submission includes a Traffic Impact Study dated August 25, 2020, prepared by F. Tavani and Associates, Inc.

## 4. <u>NATURE OF A CONDITIONAL USE.</u>

A conditional use is not an exception to a zoning ordinance, but rather, is a use to which an applicant is entitled unless objectors demonstrate, according to standards set forth in the zoning ordinance and the law, that the proposed use would adversely affect the community. Blancett Maddock v. City of Pittsburgh Zoning Board of Adjustment, 640 A.2d 498 (Pa. Cmwlth. 1994). A conditional use is nothing more than a special exception which falls within the jurisdiction of the municipal governing body as opposed to the zoning hearing board. In re Thompson, 896 A.2d 659, 670 (Pa. Cmwlth. 2006). The existence of a conditional use provision in a zoning ordinance indicates a legislative determination that the use is consistent with the municipality's zoning plan, and is a use which is presumptively consistent with the public health, safety and welfare. Borough of Perkasie v. Moulton Builders, 850 A.2d 778 (Pa. Cmwlth. 2004); Ruddy v. Lower Southampton Township Zoning Hearing Board, 669 A.2d 1051 (Pa. Cmwlth. 1995).

An applicant for conditional use approval has the burden of proving that the proposed use complies with the specific objective requirements of the zoning ordinance that are applicable to the conditional use. *Appeal of Neill*, 634 A.2d 749 (Pa. Cmwlth. 1993). Once an applicant for conditional use approval proves that the proposed use complies with the specific objective requirements for conditional use set forth in the zoning ordinance, the burden shifts to the objectors to demonstrate through substantial evidence that the proposed use would detrimentally affect the public health, safety and welfare, to a greater degree than the typical use in question. *Mann v. Lower Makefield Township*, 634 A.2d 768 (Pa. Cmwlth. 1993); *Dotterer v. Zoning Hearing Board of Upper Pottsgrove Township*, 588 A.2d 1023 (Pa. Cmwlth. 1991).

The Commonwealth Court enunciated the general rule governing special exceptions and conditional uses in *Appeal of Brickstone Realty Corp.*, 789 A.2d 333 (2001):

A special exception [or conditional use] is a conditionally permitted use, allowed by the Legislature if specifically listed standards are met. A special exception [or conditional use] is thus not an "exception" to the zoning ordinance, but a use permitted conditionally, the application for which is to be granted or denied by the zoning hearing board pursuant to express standards and criteria. Where a particular use is permitted in a zone by special exception [or conditional use], it is presumed that the local legislature has already considered that such use satisfies local concerns for the general health, safety, and welfare and that such use comports with the intent of the zoning ordinance. Thus, once the applicant for a special exception [or conditional use] shows compliance with the specific requirements of the ordinance, it is presumed that the use is consistent with the promotion of health, safety, and general welfare. The burden then shifts to objectors to prove that the proposed use is not, in fact, consistent with the promotion of health, safety and general welfare. (citations omitted)

The objectors' burden of demonstrating that a proposed use does not comply with the general health, safety and welfare criteria imposed upon conditional uses is not satisfied by showing that a proposed use will have effects that are no different than those which normally result from the construction of the permitted use. *Moyer's Landfill, Inc. v. Zoning Hearing* 

Board of Lower Providence Township, 450 A.2d 273 (Pa. Cmwlth. 1982). Rather, a conditional

use may only be denied if opponents demonstrate that the impact of the proposed use on the

public welfare is greater than that which might be expected from such a use in normal

circumstances. Ruddy v. Lower Southampton Township Zoning Hearing Board, supra.; New

Bethlehem Borough Council v. McVay, 467 A.2d 395 (Pa. Cmwlth. 1983). Moreover, those who

object to an application for a conditional use cannot meet their burden of showing that the

proposed use would violate the health, safety and welfare of the community by merely

speculating as to possible harm; rather, objectors must show a high degree of probability that the

proposed use will substantially affect the health and safety of the community. Manor Healthcare

Corp. v. Lower Moreland Township Zoning Hearing Board, 590 A.2d 65 (Pa. Cmwlth. 1991).

5. **CONCLUSION.** 

As noted above, the Plans comply with the applicable provisions of the Township Zoning

Ordinance for Density Modification Development in the R-4 District. The Plans will also provide

for common open space, limited new curb cuts to the adjoining roadways, enhanced pedestrian

connectivity, and provide for significant stormwater management where none exists.

Accordingly, the Conditional Use should be granted.

Respectfully Submitted

GEORGE W. BROSEMAN, ESQUIRE DANIEL P. ROWLEY, ESQUIRE

Attorneys for Applicant

Date: September 24, 2020

- 10 -

15709/1/6283514v13

## **EXHIBIT A-1**

## REDACTED DEED FOR 208 & 228 STRAFFORD AVENUE

PRÉPARED BY AND RETURN TO: Chicago Title Insurance Company Suite 1325 1515 Market Street Philadelphia, PA 19102

8109-25100(9)

RD BK04569-0060 2009038189 06/23/2009 01:57:53 PM:1 RCD FEE: \$100.00 OT-DEED

RCD FEE: \$100.00

COUNTY

36-RADNOR \$0.00

THOMAS J. JUDGE SR. ROO

This Indenture Made this 24th day of April

, 2009

Between DORRANCE H. HAMILTON, a/k/a DORRANCE HILL HAMILTON, of Boca Grande, Florida (hereinafter called the Grantor)

#### **AND**

DORRANCE H. HAMILTON, MARGARET H. DUPREY, NATHANIEL PETER HAMILTON, JR., S. MATTHEWS V. HAMILTON, JR., and BARBARA R. COBB, TRUSTEES of the DORRANCE H. HAMILTON MARCH 15, 1996 REVOCABLE AGREEMENT OF TRUST

(hereinafter called the Grantees)

Witnesseth that the said Grantor for and in consideration of the sum of lawful money of the United States of America, unto her well and truly paid by the said Grantees at or before the sealing and delivery hereof, the receipt whereof is hereby acknowledged, has granted, bargained and sold, aliened, enfeoffed released and confirmed, and by these presents does grant, bargain and sell, alien, enfeoff, release and confirm unto the said Grantees, their successors and assigns;

#### PREMISES "A"

ALL THAT CERTAIN lot or tract of land with the buildings and improvements thereon erected, Situate in the Township of Radnor, County of Delaware, Commonwealth of Pennsylvania, described according to a survey thereof, made by John F. Kauffman, Civil Engineer on September 22, 1886, as follows, to wit:

BEGINNING at a point in the center line of a public road leading from the Philadelphia and Lancaster Tumpike to the Gulf Road and known as the Eagle Road, where the center line of a new Avenue leaving to Eagle Station meets the said center line of the said Eagle Road; thence along the center line of the said new Avenue North twenty five degrees, two minutes East three hundred twenty and twelve one-hundredths feet; thence by a line parallel with the said Eagle Road and along the line of land conveyed by Henry D. Hughes and wife to Richard T. McCarter, Jr., South sixty four degrees, fifty eight minutes West two hundred seventy two and fifteen one-hundredths feet to a point in line of land conveyed by William Robb to the said Clara T. Shaw; thence along the said last mentioned land by a line parallel with the aforesaid avenue South twenty five degrees, two minutes East three hundred twenty and twelve one-hundredths feet to the aforesaid center line of said Eagle Road; thence along the center line of said Eagle Road North sixty four degrees, fifty eight minutes East two hundred seventy two and fifteen one-hundredths feet to the first mentioned point and place of beginning.

ALSO ALL THAT CERTAIN tract of land, situate in the Township and County aforesaid,

BEGINNING at a stone in the center line of a public road called the Eagle Road the distance of two hundred thirty eight feet and one tenths of a foot measured along the center line of the said road Northeastwardly from the corner of land now or late of John B. Thayer; thence along the said center line of the said Road North sixty four degrees, fifty eight minutes East two hundred five and seven tenths feet to a stone set for a corner in the center line of an Avenue thirty three feet wide (lately vacated); thence along the center line of the said vacated Avenue being also a line of land conveyed by Harry C. Banks and wife to the said Clara T. Shaw North twenty five degrees, two minutes West three hundred twenty and twelve one-hundredths feet to a corner of land conveyed by William Robb to Richard T. McCarter Jr.; thence along the said land of the said Richard T. McCarter Jr., South sixty four degrees, fifty eight minutes West two hundred five and seven tenths feet to a point in the middle of land of Ellwood Wilson; thence along the said land South twenty five degrees, two minutes East three hundred twenty and twelve one-hundredths feet to the first mentioned point and place of beginning.

BEING known as 204 Strafford Avenue.

BEING designated as Uniform Parcel Identifier No. 36-01-00538-00.

#### PREMISES "B"

ALL THAT CERTAIN lot or piece of ground with the buildings and improvements thereon erected, situate in the Township of Radnor, County of Delaware, State of Pennsylvania, described according to a survey thereof made by Wilson R. and John B. Yerkes, Civil Engineers and Surveyors of Byrn Mawr, Pennsylvania, dated June 6, 1962, as follows:

**BEGINNING** at a point in the middle of Strafford Avenue (50 feet wide) said point is at the distance of three Hundred twenty and forty-five one hundredths feet (320.45') measured North twenty-five degrees two minutes West (N 25°02' W.) along the middle of Strafford Avenue from its intersection with the middle of Eagle Road; thence leaving Strafford Avenue by land of L. Weinrich South sixty-four degrees fifty-eight minutes West (S. 64°58' W.) four hundred seventy-seven and eighty-five one hundredths feet (477.85') to an iron pin in line of land of Lecian Von Bermuth; thence partly by the same and partly by land of various owners North twenty-five degrees two minutes West (N. 25°02' W.) four hundred twenty-one and fifty one hundredths feet (481.50') to a stone; thence by land of Elisabeth J. Grant North sixty-five degrees five minutes East (N. 65°05' E.) four hundred seventy-seven and eighty-five one hundredths feet (477.85') to a point in the middle of Strafford Avenue aforesaid; thence along the middle of same South twenty-five degrees two minutes East (S. 25°02' E.) four hundred twenty and forty-five one hundredths feet (420.45') to the place of beginning.

BEING known as 228 Strafford Avenue.

**BEING** designated as Uniform Parcel Identifier No. 36-01-00539-00 and 36-01-00540-00.

#### PREMISES "C"

ALL THAT CERTAIN lot or piece of ground with the buildings and improvements thereon erected, situate in the Township of Radnor, County of Delaware and Commonwealth of Pennsylvania, bounded and described according to a Map of Property of Wallace and Warner Corporation, made by M.R. and J.B. Yerkes, Civil Engineers, Bryn Mawr, Pennsylvania, dated October 24, 1946 and revised November 8, 1946, (new lot numbers) September 25, 1947, as follows, to wit:



BEGINNING at a spike marking the intersection of the middle lines of Fairfield Lane and Forrest Lane (both forty feet wide); thence along the middle of Forrest Lane, the two following courses and distances: (1) South thirty four degrees, thirty seven minutes East, nine and eighty four one-hundredths feet to a point; and (2) South twenty three degrees, forty seven minutes East, one hundred three feet to a point; thence leaving Forrest Lane, South sixty five degrees, seven minutes West, one hundred thirty four and eighty four one-hundredths feet to a stone; thence by other land of Wallace and Warner of which this is a part, North twenty four degrees West, one hundred seventeen and twenty nine one-hundredths feet to a point in the middle of Fairfield Lane; thence along the middle of same, North sixty seven degrees, two minutes East, one hundred thirty three and forty two one-hundredths feet to the place of beginning.

**BEING** Lot No. 12 on the above mentioned Plan.

BEING known as 142 Fairfield Lane.

BEING designated as Uniform Parcel Identifier No. 36-01-00200-00.

#### PREMISES "D"

ALL THAT CERTAIN lot or piece of ground with the buildings and improvements thereon to be erected, situate in the Township of Radnor, County of Delaware and Commonwealth of Pennsylvania and described according to a Map of Property of Edwin T. Myskowski, et ux made by Yerkee Engineering Co., Surveyors, Bryn Mawr, Pennsylvania dated January 3, 1969 and last revised February 3, 1969 as follows:

BEGINNING at a point in the middle line of Forrest Lane (proposed to be opened to the width of 30 feet), which said point is measured, the two following courses and distances along said middle line from its intersection with the extended middle line of Fairfield Lane (forty feet wide): (1) from said point of intersection South thirty four degrees, thirty seven minutes East, nine and eighty four one hundredths feet and (2) South twenty three degrees, forty seven minutes East, forty three and forty six one-hundredths feet to the point and place of beginning; thence extending from said beginning point leaving Forest Lane by other land of Edwin T. Myskowski of which this was a part, North sixty five degrees seven minutes East, one hundred twenty three feet to a point in line of land now or late of J.B. Parker, thence extending along said land South twenty three degrees, forty seven minutes East, fifty nine and fifty four one-hundredths feet to a point a corner of land now or late of Lecian Von Bermuth; thence extending along said land South sixty five degrees, seven minutes West, one hundred twenty three feet to a point in the middle line of Forest Lane aforesaid; thence extending along the middle line of same North twenty three degrees, forty seven minutes West, fifty nine and fifty four onehundredths feet to the place of beginning.

BEING Lot No. 2 as shown on said Plan.

BEING known as 22 Forrest Lane.

BEING designated as Uniform Parcel Identifier No. 36-01-00236-03.

**BEING AS TO PREMISES "A"** the same premises which H. Everett Shore, III, Straw Party by deed dated 6/3/1977 and recorded 6/10/1977 in the Office of the Recorder of Deeds in and for the County of Delaware in Deed Book 2609 page 716, granted and conveyed unto Dorrance H. Hamilton, in fee.

BEING AS TO PREMISES "B" the same premises which Thomas Hart and S. Leonard Kent, Jr., Executors under the Will of J. Brooks B. Parker, deceased by Deed dated 6/27/1952 and recorded 7/2/1952 in the Office of the Recorder of Deeds in and for the County of Delaware in Deed Book 1532 page 347, granted and conveyed unto Dorrance Hill Hamilton, in fee.



i	
	BEING AS TO PREMISES "C" the same premises which Gerard V. Cavanaugh and Danielle Cavanaugh, his wife by Deed dated 7/1/1985 and recorded 7/15/1985 in the Office of the Recorder of Deeds in and for the County of Delaware in Volume 251 page 1525, granted and conveyed unto Dorrance H. Hamilton, in fee.
	BEING AS TO PREMISES "D" the same premises which H. Everett Shore, III, Straw Party by Deed dated 1/19/1984 and recorded 2/23/1984 in the Office of the Recorder of Deeds in and for the County of Delaware in Volume 139 page 983, granted and conveyed unto Dorrance H. Hamilton, in fee.
•	
!	
•	

Together with all and singular the buildings, improvements, ways, streets, alleys, driveways, passages, waters, water-courses, rights, liberties, privileges, hereditaments and appurtenances, whatsoever unto the hereby granted premises belonging, or in any wise appertaining, and the reversions and remainders, rents, issues, and profits thereof; and all the estate, right, title, interest, property, claim and demand whatsoever of the said Grantor, as well at law as in equity, of, in, and to the same.

**UNDER AND SUBJECT,** nevertheless to matters of record, to the extent valid and enforceable, subsisting and in fact affect title to the property or any part thereof.

To have and to hold the said lot or piece of ground above described with the buildings and improvements thereon erected; with the hereditaments and premises hereby granted, or mentioned, and intended so to be, with the appurtenances, unto the said Grantees, their successors and assigns to and for the only proper use and behoof of the said Grantees, their successors and assigns, forever. Under and subject, as aforesaid.

And the said Grantor, for herself, her heirs, executors and administrators does by these presents, grant, covenant, promise and agree, to and with the said Grantees, their successors and assigns, that she the said Grantor, her heirs, all and singular the hereditaments and premises herein above described and granted, or mentioned and intended so to be with the appurtenances unto the said Grantees, their successors and assigns, against her the said Grantor, her heirs and against all and every person or persons whomsoever lawfully claiming or to claim the same or any part thereof, by, from, or under him, her, them or any of them shall and will, UNDER AND SUBJECT as aforesaid, WARRANT and forever DEFEND.

nstrument Number: 2009038189 Book/Page: RECORD-04569/0066 Date Filed: 6/23/2009 1:57:53	PM
In Witness Whereof, the said Grantor has caused these presents to be duly executed and dated the day and year first above written.	
Sealed and Delivered IN THE PRESENCE OF US	
Dorrance H. Hamilton (SEAL)	
Dorrance Hill Hamilton (SEAL)	

Instrument Number: 2009038189 Book/Page: RECORD-04569/0067 Date Filed: 6/23/2009 1:57:53 PM

COMMONWEALTH OF PENNSYLVANIA			
COUNTY OF Delaware	)SS )		

On this, the 24th day of , 2009, before me, a Notary Public and the undersigned officer, personally appeared **Dorrance H. Hamilton**, a/k/a **Dorrance Hill Hamilton**, known to me (or satisfactorily proven) to be the person whose name is subscribed to the within instrument, and acknowledged that she executed the same for the purposes therein contained.

In Witness Whereof, I hereunto set my hand and official seal.

NOTARIAL BEAL ROBEMARIE COLYVAS Notary Public RADNOR TWP, DELAWARE COUNTY My Commission Expires Oct 16, 2011 Notary Public

My Commission Expires:

10/14/2011

REV-183 EX (11-04)



COMMONWEALTH OF PENNSYLVANIA DEPARTMENT OF REVENUE BUREAU OF INDIVIDUAL TAXES PO BOX 280603 HARRISBURG PA 17128-0603

THE RECORDER'S REFUSAL TO RECORD THE DEED.

# REALTY TRANSFER TAX STATEMENT OF VALUE

See Reverse for Instructions

REC	ORDER'S USE ONLY
State Tax Paid	0
Book Number	4564
Page Number	60.
Date Recorded	(1/2/NC

Complete each section and file in duplicate with Recorder of Deeds when (1) the full value/consideration is not set forth in the deed, (2) when the deed is without consideration, or by gift, or (3) a tax exemption is claimed. A Statement of Value is not required if the transfer is wholly exempt from tax based on: (1) family relationship or (2) public utility easement. If more space is needed, attach additional sheet(s).

A. CORRESPONDENT - All inq	uiries may	be directe	d to the following				
Name		9 Danking 11	D	Telephone Nun	NOON:		
Anthony T. Fratianne, Esquire, Mo	rgan, Lewis	& Bockius, LL	<del></del>		Louis	17000	
Street Address 1701 Market Street			City Philadelphia		State PA	Zip Code 19103	
B. TRANSFER DATA			Date of Acceptar	ice of Document			
Grantor(s)/Lessor(s)			Grantee(s)/Lessee(s)				
Dorrance H. Hamilton, a/k/a Dorrance Hill Hamilton			Trustees of the Dorrance H. Hamilton 3/15/96 Rev. Agr. of Ti				
Street Address			Street Address				
			Suite 316, 200 Eag	ile Road			
City	State	Zip Code	City		State	Zip Code	
Boca Grande	FL		Wayne		PA	19087	
C. PROPERTY LOCATION							
Street Address			City, Township, Borough	)			
See Exhibit "A"			Radnor Township				
County	School	District	<u> </u>	Tax Parcel Number			
Delaware				See Exhibit "A"			
D. VALUATION DATA							
Actual Cash Consideration	2. Othe	r Consideration	ation 3. Total Co		Consideration		
4. County Assessed Value 5. Common		mon Level Ratio	Factor 6. Fair Market Value = See Exhib				
E. EXEMPTION DATA		· · · · · · · · · · · · · · · · · · ·		<u> </u>			
1a. Amount of Exemption Claimed	th Pan	centage of intere	et Conveyed	T			
7a. Aligora di Exemplici Granned	ID. Cen	Cernage Of Micerc	ist Conveyed				
2. Check Appropriate Box Below  Will or intestate succession	v for Exemp		d	- 15	state File	Number	
☐ Transfer to Industrial Develo	pment Agen		Name of Decedenty	(4	,010H0 1 R0	, turinger,	
☐ Transfer to a trust. (Attach c	omplete cop	y of trust agre	eement identifying all	beneficiaries.)			
☐ Transfer between principal a	ind agent. (A	ttach comple	te copy of agency/str	aw party agreement	t <b>.)</b>		
Transfers to the Commonwe of condemnation, (If condem	ealth, the Uni	ted States an	ed Instrumentalities by mnation, attach copy	gift, dedication, co	ndemna	tion or in lieu	
☐ Transfer from mortgagor to a	holder of a	mortgage in d	efault. Mortgage Bool	Number	, Page N	umber	
Corrective or confirmatory d	eed. (Attach	complete cor	y of the prior deed b	eing corrected or co	nfirmed.	.)	
☐ Statutory corporate consolid							
Other (Please explain exem	ption claimed	d, if other tha	n listed above.) _72	P.S. 8102 C.3 (8.1)	A trans	fer for no or	
nominal actual consideration	to a trustee	of a living trus	st from the settlor of th	e living trust.			
Under penalties of law, I declare the of my knowledge and belief, it is tru			atement, including acc	companying informa	ition, an	d to the best	
Signature of Conespondent or Responsible				D	ate 4/1	4/09	
- (NY 1/X)							
FAILURE TO COMPLETE THIS FO	ORM PROPI	ERLY OR AT	TACH APPLICABLE	DOCUMENTATION	MAY R	ESULT IN	

## **EXHIBIT "A"**

#### Section C: Street Address

204 Strafford Avenue (Premises "A") 228 Strafford Avenue (Premises "B") 142 Fairfield Lane (Premises "C") 22 Forrest Lane (Premises "B")

#### Section C: Tax Parcel Number

Uniform Parcel Identifier No. 36-01-00538-00 (Premises "A")
Uniform Parcel Identifier No. 36-01-00539-00 and
No. 36-01-00540-00 (Premises "B")
Uniform Parcel Identifier No. 36-01-00200-00 (Premises "C")
Uniform Parcel Identifier No. 36-01-00236-03 (Premises "D")

#### Section D-4: County Assessed Value

(Premises "A")
(Premises "B")
(Premises "B")
(Premises "C")
(Premises "D")

#### Section D-6: Fair Market Value

(Premises "A")
(Premises "B")
(Premises "B")
(Premises "C")
(Premises "D")

TOTAL

CTIC File #08-PHI-1046

## DEED

Grantor: DORRANCE H. HAMILTON, a/k/a DORRANCE HILL HAMILTON

## TO

Grantees: DORRANCE H. HAMILTON, MARGARET H. DUPREY,

NATHANIEL PETER HAMILTON, JR., S. MATTHEWS V. HAMILTON, JR., and

BARBARA R. COBB, TRUSTEES of the DORRANCE H. HAMILTON

MARCH 15,1996 REVOCABLE AGREEMENT OF TRUST

Premises: 204 Strafford Avenue (Premises "A")

228 Strafford Avenue (Premises "B") 142 Fairfield Lane (Premises "C") 22 Forrest Lane (Premises "D")

Township of Radnor County of Delaware

Commonwealth of Pennsylvania

Uniform Parcel Identifier No. 36-01-00538-00 (Premises "A")

Uniform Parcel Identifier No. 36-01-00539-00 and

36-01-00540-00 (Premises "B")

Uniform Parcel Identifier No. 36-01-00200-00 (Premises "C") Uniform Parcel Identifier No. 36-01-00236-03 (Premises "D")

The address of the above named Grantees is:

218 Enterprises Suite 316 200 Eagle Road Wayne, PA 19087

Certified by:

## **EXHIBIT A-2**

## REDACTED DEED FOR 18 FORREST LANE

**RETURN TO:** Chicago Title Insurance Co. 601 Market Street, Ste. #2550 8103.00452 (36)

This Indenture Made this 17" day of October, 2003

Between.

James W. Lament and Elizabeth A. Ashdale-Lament (here nafter called the Grantors).

AND

Trustees Under Agreement of Trust dated March 15, 1996, as Amended, Dorrance H. Harrilton, Settlor (hereinafter called the Grantees)

Witnesseth That the said Grantor for and in consideration of the sum of lawful money of the United States of America, unto them well and truly paid by the said Grantees at or before the sealing and delivery hereof, the receipt whereof is hereby acknowledged, has granted, bargained and sold, released and confirmed, and by these presents does grant, bargain and sell, release and confirm unto the said Grantees, their successors and assigns.

ALL THAT CERTAIN lot or piece of ground with the buildings and improvements thereon erected. Situate in the Township of Radnor, County of Delaware, Commonwealth of Pennsylvania and described according to a Map or Property of Edwin I. Myskowski, et ux, made by Yerkes Engineering Co., Surveyors, Bryn Mawr, PA, dated 1/3/1969 as follows, to wit:

BEGINNING at a point on the middle line of Forrest Lane of variable widths at the distance of 674.37 feet measured Southeastwardly along said middle line of Forrest Lane from the center line of Old Eagle School Road; thence extending from said point of beginning North 65 degrees 07 minutes 00 seconds East, along the Strafford Realty Company Subdivision Plan 129.79 feet to a point; thence extending South 23 degrees 47 minutes 00 seconds East along land now or late of J. B. Parker, 79.06 feet to a point; thence extending South 65 degrees 07 minutes 00 seconds West along Lot No. 2 on said plan 123 feet to a point on the middle line of Forrest Lane, aforesaid; thence extending along the same, the two following courses and distances: (1) North 23 degrees 47 minutes 00 seconds West 43.46 feet to a point; and (2) North 34 degrees 37 minutes 00 seconds. West 36.11 feet to the first mentioned point and place of beginning.

BEING Lot No. 1 on said Plan.

BEING Folio Number 36-01-00236-02

RD BK02991-1480 2003144115 10280003 to 32 44 AM 1

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at-DEED

: 2003144115 Page: 1480.00

Mir Rollings out



COMMENTMENTH OF PETULYSVAN A DEPARTMENT OF REVENUE HUREAU OF INDIVIDUAL BAXES DEPT 240403 HARRISBURG, PA 17126 0003

# REALTY TRANSFER TAX STATEMENT OF VALUE

State (as to de)

South Number

Form till Wer

Date Recorded

See Roverse for Instructions

Complete each section and file in deplicate with Recorder of Deeds when (1) the full value/consideration is not set lattle in the deed, (2) when the deed is without consideration, or by gift, or (3) a tax exemption is claimed. A Statement of Value is not required if the transfer is whally exempt from tax based on. (1) family relationship or (2) public utility easement. If more space is needed, attach additional sheet(s)

		•	space is needed, attach		
A CORRESPO	NDENT - All	inquiries may b	e directed to th	Telephone Number.	rson:
	tle Insurar			Area Cide 215	1568-4800
Street Address	24 × 2.14	Сну	Philadelphi	Slate a PA	Zip Cods 19103
1601 Market B TRANSFER			Date of Acceptance of		
Crantor(s)/Lessor(s)			Graniee sillessee(s) T		r Agreement of
James W. Lame	nt & Elizah	eth Lament	Trust date	<u> 3/15/98 as</u>	•
Sieer Address 18 Forrest Ro	ad		Street Address	st Road	
G <sub>r</sub> Wayne	Sicie PA	7.p Cade 19087	wayne	State PA	2 p Coda 19037
C' PROPERTY	LOCATION .				alian da jaman da jaman
Sheel Address 18 FOTT	est Road		City, Township, Bornug Radi		
Ceumy Delaware		School District	<u> </u>	Tax Porcel Number	_
		Radno	er 	36-01-002	36~02 
D VALUATIO		7. Other Canaderation		3. Total Consideratio	
- Activos Costa Comacos conces		7. Office Canadardiios		. Tolci Cansaleralia	n
4. County Assessed Value		5. Common Level Ratio Factor		6 Fair Market Value	
F EVENETIAL	1 6 0 5 0	×		ε	
E EXEMPTION  Is. Amount of Exemption Claim		1b. Percentage of Interes	Conveyed		
Transfer to Industria  Transfer to a trust. (  Transfer between ps  Transfers to the Con (If condemnation at  Transfer from morts  Corrective or confirm	Attach complete copincipal and agent. (Annonwealth, the Unit in lieu of condemnationagor to a holder of consolidation, merge	iname of Decry  y of trust agreement id  attach complete copy of ed States and Instrume ion, attach copy of res a mortgage in default, complete copy of the or or division. (Attach a	entifying all beneficiar f agency/straw party of intolities by giff, dedica olution.) Mangage Book Numb prior deed being corre	agreement.) ation, condemnation of over, Posted or confirmed.)	r in lieu of condemnation. age Number
Under penalties of law, I de and belief, it is true, corre Signaluse of Correspondent e	ct and complete.	omined this Statement	, including accompan		to the best of my knowledge the

FAILURE TO COMPLETE THIS FORM PROPERLY OR ATTACH APPLICABLE DOCUMENTATION MAY RESULT IN THE RECORDER'S REFUSAL TO RECORD THE DEED.

BEING the same premises which Madeline L. Fisher, by Indenture bearing date April 29, 2003 and recorded MAY 5, 2003 in the Office of the Recorder of Deeds, in and for the County of Delaware, in Volume, 2761 page 2189 etc., granted and conveyed unto James W. Lament and Elizabeth A. Ashdale-Lament. Husband and Wife, in fee

(Remainder of page intentionally left blank)

Together with all and singular the improvements, ways, streets, alleys, driveways, passages, waters, water-courses, rights, liberties, privileges, hereditaments and appurtenances, whatsoever unto the hereby granted premises belonging, or in any wise appertaining, and the reversions and remainders, rents, issues, and profits thereof; and all the estate right, title, interest, property, claim and demand whatsoever of the said Grantors, as well at law as in equity, of, in, and to the same.

To have and to hold the said lot or piece of ground above described with the buildings and improvements thereon erected; with the hereditaments and premises hereby granted, or mentioned, and intended so to be, with the appurtenances, unto the said Grantees, their successors and assigns to and for the only proper use and behoof of the said Grantees, their successors and assigns, forever

And the said Grantors, for themselves, their heirs executors, administrators do by these presents, covenant, grant and agree, to and with the said Grantees, their successors, and assigns, that they the said Grantors, their heirs, all and singular the Hereditaments and premises herein above described and granted, or mentioned and intended so to be with the Appurtenances unto the said Grantees, their successors and assigns, against them the said Grantors, their heirs and against all and every Person or Persons whomsoever lawfully claiming or to claim the same or any part thereof, by from, or under him, her, them or any of them shall and will WARRANT and forever DEFEND.

In Witness Whereof, the said Grantors have caused these presents to be duly executed dated the day and year first above written.

Sealed and Delivered

IN THE PRESENCE OF US

\_\_ (02,12)

James W. Lament

(SEAL)

Elizabeth A. Ashdale-Lament

COMMONWEALTH OF PENNSYLVANIA	) )SS
COUNTY OF DELAWARE	)

On this, the 17<sup>th</sup> day of October, 2003, before me, a notary public the undersigned officer, personally appeared James W. Lament and Elizabeth A. Ashdale-Lament, known to me (or satisfactorily proven) to be the person(s) whose name(s) is/are subscribed to the within instrument, and acknowledged that they executed the same for the purposes therein contained.

In Witness Whereof, I hereunto set my hand and official seal.

Notary Public

COMMONWEALTH OF PENNSYLYANIA

NOTARIAL SEAL NANCY SHEVLIN, Notary Public City of Philadelphia, Phila County My Commission Expires September 25, 2007

My Commission Expires:

# DEED

Grantor: James W. Lament and Elizabeth A. Ashdale-Lament

## TO

Grantee: Trustees Under Agreement of Trust dated March 15,

1996, as Amended, Dorrance H. Hamilton, Settler

PREMISES: 18 Forrest Lane

Folio Number 36-01-00236-02

Radnor Township Delaware County

The address of the above named Grantees is.

50/1 = 3/6

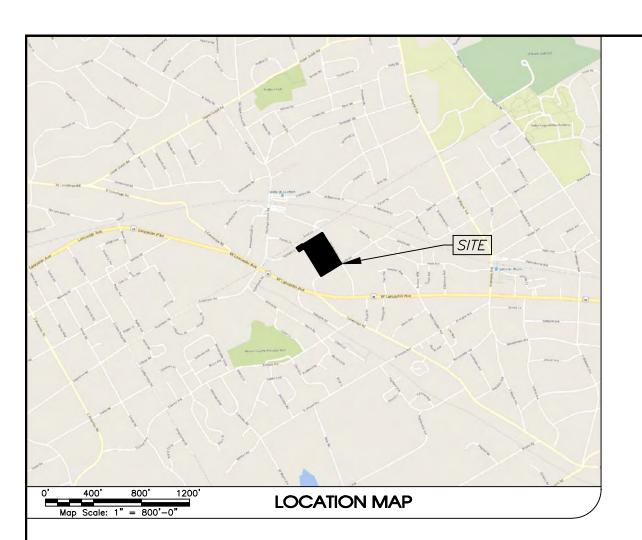
200 Ea gle Road

Wayne PR 19987

Certified by: 6- Without

## EXHIBIT "B"

# PRELIMINARY LAND DEVELOPMENT PLAN



# ADJOINING PROPERTIES (LANDS N/F)

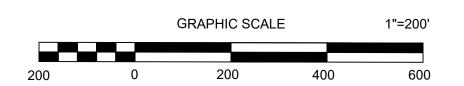
- 1. VILLAGE ASSOCIATES, 503 W LANCASTER AVE, 36-11-316
- 2. HAMILTON S MATTHEWS V JR & MIRABELLO FRACIS J & ETAL, 22 FORREST LANE, 36-11-339
- 3. HAMILTON S MATTHEWS V JR & MIRABELLO FRACIS J & ETAL, 142 FAIRFIELD LANE, 36-11-338
- 4. OCONNOR CONSTANCE M & IZZO PETER J, 11 FAIRFIELD LANE. 36-11-337
- 5. SCHUDA JOSEPH M & FRANCES E, 14 FORREST LANE, 36-11-341
- 6. CHAWLA GAGAN & CHAWLA SANNU. 21 GRANT LANE, 36-11-342
- 7. JOHNSTON ALFRED J III & JOHNSTON GLORIA 17 GRANT LANE, 36-11-343
- 8. MORRISSEY CAREN E, MORRISSEY WILLIAM L JR. 13 GRANT LANE. 36-11-344
- 9. SCHERI STEVEN W & MEGAN G, 9 GRANT LANE, 36-11-345
- 10. MAHONEY JOHN J III & MARY ANN, 5 GRANT LANE, 36-11-346
- 11. THE TRUSTEES REVOCABLE TRUST C/O HAMILTON REVOCABLE TRUST, 235 STRAFFORD AVE, 36-11-353
- 12. GRAY JEREMY P & CONTRERAS TERESA PAOLA 231 STRAFFORD AVE, 36-11-352
- 13. BROOKS JOHN W & KATHRYN R, 227 STRAFFORD AVENUE, 36-11-351
- 14. THOMASON ROBERT I & THOMASON TRAUDI. 211 STRAFFORD AVENUE, 36-11-350
- 15. HAMILTON DORRANCE H ETAL TRSTEES REVOCABLE TRUST, 205 STRAFFORD AVE, 36-11-349:001
- 16. SATTERFIELD DAVID & MORRISSEY MARY ANNE, 207 STRAFFORD AVE, 36-11-349:002
- 17. ANUJEET & TARA SAREEN, 205 STRAFFORD AVE, 36-11-349
- 18. ALAN J SILVER, 201 STRAFFORD AVE, 36-11-349:003
- 19. FUCHS JR JOHN O C/O THE STRAFFORD OFFICE BLDG, 125 0175 STRAFFORD AVE, 36-12-263
- 20. FUCHS O JOHN, 200 EAGLE RD, 36-11-314

# 208 & 228 STRAFFORD AVENUE 18 FORREST LANE

# CONDITIONAL USE AND PRELIMINARY LAND DEVELOPMENT PLAN SET



MAP SHOWING FEATURES WITHIN 300' OF PROPERTY LINE



CIVIL ENGINEER: SITE ENGINEERING CONCEPTS, LLC

ATTN: ROBERT M. LAMBERT, P.E. P.O. BOX 1992 SOUTHEASTERN, PA 19399 P: (610) 240-0450 E: RLAMBERT@SITE-ENGINEERS.COM

LANDSCAPE ARCHITECT: GLACKIN THOMAS PANZAK, INC.

ATTN: BERNARD S. PANZAK, RLA PAOLI EXECUTIVE GREEN 1, SUITE 300 PAOLI, PA 19301 P: (610) 408-9011 E: BPANZAK@GLACKINPLAN.COM

APPLICANT/OWNER: THE TRÚSTEES OF THE DORRANCE HAMILTON 3/15/1996 REVOCABLE AGREEMENT OF TRUST

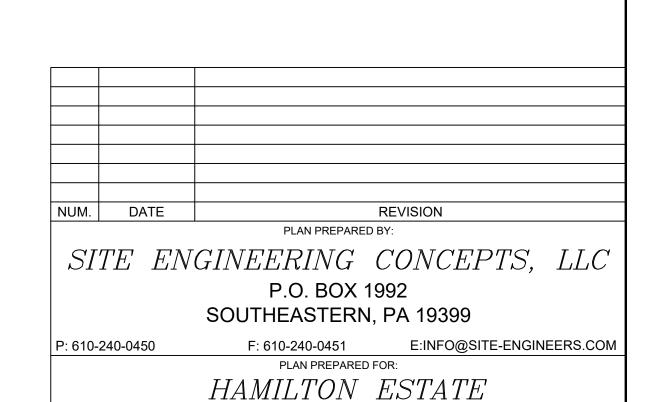
ATTN: CHARLES HOUDER 551 W LANCASTER AVE, SUITE 307 HAVERFORD, PA 19041 P: (610) 389-0305 E: DCH@HAVERFORDPROPERTIES.COM

# DRAWING SCHEDULE

- 1. COVER SHEET
- 2. EXISTING CONDITIONS PLAN
- 3. DEMOLITION PLAN
- 4. RECORD PLAN
- 5. POST CONSTRUCTION STORMWATER PLAN
- 6. DURING CONSTRUCTION E&S
- 7. PCSM DETAILS
- 8. CONSTRUCTION DETAILS
- 9. E&S DETAILS
- 10. PROFILES
- 11. PROFILES

DAVID J. SANDERS, P.E. PE057436

- 12. LP-1 LANDSCAPE PLAN
- 13. LP-2 LANDSCAPE DETAILS
- 14. LI-1 LIGHTING PLANS
- 15. LI-2 LIGHTING DETAILS



208 & 228 STRAFFORD AVE AND 18 FORREST LANE **WAYNE, PA 19087** 

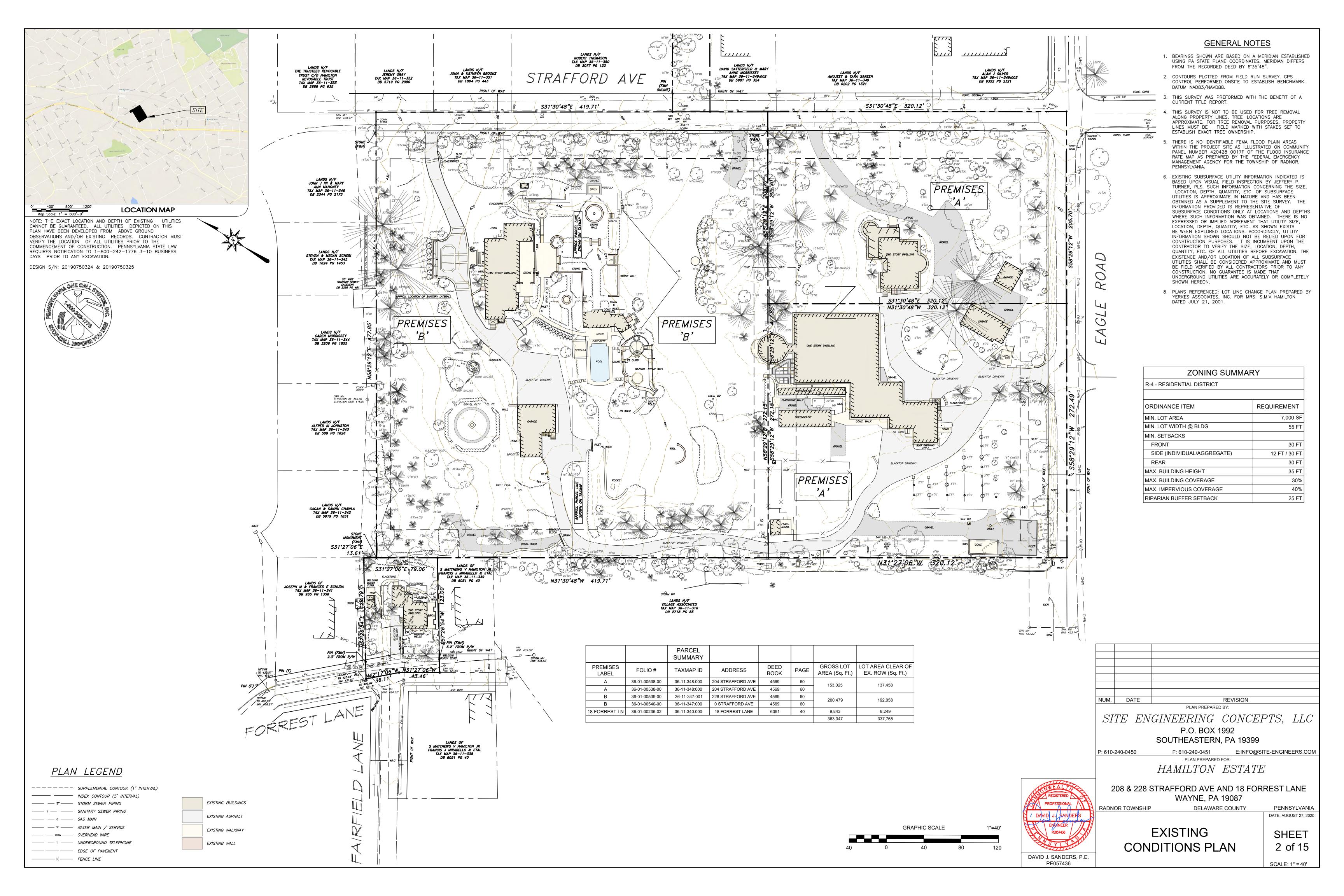
RADNOR TOWNSHIP DELAWARE COUNTY PENNSYLVANIA

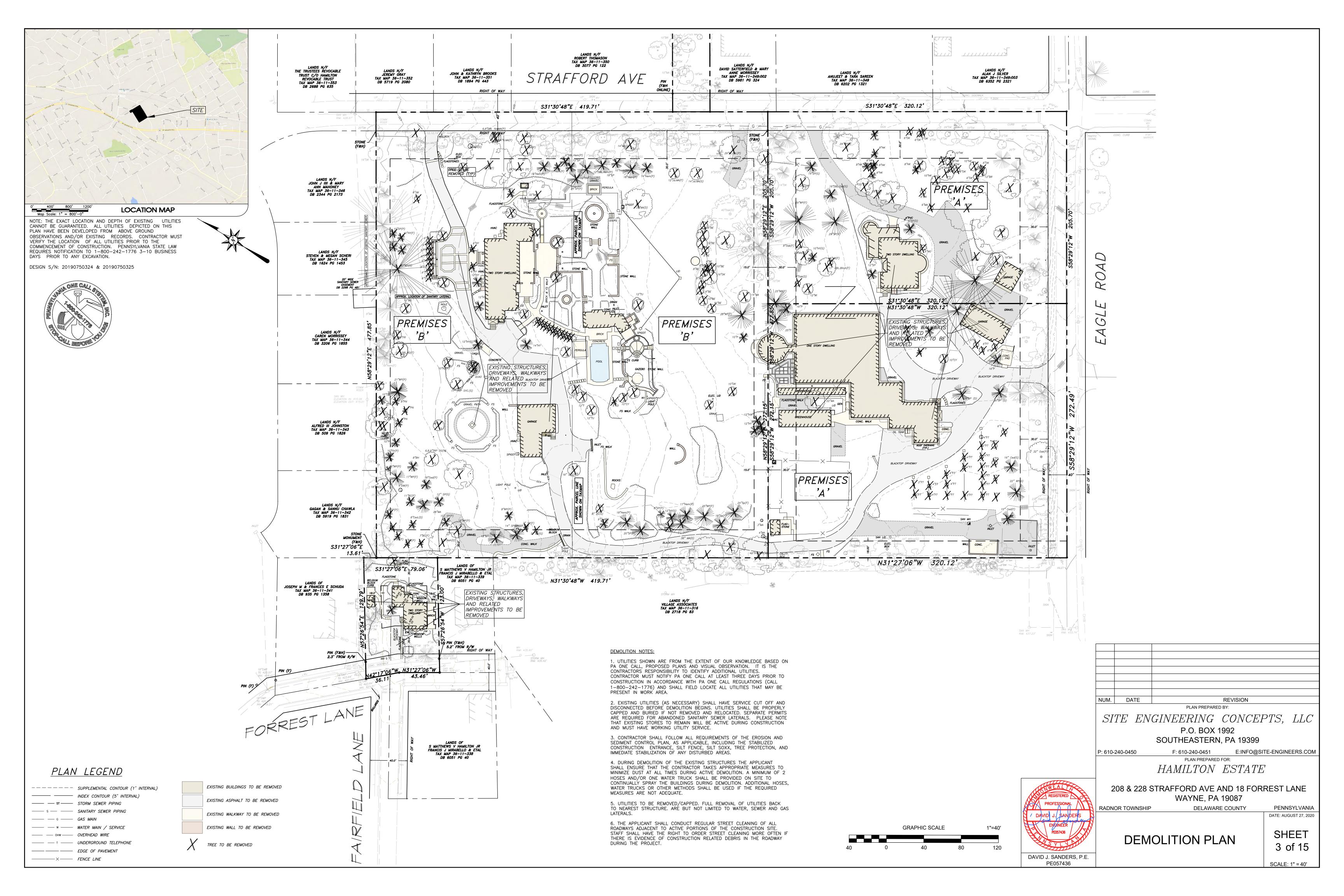
**COVER SHEET** 

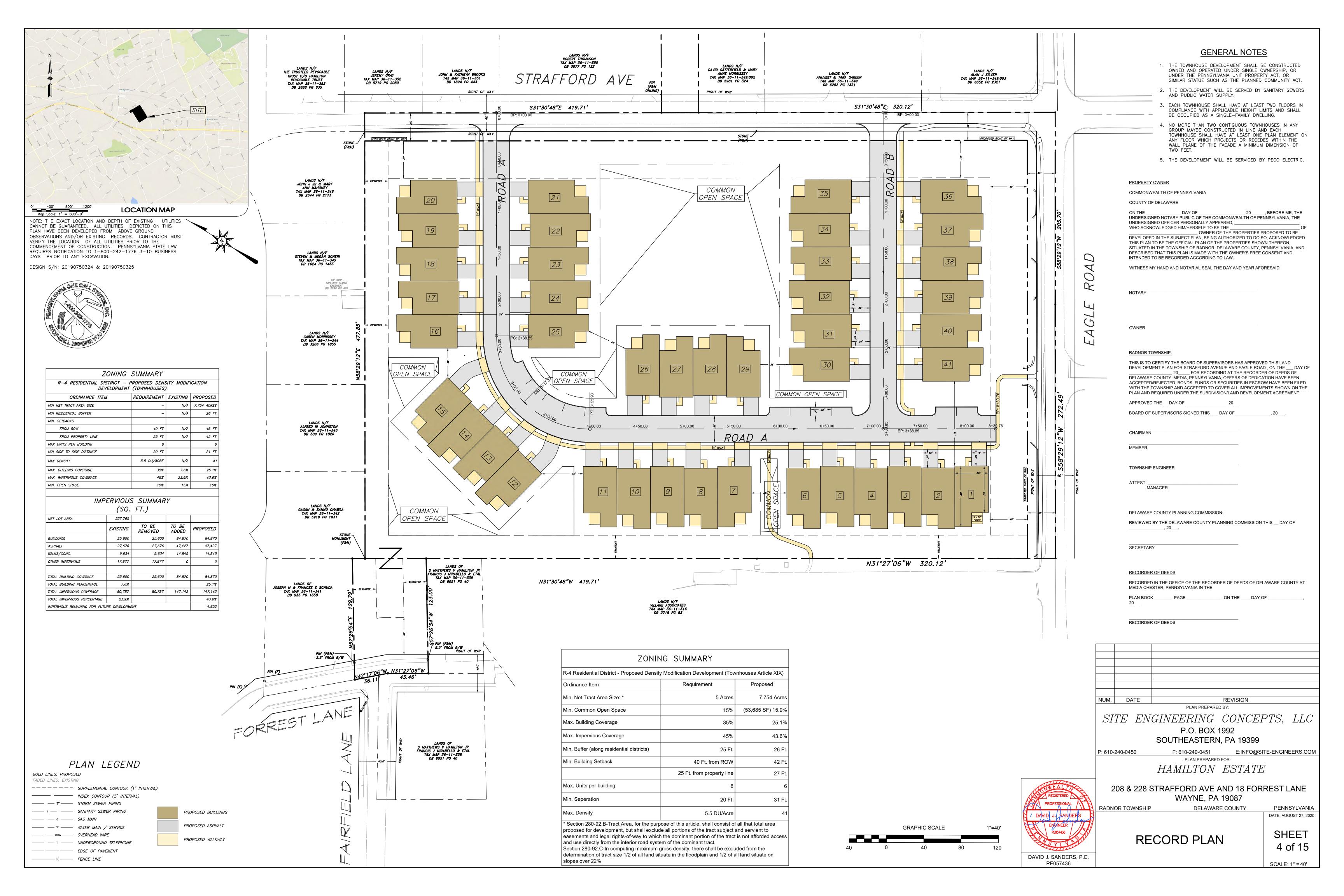
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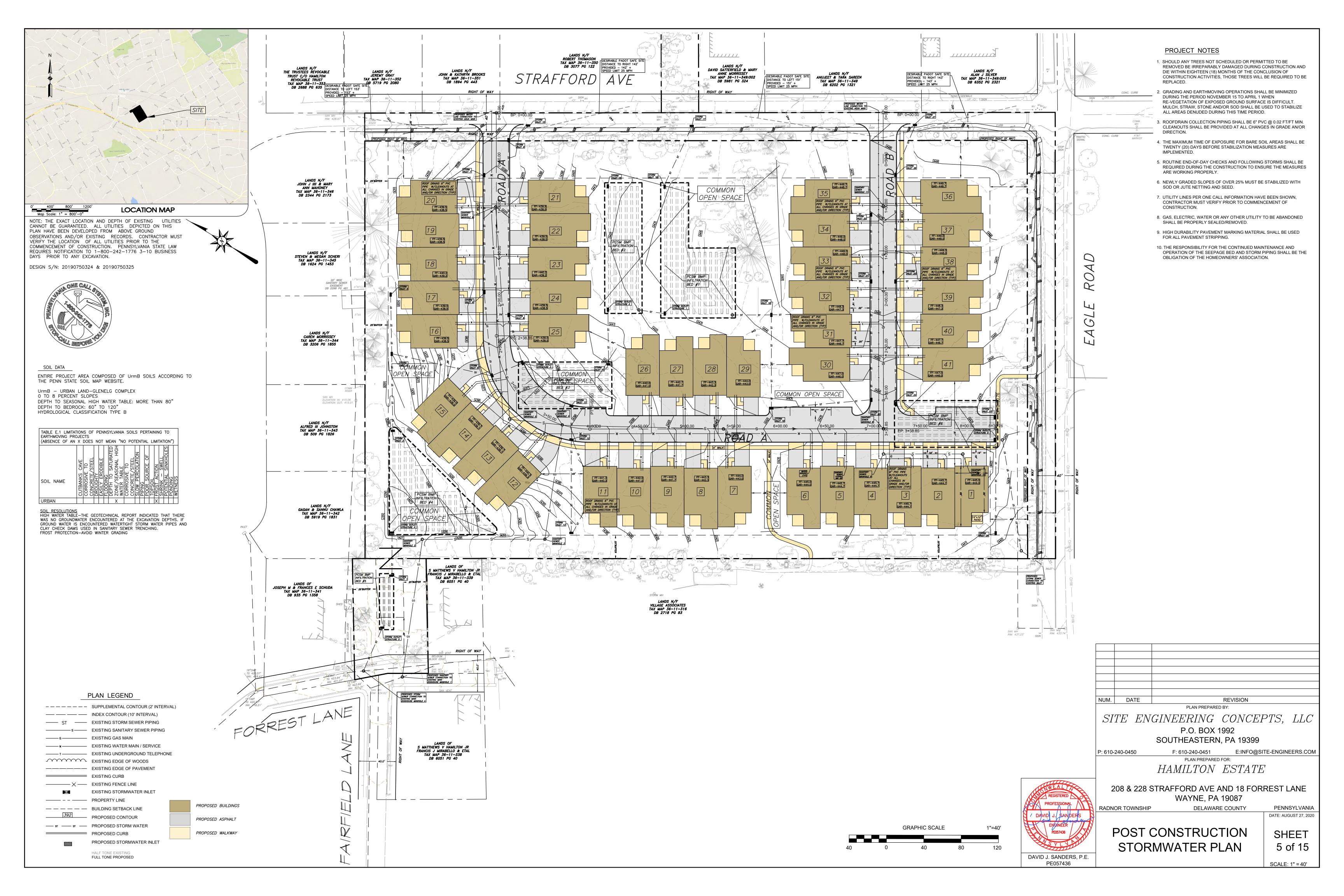
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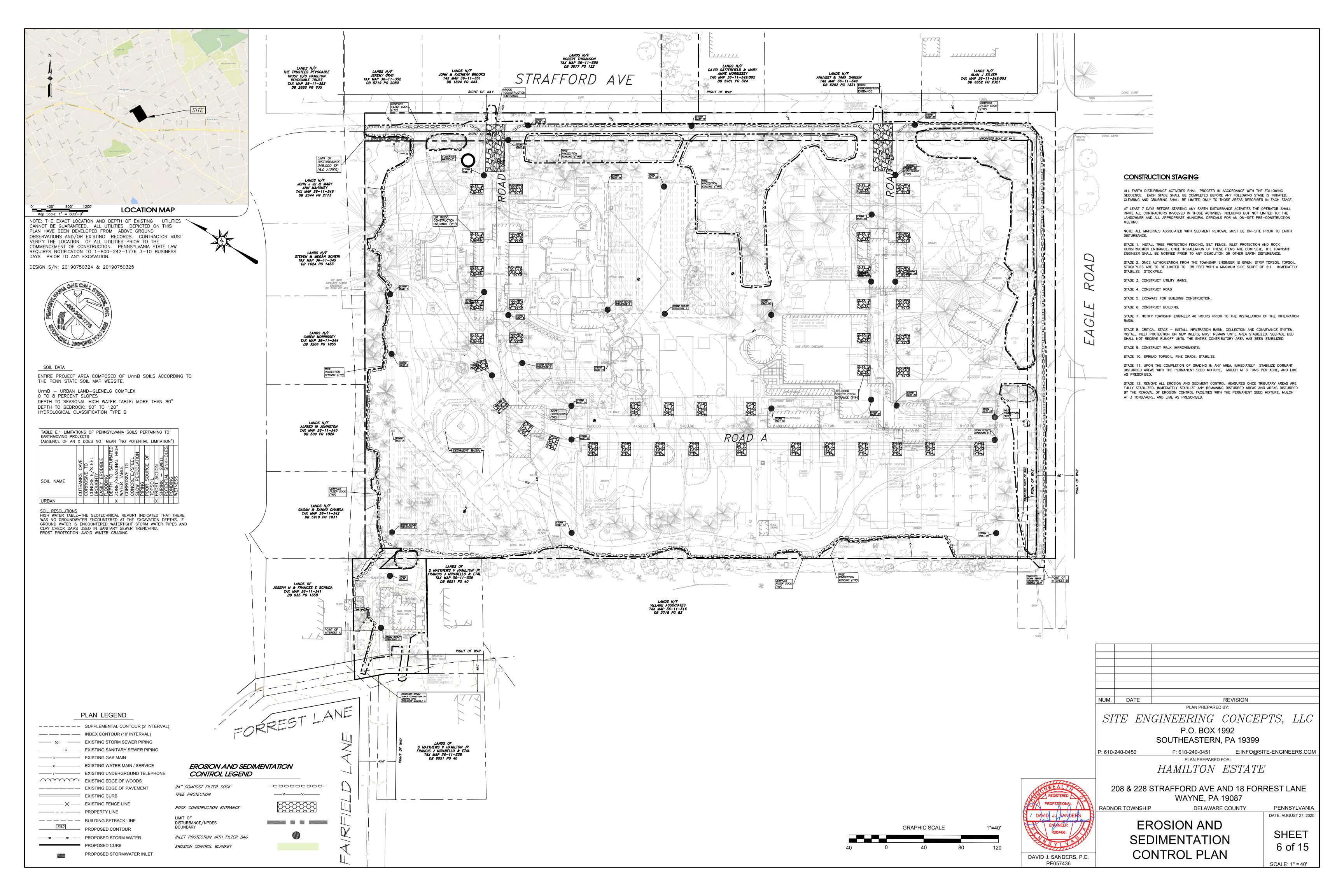
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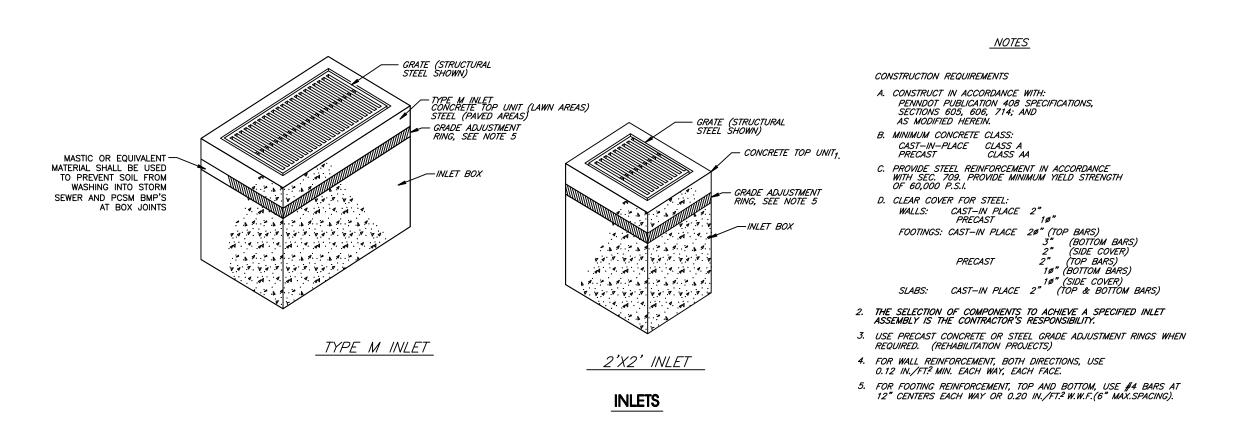


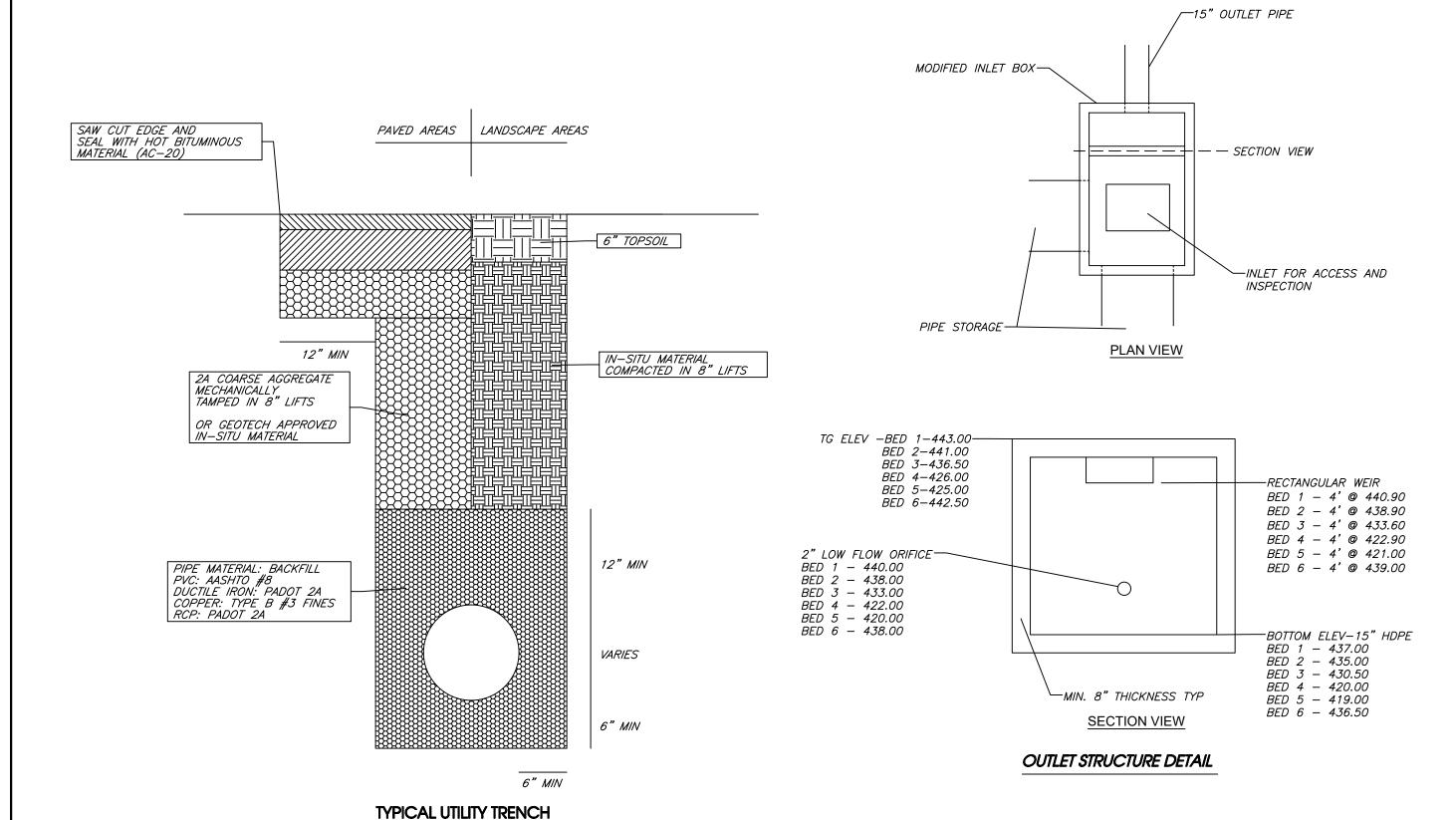


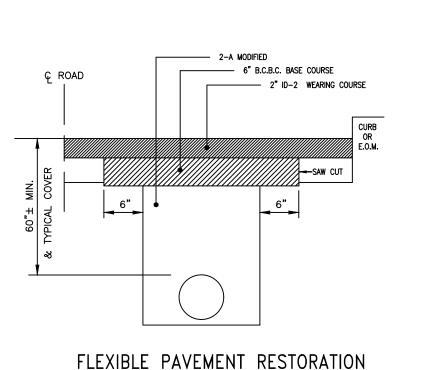


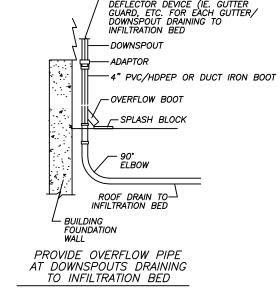


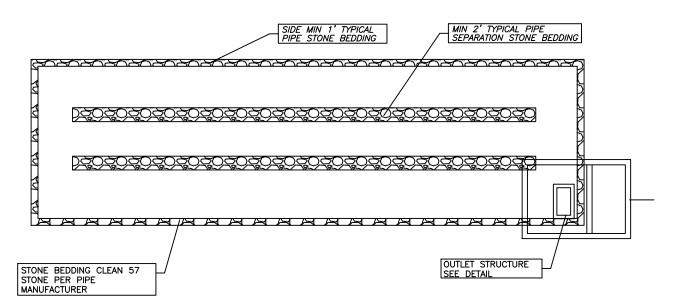








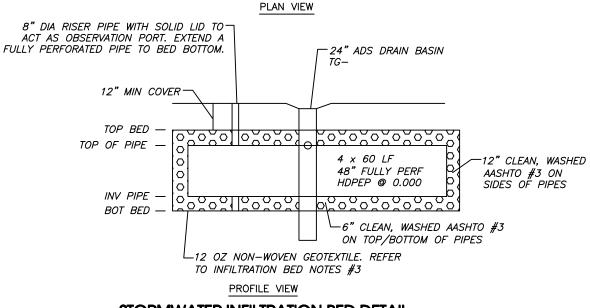




## STORMWATER PIPE STORAGE NOTES

- 1. BEDS 1-6 48" PIPE STORAGES SHALL BE FULLY PERFORATED HDPE. 2. ALL CONVEYANCE PIPING SHALL BE SOLID HDPE WITH WATERTIGHT JOINTS.
- 3. 6" STONE BEDDING BELOW PIPE 4. 6" STONE COVERAGE ABOVE PIPE

### PIPE STORAGE DETAIL



#### STORMWATER INFILTRATION BED DETAIL

PIPE STORAGE CONSTRUCTION SEQUENCE

1. EXCAVATE AREA TO PROPOSED UNCOMPACTED SUBGRADE. 2. PLACE LINER PER MANUFACTURER SPECIFICATIONS ON ALL SIDES 3. CAREFULLY PLACE STONE BEDDING TO NOT DAMAGE LINER. 4. CONSTRUCT PIPE SYSTEM AND OUTLET STRUCTURES. 5. PLACE REMAINING STONE AROUND PIPES. 6. PLACE TOP PORTION OF LINER.

SHORT TERM/ROUTINE MAINTENANCE OF PIPE STORAGE: 1. MAINTENANCE ACTIVITIES TO BE DONE ANNUALLY AND WITHIN 48 HRS AFTER EVERY MAJOR STORM EVENT (>1 INCH RAINFALL DEPTH). 2. KEEPING ALL DEBRIS, INCLUDING GRASS CLIPPINGS, LEAVES, AND MOTOR OIL CLEAR OF INLET AND OUTLET STRUCTURES. 3. REMOVING ANY ACCUMULATED DEBRIS.

4. IF STORM WATER FACILITIES DO NOT DRAIN WITHIN 48 HOURS AFTER THE LAST STORM EVENT, THEY SHALL BE INSPECTED AND THEN FIXED OR REPLACED AS DETERMINED

LONGTERM/NON-ROUTINE MAINTENANCE OF PIPE STORAGE:

- INSECTS AND/OR ODOR BECOME PROBLEMS - STANDING WATER LASTING LONGER THAN 72 HRS - VISIBLE SIGNS OF SEDIMENT ACCUMULATION

1. CLEAR INLETS/PIPES/STONE/PAVING OF DEBRIS, VACUUM, RECONSTRUCT AS

2. REMOVE ACCUMULATED SEDIMENT/POLLUTANTS.

MUST BE AT LEAST 90 HOLES / LINEAR FOOT

REMOVED AND REPLACED.

5. PROVIDE FOR COMPLETION OF A WRITTEN REPORT DOCUMENTATION EACH INSPECTION AND ALL BMP REPAIR AND MAINTENANCE ACTIVITY.

6. CATCH BASINS AND INLETS SHOULD BE INSPECTED AND CLEANED AT LEAST TWO TIMES PER YEAR AND AFTER SIGNIFICANT STORM EVENTS. TIMES FER TEAR AND AFTER SIGNIFICAINT STORM EVENTS.

7. IF STORM WATER FACILITIES DO NOT DRAIN AFTER STORM EVENTS, THEY SHALL
BE INSPECTED AND THEN FIXED OR REPLACED AS DETERMINED NECESSARY. IN THE EVENT THE PIPE STORAGE FAILS THE SYSTEM WILL NEED TO BE COMPLETELY

1. SCARIFY BOTTOM AND SIDES OF BASIN, TAKING CARE NOT TO COMPACT SOIL 2. PIPE MUST HAVE PERFORATIONS NO LESS THAN 5/16" DIAMETER AND PROVIDE AN OPEN AREA OF NOT LESS THAN 3.31 SQUARE INCHES PER SQUARE FOOT OF THE FOLLOWING FORMULA DETERMINES THE # OF HOLES/LINEAR FOOT OF PIPE: [13.24 \* (DIA OF PIPE IN FEET)] / [HOLE SIZE IN INCHES] ^2 , SO FOR A 8" DIA PIPE THERE

3. ENTIRE BED, SIDES TOP AND BOTTOM, SHALL BE WRAPPED IN SYNTHETIC INDUSTRIES NON-WOVEN #1201 OR ENGINEER APPROVED EQUAL. PROVIDE A MINIMUM 1' OVERLAP AT ALL SEAMS AND JOINTS. WHERE PROTRUSIONS OR PENETRATIONS OCCUR, GEOTEXTILE SHALL BE PERMANENTLY AFFIXED TO OBJECT.

4. CARE SHOULD BE TAKEN IN THE PLACING OF STONE ATOP THE GEOTEXTILE SO AS TO AVOID TEARING OR RIPPING OF THE FABRIC. STONE SHOULD NOT BE DUMPED UNTIL A 6" LAYER OF STONE IS IN PLACE.

5. PIPES CONNECTING INTO BEDS AT POINTS OTHER THAN DRAIN BASINS SHALL HAVE A STUB CONNECTION.

# **INFILTRATION BED NOTES**

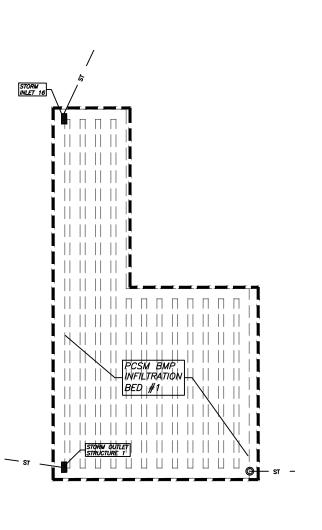
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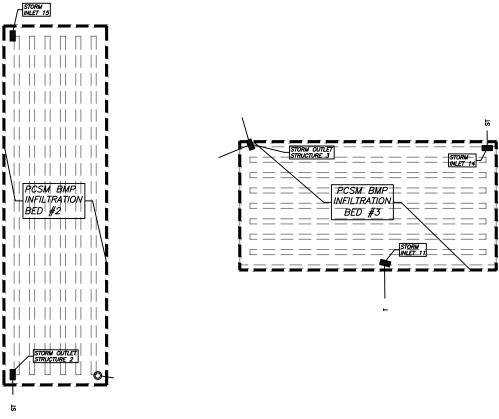
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5. PIPES CONNECTING INTO BEDS AT POINTS OTHER THAN DRAIN BASINS SHALL HAVE A STUB CONNECTION.

6. ALL CMP SHALL BE ALUMINIZED STEEL.





**INFILTRATION BED #1 DETAIL** 1,464 LF 48" HDPE

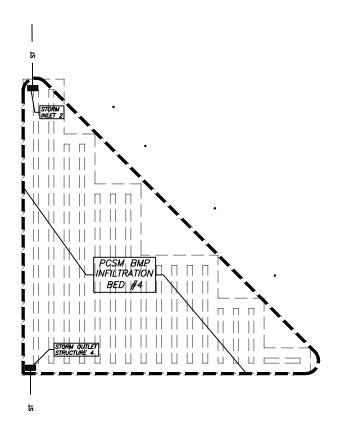
18,396 CF VOLUME

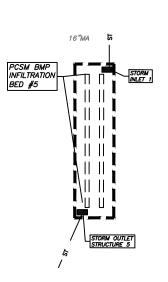
**INFILTRATION BED #2 DETAIL** 1,060 LF 48" HDPE 13,320 CF VOLUME

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**INFILTRATION BED #3 DETAIL** 

892 LF 48" HDPE 11,208 CF VOLUME





**INFILTRATION BED #5 DETAIL** 

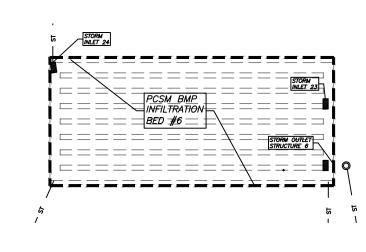
210 LF 48" HDPE 1,980 CF VOLUME

**INFILTRATION BED #4 DETAIL** 

DAVID J. SANDERS, P.E.

PE057436

1,323 LF 48" HDPE 16,624 CF VOLUME



**INFILTRATION BED #6 DETAIL** 972 LF 48" HDPE

12,216 CF VOLUME

NUM. DATE REVISION

PLAN PREPARED BY:

SITE ENGINEERING CONCEPTS, LLC P.O. BOX 1992

SOUTHEASTERN, PA 19399

F: 610-240-0451 E:INFO@SITE-ENGINEERS.COM P: 610-240-0450 PLAN PREPARED FOR: HAMILTON ESTATE

208 & 228 STRAFFORD AVE AND 18 FORREST LANE **WAYNE, PA 19087** 

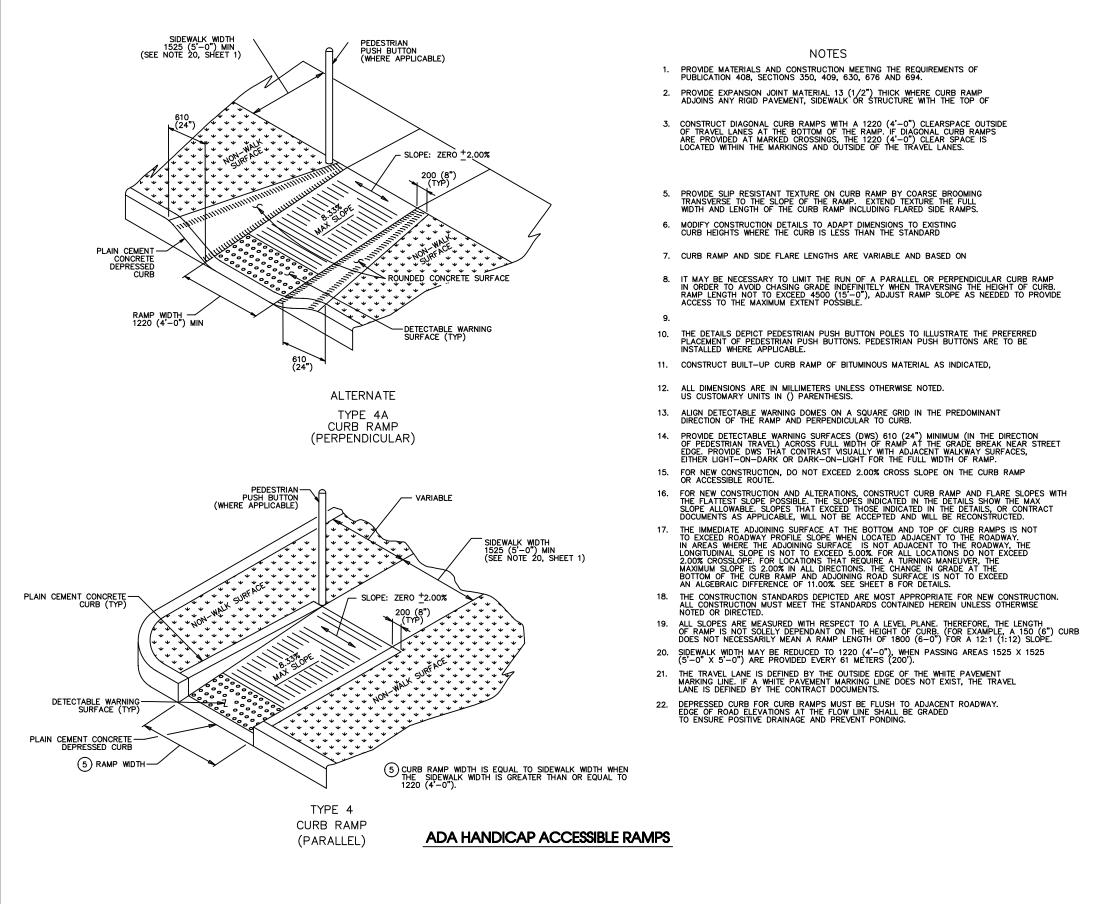
RADNOR TOWNSHIP DELAWARE COUNTY POST CONSTRUCTION

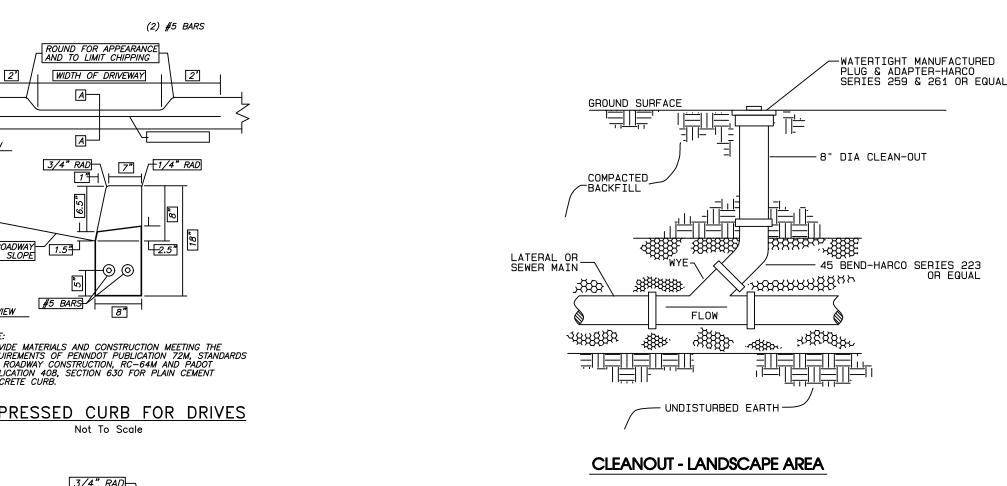
SHEET STORMWATER **DETAILS** 

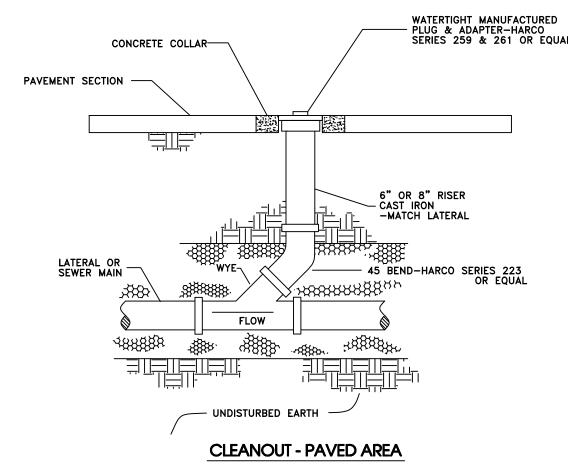
7 of 15 NO SCALE

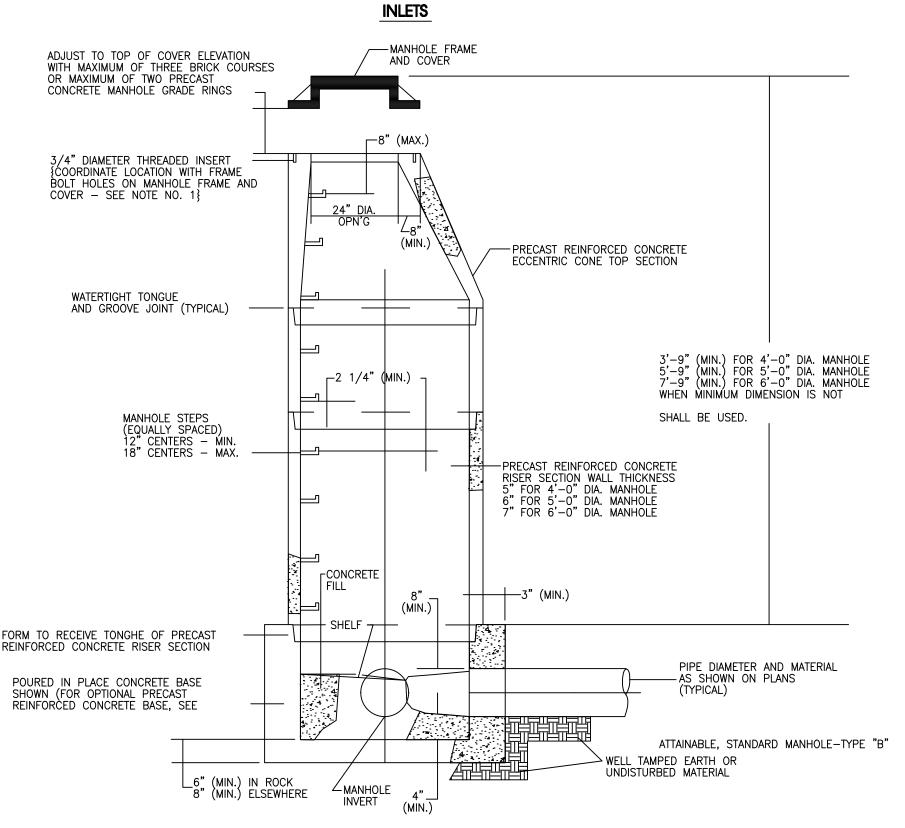
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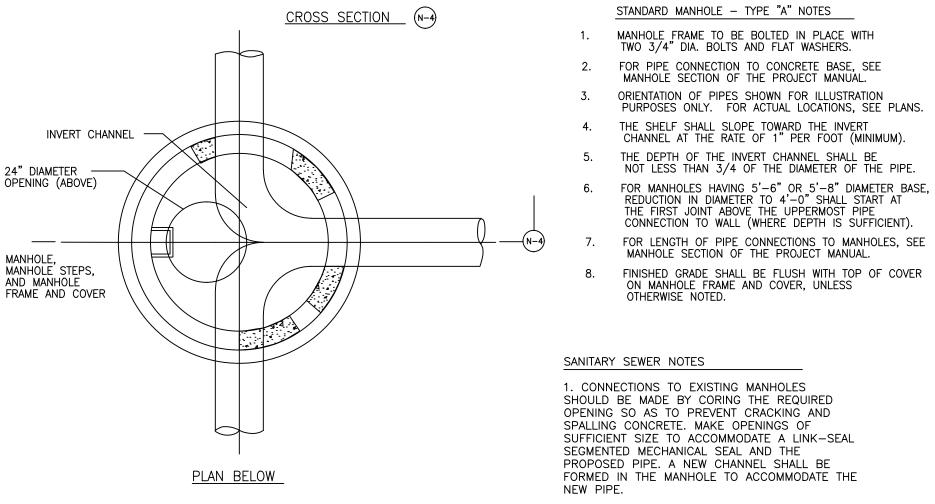
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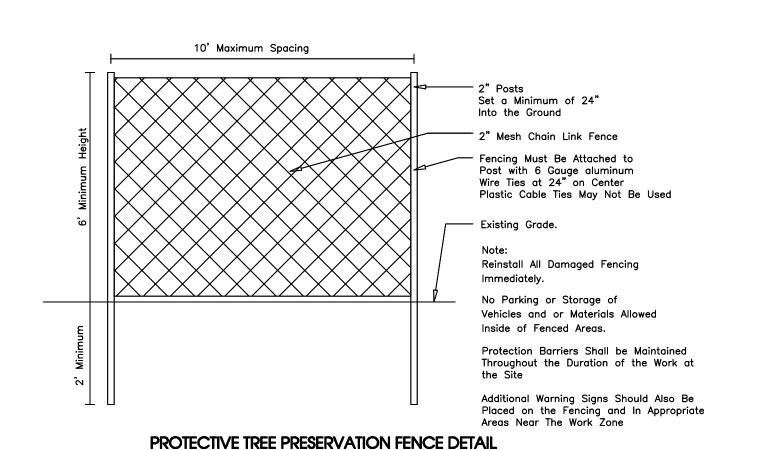


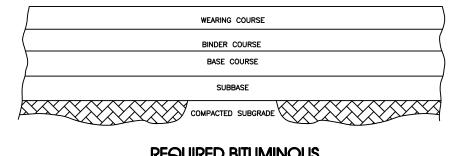




PRE-CAST MANHOLE

ECCENTRIC CONE TOP SECTION





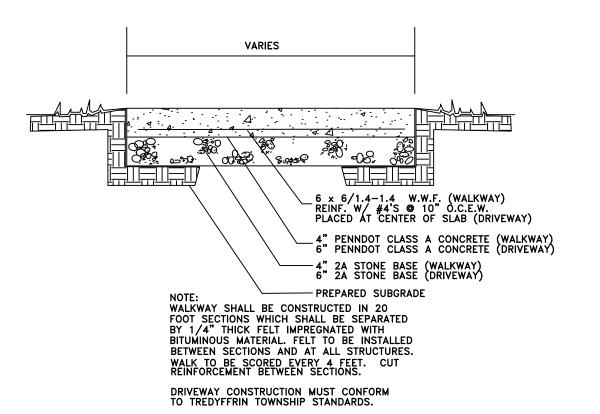
#### **REQUIRED BITUMINOUS** PAVEMENT SECTION N.T.S.

	LOCATION	
DEPTH	STREET	INDIVIDUAL DRIVEWAYS
WEARING:	2"	2"
BINDER:	2" (ID-2 OR FB-1)	NONE
SUBBASE:	8" PA 4A 2" SCREENINGS -VIBRATORY COMPACTED TO FILL VOIDS	8"

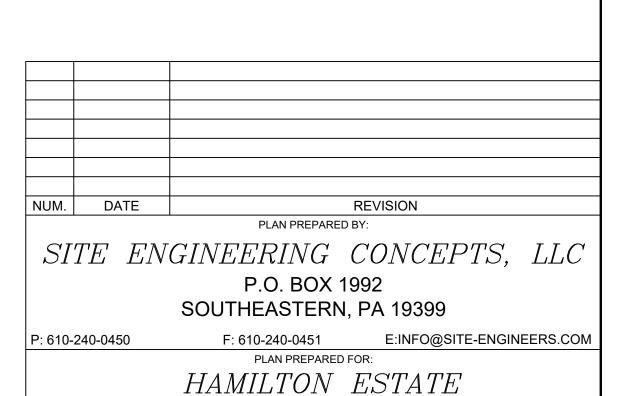
# BITUMINOUS PAVEMENT SPECIFICATIONS, ALL SECTIONS, APPLICABLE

WEARING COURSE	SUPERPAVE, ASPHALT MIXTURE DESIGN, HMA WEARING COURSE, PG 64-22, 3 TO <10 MILLION ESALS, 9.5 MM MUX, SRL-H
BINDER COURSE	SUPERPAVE, ASPHALT MIXTURE DESIGN, HMA WEARING COURSE, PG 64-22, 10 TO <30 MILLION ESALS, 25.0 MM MUX, SRL-H

NOTE: CONSTRUCT COURSE AGGREGATE SUBBASE AND BITUMINOUS PAVEMENT IN ACCORDANCE WITH APPLICABLE SECTIONS OF PADOT PUB. 408.



CONCRETE PAVEMENT (WALKWAY AND DRIVEWAY)



DAVID J. SANDERS DAVID J. SANDERS, P.E.

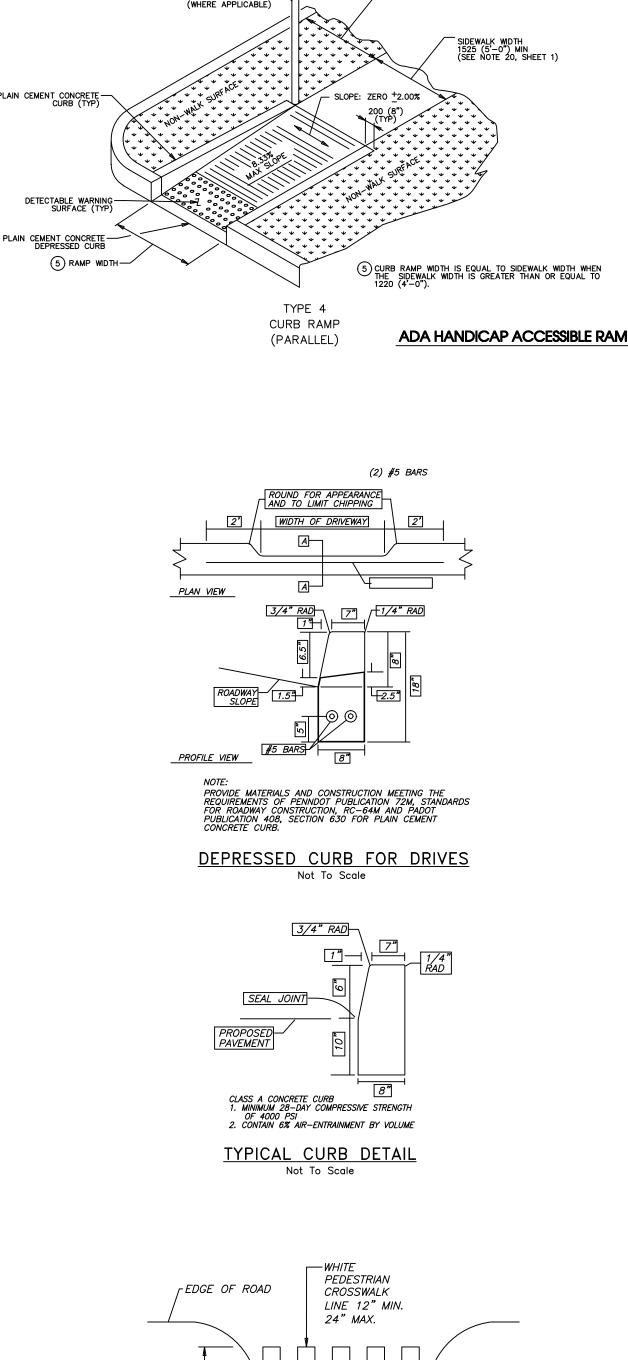
PE057436

208 & 228 STRAFFORD AVE AND 18 FORREST LANE **WAYNE**, PA 19087

RADNOR TOWNSHIP DELAWARE COUNTY PENNSYLVANIA DATE: AUGUST 27, 2020

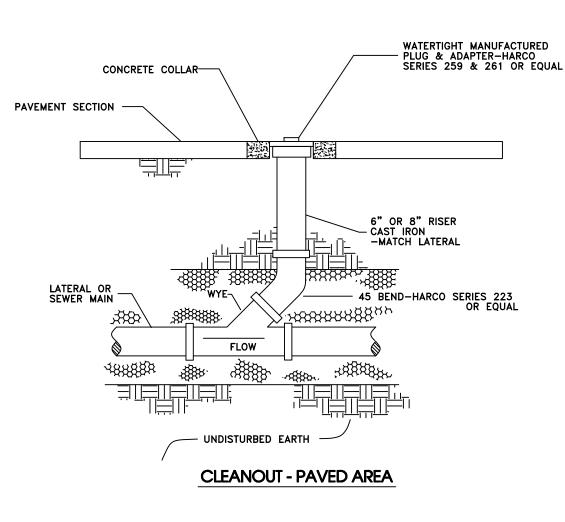
> CONSTRUCTION **DETAILS**

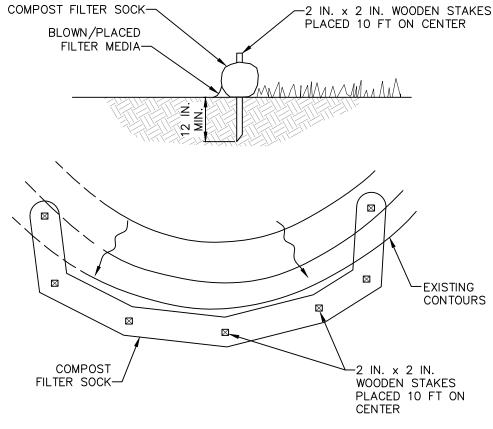
SHEET 8 of 15 NO SCALE



EDGE OF ROAD-

**CROSSWALK DETAIL** 





SOCK FABRIC SHALL MEET STANDARDS OF TABLE 4.1 OF THE PA DEP EROSION CONTROL MANUAL. COMPOST SHALL MEET THE STANDARDS OF TABLE 4.2 OF THE PA DEP EROSION CONTROL MANUAL

COMPOST FILTER SOCK SHALL BE PLACED AT EXISTING LEVEL GRADE. BOTH ENDS OF THE BARRIER SHALL BE EXTENDED AT LEAST 8 FEET UP SLOPE AT 45 DEGREES TO THE MAIN BARRIER ALIGNMENT. MAXIMUM SLOPE LENGTH ABOVE ANY BARRIER SHALL NOT EXCEED THAT SPECIFIED FOR THE SIZE OF THE SOCK AND THE SLOPE OF ITS TRIBUTARY AREA.

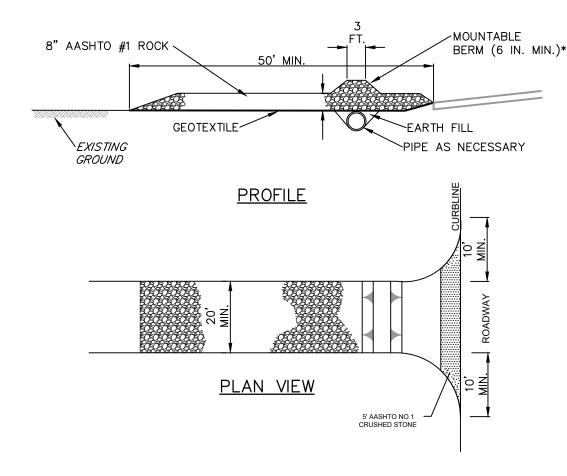
TRAFFIC SHALL NOT BE PERMITTED TO CROSS COMPOST FILTER SOCKS. ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN IT REACHES 1/2 THE ABOVE GROUND HEIGHT OF THE BARRIER AND DISPOSED IN THE MANNER DESCRIBED ELSEWHERE IN THE PLAN. COMPOST FILTER SOCKS SHALL BE INSPECTED WEEKLY AND AFTER EACH RUNOFF EVENT. DAMAGED SOCKS SHALL BE REPAIRED ACCORDING TO MANUFACTURER'S SPECIFICATIONS OR

REPLACED WITHIN 24 HOURS OF INSPECTION. BIODEGRADABLE COMPOST FILTER SOCKS SHALL BE REPLACED AFTER 6 MONTHS; PHOTODEGRADABLE SOCKS AFTER 1 YEAR. POLYPROPYLENE SOCKS SHALL BE REPLACED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS.

UPON STABILIZATION OF THE AREA TRIBUTARY TO THE SOCK, STAKES SHALL BE REMOVED. THE SOCK MAY BE LEFT IN PLACE AND VEGETATED OR REMOVED. IN THE LATTER CASE, THE MESH SHALL BE CUT OPEN AND THE MULCH SPREAD AS A SOIL SUPPLEMENT.

## STANDARD CONSTRUCTION DETAIL #4-1 COMPOST FILTER SOCK

NOT TO SCALE



REMOVE TOPSOIL PRIOR TO INSTALLATION OF ROCK CONSTRUCTION ENTRANCE. EXTEND ROCK OVER FULL WIDTH OF ENTRANCE.

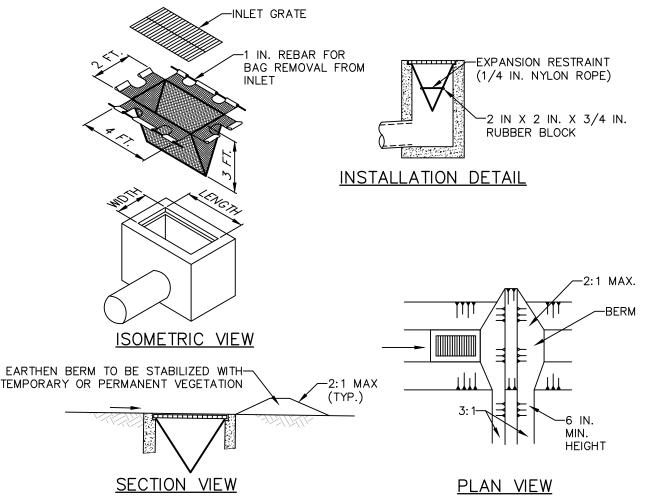
RUNOFF SHALL BE DIVERTED FROM ROADWAY TO A SUITABLE SEDIMENT REMOVAL BMP PRIOR TO ENTERING ROCK CONSTRUCTION ENTRANCE.

MOUNTABLE BERM SHALL BE INSTALLED WHEREVER OPTIONAL CULVERT PIPE IS USED AND PROPER PIPE COVER AS SPECIFIED BY MANUFACTURER IS NOT OTHERWISE PROVIDED. PIPE SHALL BE SIZED APPROPRIATELY FOR SIZE OF DITCH BEING CROSSED.

MAINTENANCE: ROCK CONSTRUCTION ENTRANCE THICKNESS SHALL BE CONSTANTLY MAINTAINED TO THE SPECIFIED DIMENSIONS BY ADDING ROCK. A STOCKPILE SHALL BE MAINTAINED ON SITE FOR THIS PURPOSE. ALL SEDIMENT DEPOSITED ON PAVED ROADWAYS SHALL BE REMOVED AND RETURNED TO THE CONSTRUCTION SITE IMMEDIATELY. IF EXCESSIVE AMOUNTS OF SEDIMENT ARE BEING DEPOSITED ON ROADWAY, EXTEND LENGTH OF ROCK CONSTRUCTION ENTRANCE BY 50 FOOT INCREMENTS UNTIL CONDITION IS ALLEVIATED OR INSTALL WASH RACK. WASHING THE ROADWAY OR SWEEPING THE DEPOSITS INTO ROADWAY DITCHES, SEWERS, CULVERTS, OR OTHER DRAINAGE COURSES IS NOT ACCEPTABLE.

## STANDARD CONSTRUCTION DETAIL #3-1 ROCK CONSTRUCTION ENTRANCE

NOT TO SCALE



MAXIMUM DRAINAGE AREA = 1/2 ACRE.

INLET PROTECTION SHALL NOT BE REQUIRED FOR INLET TRIBUTARY TO SEDIMENT BASIN OR TRAP. BERMS SHALL BE REQUIRED FOR ALL INSTALLATIONS.

ROLLED EARTHEN BERM IN ROADWAY SHALL BE MAINTAINED UNTIL ROADWAY IS STONED. ROAD SUBBASE BERM ON ROADWAY SHALL BE MAINTAINED UNTIL ROADWAY IS PAVED. EARTHEN BERM IN CHANNEL SHALL BE MAINTAINED UNTIL PERMANENT STABILIZATION IS COMPLETED OR

AT A MINIMUM, THE FABRIC SHALL HAVE A MINIMUM GRAB TENSILE STRENGTH OF 120 LBS., A MINIMUM BURST STRENGTH OF 200 PSI, AND A MINIMUM TRAPEZOIDAL TEAR STRENGTH OF 50 LBS. FILTER BAGS SHALL BE CAPABLE OF TRAPPING ALL PARTICLES NOT PASSING A NO. 40

INLET FILTER BAGS SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EACH RUNOFF EVENT. BAGS SHALL BE EMPTIED AND RINSED OR REPLACED WHEN HALF FULL OR WHEN FLOW CAPACITY HAS BEEN REDUCED SO AS TO CAUSE FLOODING OR BYPASSING OF THE INLET. DAMAGED OR CLOGGED BAGS SHALL BE REPLACED. A SUPPLY SHALL BE MAINTAINED ON SITE FOR REPLACEMENT OF BAGS. ALL NEEDED REPAIRS SHALL BE INITIATED IMMEDIATELY AFTER THE INSPECTION. DISPOSE ACCUMULATED SEDIMENT AS WELL AS ALL USED BAGS ACCORDING TO THE PLAN NOTES.

DO NOT USE ON MAJOR PAVED ROADWAYS WHERE PONDING MAY CAUSE TRAFFIC HAZARDS.

# STANDARD CONSTRUCTION DETAIL #4-16 FILTER BAG INLET PROTECTION - TYPE M INLET

DISCHARGE HOSE--INTAKE HOSE HEAVY DUTY LIFTING STRAPS (RECOMMENDED) PLAN VIEW -DISCHARGE HOSE -CLAMPS INTAKE HOSE-**ELEVATION VIEW** 

LOW VOLUME FILTER BAGS SHALL BE MADE FROM NON-WOVEN GEOTEXTILE MATERIAL SEWN WITH HIGH STRENGTH, DOUBLE STITCHED "J" TYPE SEAMS. THEY SHALL BE CAPABLE OF TRAPPING PARTICLES LARGER THAN 150 MICRONS. HIGH VOLUME FILTER BAGS SHALL BE MADE FROM WOVEN GEOTEXTILES THAT MEET THE FOLLOWING STANDARDS:

PROPERTY	TEST METHOD	MINIMUM STANDARD
AVG. WIDE WIDTH STRENGTH	ASTM D-4884	60 LB/IN
GRAB TENSILE	ASTM D-4632	205 LB
PUNCTURE	ASTM D-4833	110 LB
MULLEN BURST	ASTM D-3786	350 PSI
UV RESISTANCE	ASTM D-4355	70%
AOS % RETAINED	ASTM D-4751	80 SIEVE

A SUITABLE MEANS OF ACCESSING THE BAG WITH MACHINERY REQUIRED FOR DISPOSAL PURPOSES SHALL BE PROVIDED. FILTER BAGS SHALL BE REPLACED WHEN THEY BECOME 1/2 FULL OF SEDIMENT. SPARE BAGS SHALL BE KEPT AVAILABLE FOR REPLACEMENT OF THOSE THAT HAVE FAILED OR ARE FILLED. BAGS SHALL BE PLACED ON STRAPS TO FACILITATE REMOVAL UNLESS BAGS COME WITH LIFTING STRAPS ALREADY ATTACHED.

BAGS SHALL BE LOCATED IN WELL-VEGETATED (GRASSY) AREA, AND DISCHARGE ONTO STABLE, EROSION RESISTANT AREAS. WHERE THIS IS NOT POSSIBLE, A GEÓTEXTILE UNDERLAYMENT AND FLOW PATH SHALL BE PROVIDED. BAGS MAY BE PLACED ON FILTER STONE TO INCREASE DISCHARGE CAPACITY. BAGS SHALL NOT BE PLACED ON SLOPES GREATER THAN 5%. FOR SLOPES EXCEEDING 5%, CLEAN ROCK OR OTHER NON-ERODIBLE AND NON-POLLUTING MATERIAL MAY BE PLACED UNDER THE BAG TO REDUCE SLOPE STEEPNESS.

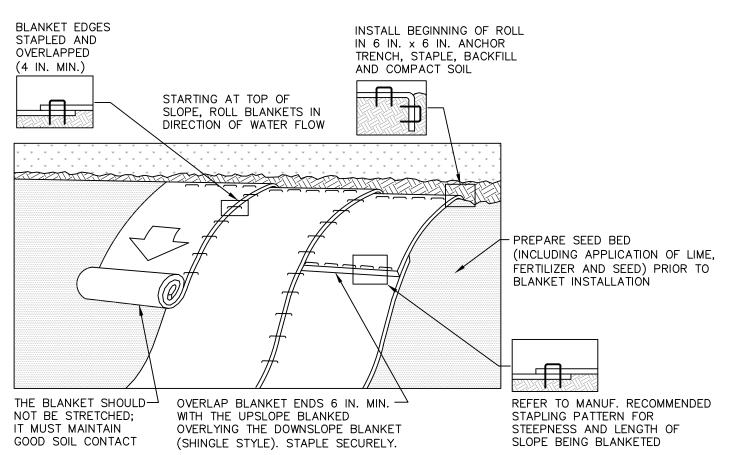
NO DOWNSLOPE SEDIMENT BARRIER IS REQUIRED FOR MOST INSTALLATIONS. COMPOST BERM OR COMPOST FILTER SOCK SHALL BE INSTALLED BELOW BAGS LOCATED IN HQ OR EV WATERSHEDS, WITHIN 50 FEET OF ANY RECEIVING SURFACE WATER OR WHERE GRASSY AREA IS NOT AVAILABLE.

THE PUMP DISCHARGE HOSE SHALL BE INSERTED INTO THE BAGS IN THE MANNER SPECIFIED BY THE MANUFACTURER AND SECURELY CLAMPED. A PIECE OF PVC PIPE IS RECOMMENDED FOR THIS PURPOSE.

THE PUMPING RATE SHALL BE NO GREATER THAN 750 GPM OR 1/2 THE MAXIMUM SPECIFIED BY THE MANUFACTURER, WHICHEVER IS LESS. PUMP INTAKES SHALL BE FLOATING AND SCREENED.

FILTER BAGS SHALL BE INSPECTED DAILY. IF ANY PROBLEM IS DETECTED, PUMPING SHALL CEASE IMMEDIATELY AND NOT RESUME UNTIL THE PROBLEM IS CORRECTED. STANDARD CONSTRUCTION DETAIL #3-16

> PUMPED WATER FILTER BAG NOT TO SCALE



# NOTES:

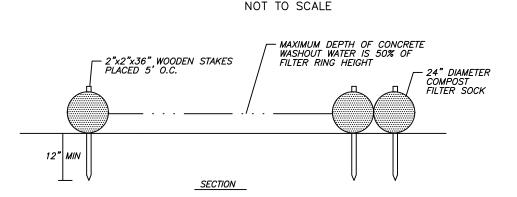
SEED AND SOIL AMENDMENTS SHALL BE APPLIED ACCORDING TO THE RATES IN THE PLAN DRAWINGS PRIOR TO INSTALLING THE BLANKET.

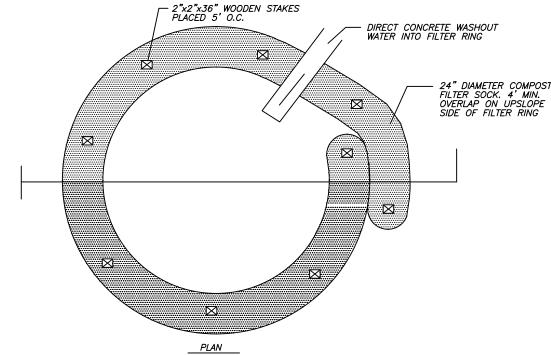
PROVIDE ANCHOR TRENCH AT TOE OF SLOPE IN SIMILAR FASHION AS AT TOP OF SLOPE. SLOPE SURFACE SHALL BE FREE OF ROCKS, CLODS, STICKS, AND GRASS.

BLANKET SHALL HAVE GOOD CONTINUOUS CONTACT WITH UNDERLYING SOIL THROUGHOUT ENTIRE LENGTH. LAY BLANKET LOOSELY AND STAKE OR STAPLE TO MAINTAIN DIRECT CONTACT WITH SOIL. DO NOT STRETCH

THE BLANKET SHALL BE STAPLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. BLANKETED AREAS SHALL BE INSPECTED WEEKLY AND AFTER EACH RUNOFF EVENT UNTIL PERENNIAL VEGETATION IS ESTABLISHED TO A MINIMUM UNIFORM 70% COVERAGE THROUGHOUT THE BLANKETED AREA. DAMAGED OR DISPLACED BLANKETS SHALL BE RESTORED OR REPLACED WITHIN 4 CALENDAR DAYS.

STANDARD CONSTRUCTION DETAIL #11-1 **EROSION CONTROL BLANKET INSTALLATION** 





INSTALLATION NOTES:

1. A SUITABLE IMPERVIOUS GEOMEMBRANE SHALL BE PLACED AT THE LOCATION OF THE WASHOUT PRIOR TO INSTALLING THE SOCKS. CARE SHOULD BE TAKEN TO ENSURE CONTINUOUS CONTACT OF THE SOCK WITH THE GEOMEMBRANE AT ALL LOCATIONS

2. INSTALL ON FLAT GRADE FOR OPTIMUM PERFORMANCE

3. 18" DIAMETER FILTER SOCK MAY BE STACKED ONTO DOUBLE 24" DIAMETER SOCKS IN PYRAMIDAL CONFIGURATION FOR ADDED HEIGHT.

GENERAL NOTES:

1. FOR ANY PROJECT ON WHICH CONCRETE WILL BE POURED OR OTHERWISE FORMED ONSITE, A SUITABLE WASHOUT FACILITY MUST BE PROVIDED FOR CLEANING OF CHUTES, MIXERS, AND HOPPERS FOR THE DELIVERY VEHICLES UNLESS ALL DELIVERY VEHICLES WILL BE CLEANED OFFSITE.

2. UNDER NO CIRCUMSTANCES MAY WASH WATER BE ALLOWED TO ENTER ANY SURFACE WATERS.

3. WASHOUT FACILITIES MUST BE MORE THAN 50 FEET FROM STORM DRAINS, OPEN DITCHES, AND 4. NOTIFICATION MUST BE PROVIDED TO DRIVERS SO THEY ARE AWARE OF THE WASHOUT FACILITIES.

MAINTENANCE NOTES:

1. CONCRETE WASHOUT FACILITIES SHOULD BE INSPECTED DAILY. DAMAGED OR LEAKING WASHOUTS SHOULD BE DEACTIVATED AND REPAIRED OR REPLACED IMMEDIATELY.

2. ACCUMULATED MATERIALS SHOULD BE REMOVED WHEN THEY REACH 75% CAPACITY.

3. PLASTIC LINERS SHOULD BE REPLACED WITH EACH CLEANING OF THE WASHOUT FACILITY.

# CONCRETE WASHOUT DETAIL (USING COMPOST SOCK)

SEEDING AND MULCHING SPECIFICATIONS

4. FOR MORE INFORMATION CONTACT: WEBSITE http://www.filtrexx.com

TEMPORARY-CESSATION OF ACTIVITY FOR 4 DAYS OR LONGER REQUIRES TEMPORARY STABILIZATION. TOPSOIL SHALL BE REPLACED IF NEEDED, REFER TO E&S NOTES FOR REQUIREMENTS

 SEEDING SHALL BE COMMON RYE GRASS APPLIED AT 45 LBS. PER ACRE
 LIMING TO BE APPLIED AT 1 TON/ACRE
 5-5-5 FERTILIZER TO BE APPLIED AT 1000 LBS/ACRE - HAY OR STRAW MULCH TO BE APPLIED AT 3 TONS/ACRE

PERMANENT

- TOPSOIL REPLACEMENT

- SEEDING SHALL BE 15% KENTUCKY BLUEGRASS, 35% KENTUCKY 31 FESCUE, 25% CHEWINGS FESCUE, 15% PERENNIAL RYE GRASS AND 10% RECLEANED REDTOP AT A RATE OF 5 LBS PER 1000 SF

- LIMING TO BE APPLIED AT 3 TONS PER ACRE

- 10-20-20 FERTILIZER TO BE APPLIED AT 1 70 TONS ACRE

HAY OR STRAW MULCH TO BE APPLIED AT 3 TONS/ACRE

THE NON-GERMINATING PERIODS ARE BETWEEN JUNE 15 THRU AUGUST 15 AND SEPTEMBER 30 THRU APRIL 15. AREAS DISTURBED DURING THESE PERIODS MUST BE LIMED, FERTILIZED, SEEDED AND MULCHED IMMEDIATELY.

EROSION CONTROL MAINTENANCE PROGRAM

**TEMPORARY** 

TEMPORARY EROSION CONTROL FACILITIES MAINTENANCE WILL CONSIST OF INSPECTION, CLEANING, REPAIR/REPLACEMENT OF THE ON-SITE EROSION CONTROL FACILITIES THAT ARE SHOWN ON THESE EROSION AND SEDIMENTATION CONTROL FACILITIES THAT ARE SHOWN ON THESE EROSION AND SEDIMENTATION CONTROL PLANS. ALL EROSION AND SEDIMENTATION CONTROL FACILITIES WILL BE CHECKED BY THE CONTRACTOR'S SITE PROJECT MANAGER ON A WEEKLY BASIS AND AFTER EACH STORM EVENT. ALL SEDIMENT MATERIAL COLLECTED BY THE CONTROL FACILITIES WILL BE CLEARED AND REDISTRIBUTE ON—SITE. ANY FACILITIES FOUND TO BE DAMAGED OR MALFUNCTIONING SHALL BE REPAIRED OR REPLACED IMMEDIATELY.

# EROSION AND SEDIMENTATION CONTROL NOTES

1. EROSION AND SEDIMENT BMPS MUST BE CONSTRUCTED, STABILIZED, AND FUNCTIONAL BEFORE SITE DISTURBANCE BEGINS WITHIN THE TRIBUTARY AREAS OF THOSE BMPS.

2. AFTER FINAL SITE STABILIZATION HAS BEEN ACHIEVED, TEMPORARY EROSION AND SEDIMENT BMPS MUST BE REMOVED. AREAS DISTURBED DURING REMOVAL OF THE BMPS MUST BE STABILIZED IMMEDIATELY. 3. VEHICLES AND EQUIPMENT MAY NEITHER ENTER DIRECTLY NOR EXIT DIRECTLY WITHOUT TRAVERSING A ROCK CONSTRUCTION ENTRANCE. 4. STOCKPILE HEIGHTS MUST NOT EXCEED 35 FEET. STOCKPILE SLOPES MUST BE 2:1 OR FLATTER.

6. UNTIL THE SITE IS STABILIZED, ALL EROSION AND SEDIMENT BMPS MUST BE MAINTAINED PROPERLY. MAINTENANCE MUST INCLUDE INSPECTIONS OF ALL EROSION AND SEDIMENT BMPS AFTER EACH RUNOFF EVENT AND ON A WEEKLY BASIS. ALL PREVENTATIVE AND REMEDIAL MAINTENANCE WORK, INCLUDING CLEAN OUT, REPAIR, REPLACEMENT, REGRADING, RESEEDING, REMULCHING AND RENETTING MUST BE PERFORMED IMMEDIATELY, IF EROSION AND SEDIMENT CONTROL BMPS FAIL TO PERFORM AS EXPECTED, REPLACEMENT BMPS, OR MODIFICATIONS OF THOSE INSTALLED WILL BE REQUIRED. 7. ALL SLOPES 4:1 OR STEEPER MUST UTILIZE EROSION CONTROL BLANKET (ECB) AND SEED OR SOD FOR STABILIZATION

8. UNTIL THE SITE IS STABILIZED ALL EROSION AND SEDIMENTATION BMP'S MUST BE MAINTAINED PROPERLY. MAINTENANCE MUST INCLUDE INSPECTIONS OF ALL EROSION AND SEDIMENTATION BMP'S AFTER EACH RUNOFF EVENT AND ON A WEEKLY BASIS. ALL SITE INSPECTIONS WILL BE DOCUMENTED IN AN INSPECTION LOG KEPT FOR THIS PURPOSE. THE COMPLIANCE ACTIONS AND DATE, TIME AND NAME OF PERSON CONDUCTING THE INSPECTION. THE INSPECTION LOG WILL BE KEPT ON—SITE AT ALL TIMES AND MADE AVAILABLE TO THE DISTRICT UPON REQUEST.

9. ALL PREVENTATIVE AND REMEDIAL MAINTENANCE WORK, INCLUDING CLEAN OUT, REPAIR, REPLACEMENT, REGRADING, RESEEDING, REMULCHING AND RENETTING, MUST BE PERFORMED IMMEDIATELY. IF EROSION AND SEDIMENTATION BMP'S FAIL TO PERFORM AS EXPECTED, REPLACEMENT BMP'S OR MODIFICATIONS OF THOSE INSTALLED WILL BE NEEDED. ). WHERE BMP'S ARE FOUND TO FAIL TO ALLEVIATE EROSION AND SEDIMENT DLLUTION THE PERMITTEE OR CO—PERMITTEE SHALL INCLUDE THE FOLLOWING

THE LOCATION AND SEVERITY OF THE BMP'S FAILURE AND ANY POLLUTION B. ALL STEPS TAKEN TO REDUCE, ELIMINATE AND PREVENT THE RECURRENCE OF THE NON-COMPLIANCE.
C. THE TIME FRAME TO CORRECT THE NON-COMPLIANCE, INCLUDING THE EXACT DATES WHEN THE ACTIVITY WILL RETURN TO COMPLIANCE.

11. BEFORE INITIATING ANY REVISION TO THE APPROVED EROSION AND SEDIMENT CONTROL PLAN OR REVISIONS TO OTHER PLANS WHICH AFFECT THE EFFECTIVENESS OF THE APPROVED E&S CONTROL PLAN, THE OPERATOR MUST RECEIVE APPROVAL OF THE REVISIONS FROM THE MONTGOMERY COUNTY CONSERVATION DISTRICT THE OPERATOR SHALL ASSURE THAT THE APPROVED EROSION AND SEDIMENT CONTROL PLAN IS PROPERLY AND COMPLETELY IMPLEMENTED. IMMEDIATELY UPON DISCOVERING UNFORESEEN CIRCUMSTANCES POSING THE POTENTIAL FOR ACCELERATED EROSION AND/OR SEDIMENT POLLUTION, THE OPERATOR SHALL IMPLEMENT APPROPRIATE BEST MANAGEMENT PRACTICES TO ELIMINATE POTENTIAL FOR ACCELERATED EROSION AND/OR SEDIMENT POLLUTION. 12. ALL PUMPING OF SEDIMENT—LADEN WATER, OR POTENTIALLY SEDIMENT LADEN WATER, SHALL BE THROUCH A SEDIMENT CONTROL BMP, SUCH AS A PUMPED WATER FILTER BAG DISCHARGING OVER NON—DISTURBED AREA

# 13. THE CONTRACTOR IS ADVISED TO BECOME THOROUGHLY FAMILIAR WITH THE PROVISIONS OF THE APPENDIX 64, EROSION CONTROL RULES AND REGULATIONS, TITLE 25, PART 1, DEPARTMENT OF ENVIRONMENTAL PROTECTION, SUBPART C, PROTECTION OF NATURAL RESOURCES, ARTICLE III, WATER RESOURCES, CHAPTER 102, EROSION CONTROL. 14. THE OPERATOR SHALL REMOVE FROM THE SITE, RECYCLE, OR DISPOSE OF ALL BUILDING MATERIALS AND WASTES IN ACCORDANCE WITH THE DEPARTMENT'S SOLID WASTE MANAGEMENT REGULATIONS ACT 25 PA CODE 260.1 ET SEQ 271.1 ET SEQ, AND 287.1 ET SEQ. THE CONTRACTOR SHALL NOT ILLEGAL BURY, DUMP OR DISCHARGE ANY BUILDING MATERIAL OR WASTE AT THE SITE.

15. A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN MUST BE AVAILABLE AT THE PROJECT SITE AT ALL TIMES. THE OPERATOR SHALL ASSURE THAT AN EROSION AND SEDIMENT CONTROL PLAN HAS BEEN PREPARED, APPROVED BY THE MONTGOMERY COUNTY CONSERVATION DISTRICT, AND IS BEING IMPLEMENTED AND MAINTAINED FOR ALL SOIL AND/OR ROCK BORROW AREAS, REGARDLESS OF THEIR LOCATIONS. 6. THE NPDES BOUNDARY IS EQUAL TO THE OUTER PERIMETER BOUNDARY OF HE SITE, AND ANY OFF—SITE AREAS WITHIN THE LIMITS OF DISTURBANCE THAT ARE HE RESPONSIBILITY OF THE DEVELOPER TO INSTALL, INCLUDING OFF—SITE FACILITIES SUCH AS UTILITIES AND ROADWAY IMPROVEMENTS.

17. AN AREA SHALL BE CONSIDERED TO HAVE ACHIEVED FINAL STABILIZATION WHEN IT HAS A MINIMUM UNIFORM 70% PERENNIAL VEGETATIVE COVER OR OTHER PERMANENT NON-VEGETATIVE COVER WITH A DENSITY SUFFICIENT TO RESIST ACCELERATED SURFACE EROSION AND SUBSURFACE CHARACTERISTICS SUFFICIENT TO RESIST SLIDING AND OTHER MOVEMENTS. IMMEDIATELY AFTER EARTH DISTURBANCE ACTIVITIES CEASE, THE OPERATOR SHALL STABILIZE ANY AREAS DISTURBED BY THE ACTIVITIES. DURING NON-GERMINATING PERIODS, MULCH MUST BE APPLIED AT THE SPECIFIED RATES. DISTURBED AREAS WHICH ARE NOT AT FINISHED GRADE AND WHICH WILL BE REDISTURBED WITHIN 1 YEAR MUST BE STABILIZED IN ACCORDANCE WITH THE TEMPORARY VEGETATIVE STABILIZATION SPECIFICATIONS. DISTURBED AREAS WHICH ARE AT FINISHED GRADE OR WHICH WILL NOT BE REDISTURBED WITHIN 1 YEAR MUST BE STABILIZED IN ACCORDANCE WITH THE PERMANENT VEGETATIVE STABILIZATION SPECS.

19. RUNOFF CROSSING TO THE ADJACENT PROPERTY DURING THE CONSTRUCTION PHASE OF THE PROJECT SHALL BE MANAGED SO THAT THE WATER QUALITY/QUANTITY IMPACT IS MINIMIZED TO THE ADJACENT PROPERTY. DIVERSION BERMS, STONED STAGING AREAS, AND INLETS/PIPING SHALL BE PROVIDED AS NEEDED TO INSURE ACCEPTABLE CONDITIONS DURING CONSTRUCTION.

18. TOPSOIL REMOVED BY GRADING OPERATIONS SHALL BE REDISTRIBUTED AND STABILIZED AS QUICKLY AS POSSIBLE FOLLOWING THE COMPLETION OF THE PROJECT PHASE

# UTILITY LINE TRENCH EXCAVATION NOTES

A. LIMIT ADVANCED CLEARING AND GRUBBING OPERATIONS TO A DISTANCE EQUAL TO TWO TIMES THE LENGTH OF PIPE INSTALLATION THAT CAN BE COMPLETED IN ONE DAY.

B. WORK CREWS AND EQUIPMENT FOR TRENCHING, PLACEMENT OF PIPE, PLUG CONSTRUCTION AND BACKFILLING WILL BE SELF-CONTAINED AND SEPARATE FROM CLEARING AND GRUBBING AND SITE RESTORATION AND STABILIZATION OPERATIONS.

C. ALL SOIL EXCAVATED FROM THE TRENCH WILL BE PLACED ON THE UPHILL SIDE OF THE TRENCH.

D. LIMIT DAILY TRENCH EXCAVATION TO THE LENGTH OF PIPE PLACEMENT, PLUG INSTALLATION AND BACKFILLING THAT CAN BE COMPLETED THE SAME DAY.

E. WATER WHICH ACCUMULATES IN THE OPEN TRENCH WILL BE COMPLETELY REMOVED BY PUMPING BEFORE PIPE PLACEMENT AND/OR BACKFILLING BEGINS. WATER REMOVED FROM THE TRENCH SHALL BE PUMPED THROUGH A FILTRATION DEVICE.

F. ON THE DATE FOLLOWING PIPE PLACEMENT AND TRENCH BACKFILLING, THE DISTURBED AREA WILL BE GRADED TO FINAL CONTOURS AND IMMEDIATELY STABILIZED.

# **CLEAN FILL NOTES**

IF THE SITE WILL NEED TO IMPORT OR EXPORT MATERIAL FROM THE SITE, THE RESPONSIBILITY FOR PERFORMING ENVIRONMENTAL DUE DILIGENCE AND DETERMINATION OF CLEAN FILL WILL REST WITH BRYN MAWR COLLEGE.

APPENDIX C - STANDARD E&S PLAN NOTES

1.All earth disturbances, including clearing and grubbing as well as cuts and fills shall be done in accordance with the approved E&S plan. A copy of the approved drawings (stamped, signed and dated by the reviewing agency) must be available at the project site at all times. The reviewing agency shall be notified of any changes to the approved plan prior to implementation of those changes. The reviewing agency may require a written submittal of those changes for review and

approval at its discretion. 2. At least 7 days prior to starting any earth disturbance activities, including clearing and grubbing, the owner and/or operator shall invite all contractors, the landowner, appropriate municipal officials, the E&S plan preparer, the PCSM plan preparer, the licensed professional responsible for oversight of critical stages of implementation of the PCSM plan, and a representative from the local conservation district to an on-site preconstruction meeting.

3. At least 3 days prior to starting any earth disturbance activities, or expanding into an area previously unmarked, the Pennsylvania One Call System Inc. shall be notified at 1-800-242-1776 for the location of existing underground utilities. 4. All earth disturbance activities shall proceed in accordance with the sequence provided on the plan

drawings. Deviation from that sequence must be approved in writing from the local conservation district or by the Department prior to implementation.

5. Areas to be filled are to be cleared, grubbed, and stripped of topsoil to remove trees, vegetation, roots and other objectionable material.

6. Clearing, grubbing, and topsoil stripping shall be limited to those areas described in each stage of the construction sequence. General site clearing, grubbing and topsoil stripping may not commence in any stage or phase of the project until the E&S BMPs specified by the BMP sequence for that stage or phase have been installed and are functioning as described in this E&S plan.

7. At no time shall construction vehicles be allowed to enter areas outside the limit of disturbance boundaries shown on the plan maps. These areas must be clearly marked and fenced off before clearing and grubbing operations begin.

8. Topsoil required for the establishment of vegetation shall be stockpiled at the location(s) shown on the plan maps(s) in the amount necessary to complete the finish grading of all exposed areas that are to be stabilized by vegetation. Each stockpile shall be protected in the manner shown on the plan drawings. Stockpile heights shall not exceed 35 feet. Stockpile slopes shall be 2H:1V or flatter. 9. Immediately upon discovering unforeseen circumstances posing the potential for accelerated erosion and/or sediment pollution, the operator shall implement appropriate best management practices to minimize the potential for erosion and sediment pollution and notify the local conservation district

and/or the regional office of the Department. 10. All building materials and wastes shall be removed from the site and recycled or disposed of in accordance with the Department's Solid Waste Management Regulations at 25 Pa. Code 260.1 et seq., 271.1, and 287.1 et. seq. No building materials or wastes or unused building materials shall be

burned, buried, dumped, or discharged at the site. 11. All off—site waste and borrow areas must have an E&S plan approved by the local conservation district or the Department fully implemented prior to being activated.

12. The contractor is responsible for ensuring that any material brought on site is clean fill. Form FP-001 must be retained by the property owner for any fill material affected by a spill or release of a regulated substance but qualifying as clean fill due to analytical testing.

13. All pumping of water from any work area shall be done according to the procedure described in this plan, over undisturbed vegetated areas.

14. Vehicles and equipment may neither enter directly nor exit directly from lots onto 15. Until the site is stabilized, all erosion and sediment BMPs shall be maintained properly. Maintenance shall include inspections of all erosion and sediment BMPs after each runoff event and on a weekly basis. All preventative and remedial maintenance work, including clean out, repair, replacement, regrading, reseeding, remulching and renetting must be performed immediately. If the E&S BMPs fail to perform as expected, replacement BMPs, or modifications of those installed will be required. 16. A log showing dates that E&S BMPs were inspected as well as any deficiencies found and the date they were corrected shall be maintained on the site and be made available to regulatory agency

officials at the time of inspection. 17. Sediment tracked onto any public roadway or sidewalk shall be returned to the construction site by the end of each workday and disposed in the manner described in this plan. In no case shall the sediment be washed, shoveled, or swept into any roadside ditch, storm sewer, or surface water.

18. All sediment removed from BMPs shall be disposed of in the manner described on the plan drawings. 19. Areas which are to be topsoiled shall be scarified to a minimum depth of 3 to 5 inches -6 to 12 inches on compacted soils — prior to placement of topsoil. Areas to be vegetated shall have a minimum 4 inches of topsoil in place prior to seeding and mulching. Fill outslopes shall have a

minimum of 2 inches of topsoil. 20. All fills shall be compacted as required to reduce erosion, slippage, settlement, subsidence or other related problems. Fill intended to support buildings, structures and conduits, etc. shall be compacted in accordance with local requirements or codes.

21. All earthen fills shall be placed in compacted layers not to exceed 9 inches in thickness. 22. Fill materials shall be free of frozen particles, brush, roots, sod, or other foreign or objectionable materials that would interfere with or prevent construction of satisfactory fills. 23. Frozen materials or soft, mucky, or highly compressible materials shall not be incorporated into fills.

24. Fill shall not be placed on saturated or frozen surfaces. 25. Seeps or springs encountered during construction shall be handled in accordance with the standard and specification for subsurface drain or other approved method. 26. All graded areas shall be permanently stabilized immediately upon reaching finished grade. Cut

21. GRADING AND EARTHMOVING OPERATIONS SHALL BE MINIMIZED DURING THE PERIOD FROM NOVEMBER 15 TO APRIL 1 WHEN RE-VEGETATION OF EXPOSED GROUND SURFACE IS DIFFICULT. MULCH, STRAW, STONE AND/OR SOD SHALL BE USED TO STABILIZE ALL AREAS DENUDED DURING THIS TIME PERIOD.

Slopes in competent bedrock and rock fills need not be vegetated. Seeded areas within 50 feet of a surface water, or as otherwise shown on the plan drawings, shall be blanketed according to the standards of this plan. standards of this plan.

27. Immediately after earth disturbance activities cease in any area or subarea of the project, the operator shall stabilize all disturbed areas. During non-germinating months, mulch or protective blanketing shall be applied as described in the plan. Areas not at finished grade, which will be reactivated within 1 year, may be stabilized in accordance with the temporary stabilization specifications. Those areas which will not be reactivated within 1 year shall be stabilized in accordance with the permanent stabilization specifications.

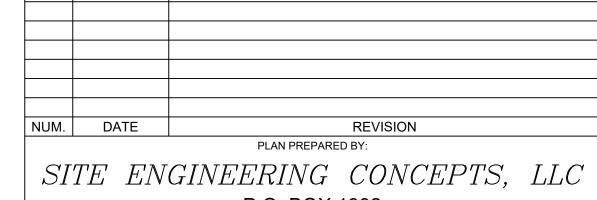
28. Permanent stabilization is defined as a minimum uniform, perennial 70% vegetative cover or other permanent non-vegetative cover with a density sufficient to resist accelerated erosion. Cut and fill slopes shall be capable of resisting failure due to slumping, sliding, or other movements.

29. E&S BMPs shall remain functional as such until all areas tributary to them are permanently stabilized or until they are replaced by another BMP approved by the local conservation district or the Department.

30. Upon completion of all earth disturbance activities and permanent stabilization of all disturbed areas, the owner and/or operator shall contact the local conservation district for an inspection prior to removal/conversion of the E&S BMPs.

31. After final site stabilization has been achieved, temporary erosion and sediment BMPs must be removed or converted to permanent post construction stormwater management BMPs. Areas disturbed during removal or conversion of the BMPs shall be stabilized immediately. In order to ensure rapid revegetation of disturbed areas, such removal/conversions are to be done only during the germinating

32. Upon completion of all earth disturbance activities and permanent stabilization of all disturbed areas, the owner and/or operator shall contact the local conservation district to schedule a final inspection. 33. Failure to correctly install E&S BMPs, failure to prevent sediment-laden runoff from leaving the construction site, or failure to take immediate corrective action to resolve failure of E&S BMPs may result in administrative, civil, and/or criminal penalties being instituted by the Department as defined in Section 602 of the Pennsylvania Clean Streams Law. The Clean Streams Law provides for up to \$10,000 per day in civil penalties, up to \$10,000 in summary criminal penalties, and up to \$25,000 in misdemeanor criminal penalties for each violation.



P.O. BOX 1992 SOUTHEASTERN. PA 19399

P: 610-240-0450 F: 610-240-0451

E:INFO@SITE-ENGINEERS.COM PLAN PREPARED FOR HAMILTON ESTATE

208 & 228 STRAFFORD AVE AND 18 FORREST LANE

**WAYNE, PA 19087** 

**DELAWARE COUNTY** 

RADNOR TOWNSHIP

DAVID J. SANDERS

DAVID J. SANDERS, P.E. PE057436

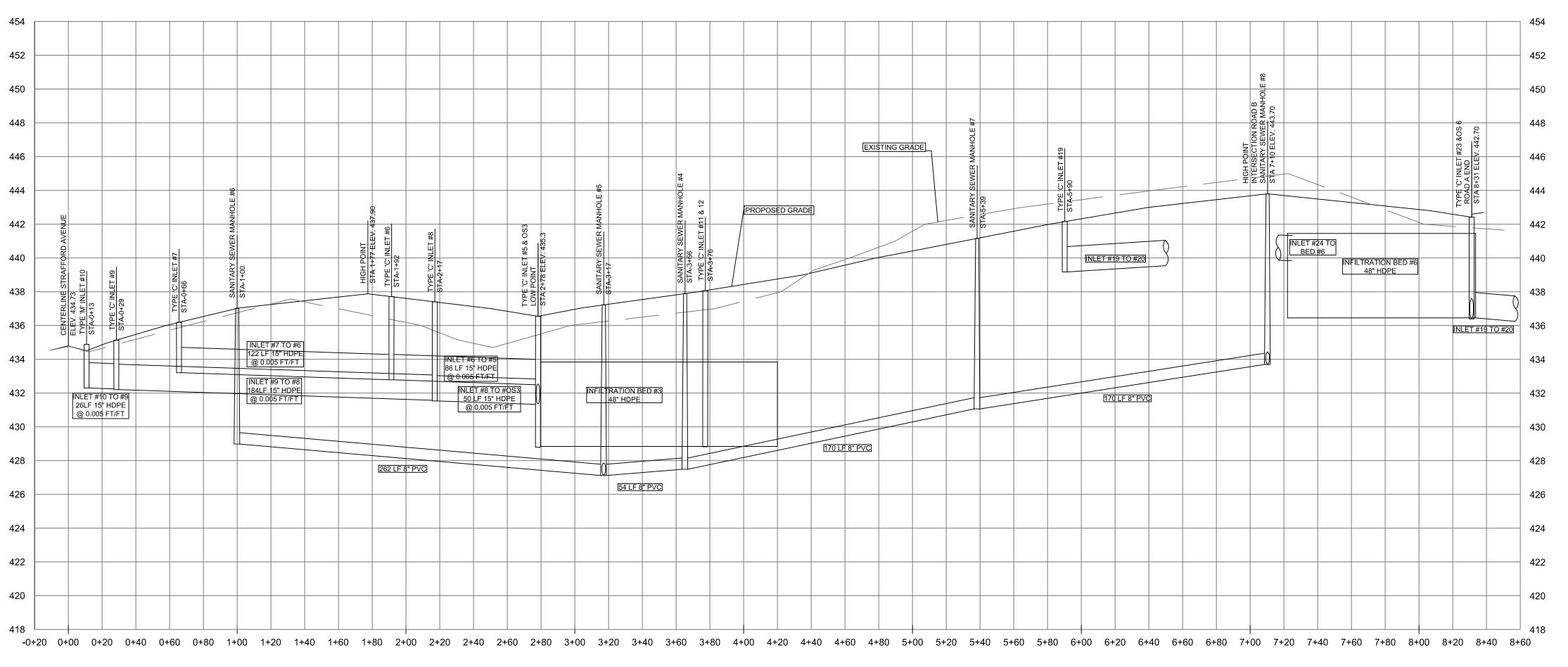
**EROSION AND SEDIMENTATION CONTROL DETAILS** 

SHEET 9 of 15

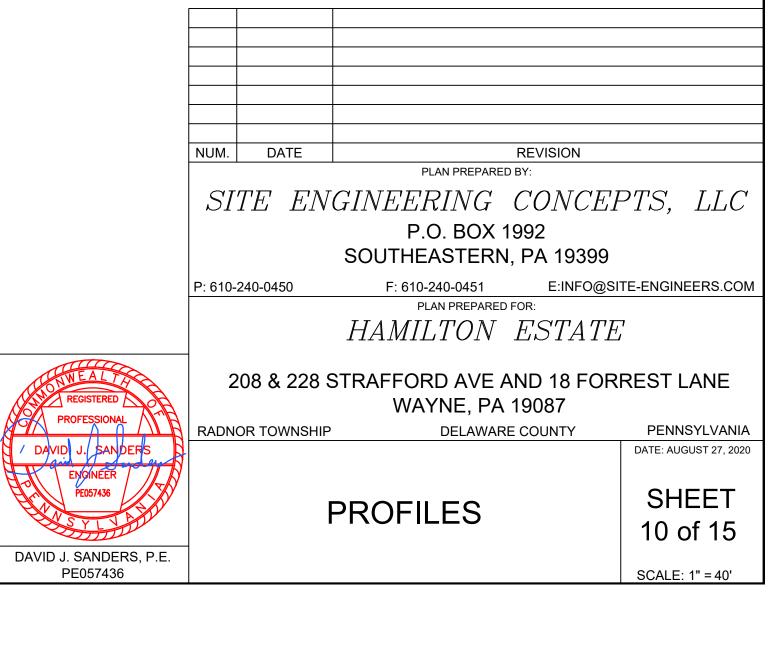
PENNSYLVANIA

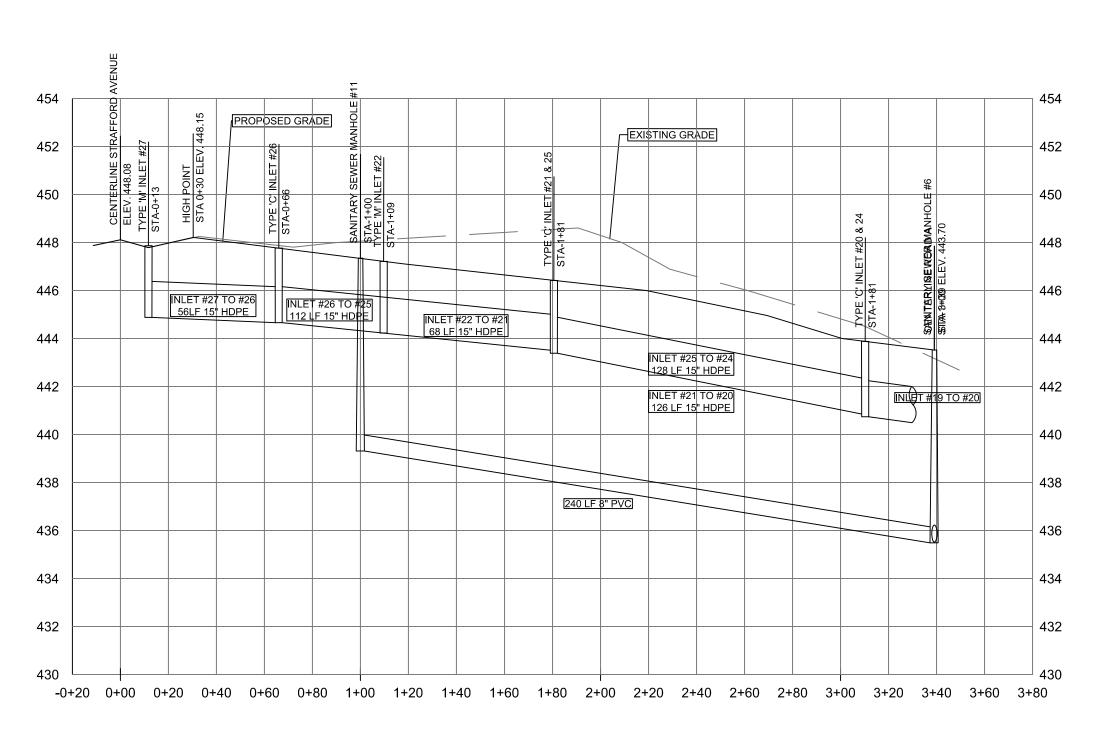
DATE: AUGUST 27, 2020

NO SCALE

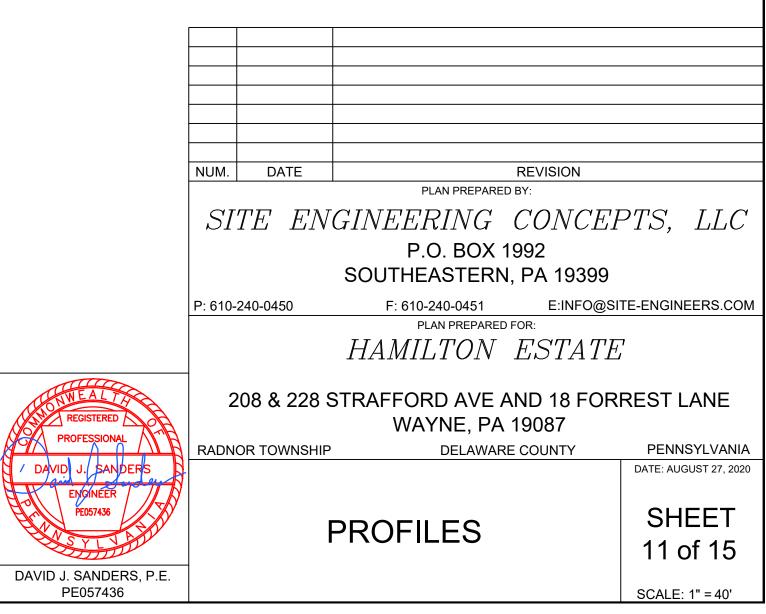


ROAD A PROFILE





ROAD B PROFILE



# GENERAL NOTES:

- Base information including topography, grading, utilities, building and existing vegetation location prepared by Site
- 3. The Landscape Architect's seal applies only to the landscape
- 4. All plant material is to be field adjusted as necessary to address existing site conditions, proposed unit landscaping, and to ensure that clear sight triangles and clear sight lines are
- 5. Proposed shade trees shall not be planted over the top of any sewer or water laterals, mains or services. Sewer and water laterals, mains and services are shown on Landscape Plans for
- reference only. See engineers plans for location of all utilities. Utilities are shown on Landscape Plans for reference only. See engineers plans for location of all utilities.
- All trees shall be pruned as necessary to maintain required light

# COMPLIANCE WITH SECTION 255-38 Subsection B

CHADE TREEC	DEOLUBED	EXISTING	DDODOCED
SHADE TREES	REQUIRED	TO REMAIN	PROPOSED
Existing local street length: 789'			
1 Street tree required at intervals not less than 30' on 1 side	27	28	0
Proposed road length 2,149':			
1 Street tree required at intervals not less than 30' on both sides	71	0	71 (6 shown along trails)

# COMPLIANCE WITH SECTION 255-42 (Tables 1 and 2)

BUFFER SCREENS	LENGTH	REQUIRED	EXISTING TO REMAIN	PROVIDED
Buffer screen class 'A' required between proposed Planned Cluster Development use and Major Collector Street.	450'	11 canopy trees 15 evergreen trees	8 canopy trees 6 evergreen trees	3 canopy trees 9 evergreen trees
Buffer screen class 'B' required between proposed Planned Cluster Development use and existing Retail use.	585'	39 evergreen trees 73 evergreen shrubs	0 0	39 evergreen trees 73 evergreen shrubs

# **COMPLIANCE WITH SECTION 263-4**

TREE REPLACEMENT	# TREES REMOVED	REPLACEMENTS REQUIRED	REPLACEMENTS PROVIDED
1 Replacement tree required for every 6"-18" DBH tree removed	156	156	156
3 Replacement trees required for every 19"-29" DBH tree removed (2) being large canopy trees	24	72 total (48 canopy)	72 total (48 canopy)
6 Replacement trees required for every 30"+ DBH tree removed (4) being large canopy trees	6	36 total (24 canopy)	36 total (24 canopy)
N	187	264 total (72 to be canopy trees)	264 total (72 to be canopy trees)

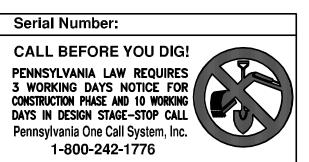
Replacement trees for hazardous and dead trees are not provided.

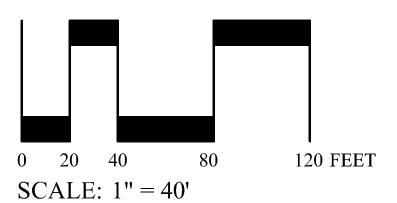
# COMPLIANCE WITH SECTION 280-91-G

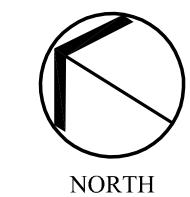
BUFFER	REQUIRED	PROVIDED
Density Modification development requires	25' buffer	25' buffer
a 25' buffer along all residentially zoned districts		

KEY	QTY	BOTANICAL NAME	COMMON NAME	SIZE	REMARKS
DECID	OUOUS TR	EES			
ACSG	13	Acer saccharum 'Green Mountain'	Green Mountain Sugar Maple	2"-2.5" cal.	B&B
BENH	3	Betula nigra 'Heritage'	Heritage River Birch	2"-2.5" cal.	B&B
CECF	9	Cercis canadensis 'Forest Pansy'	Forest Pansy Redbud	2"-2.5" cal.	B&B
COR	6	Cornus 'Rutcan'	Constellation Flowering Dogwood	2"-2.5" cal.	B&B
CORG	7	Cornus 'Rutgan'	Stellar Pink Flowering Dogwood	2"-2.5" cal.	B&B
FAGR	14	Fagus grandfolia	American Beech	2"-2.5" cal.	B&B
GLTI	28	Gleditsia triacanthos v. inermis 'Shademaster'	Shademaster Honeylocust	2"-2.5" cal.	B&B
MAV	14	Magnolia virginiana	Sweet Bay Magnolia	2"-2.5" cal.	B&B
NYSR	11	Nyssa sylvatica 'Red Rage'	Red Rage Black Gum	2"-2.5" cal.	B&B
PLAB	24	Platanus x acerifolia 'Bloodgood'	Bloodgood London Planetree	2"-2.5" cal.	B&B
QUB	11	Quercus bicolor	Swamp White Oak	2"-2.5" cal.	B&B
QUP	25	Quercus phellos	Willow Oak	2" <b>-</b> 2.5" cal.	B&B
ÛLAP	20	Ulmus americana 'Princeton'	Princeton Elm	2"-2.5" cal.	B&B
EVER	GREEN TE	REES			
ABC	8	Abies concolor	White Fir	8'-10' ht.	В&В
CRJY	25	Cryptomeria japonica 'Yoshino'	Yoshino Cryptomeria	8'-10' ht.	B&B
ILO	14	Ilex opaca	American Holly	8'-10' ht.	B&B
PIA	33	Picea abies	Norway Spruce	8'-10' ht.	B&B
PIG	17	Picea glauca	White Spruce	8'-10' ht.	B&B
PIO	43	Picea orientalis	Oriental Spruce	8'-10' ht.	B&B
PISF	8	Pinus strobus 'Fastigiata'	Columnar White Pine	8'-10' ht.	B&B
THGG	53	Thuja plicata 'Green Giant'	Green Giant Arborvitae	8'-10' ht.	B&B
SHRUI	BS				
PRLS	41	Prunus laurocerasus 'Schipkaensis'	Skip Laurel	24"-30" ht.	Cont.
VIRC	32	Viburnum rhytidophyllum 'Cree'	Cree Leatherleaf Viburnum	24"-30" ht.	Cont.

1. Where there is a discrepancy between the plant list and plan, the plan shall prevail.





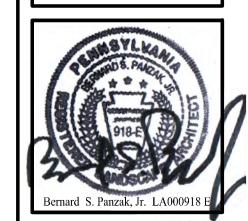


**GLACKIN THOMAS PANZAK** LAND PLANNING LANDSCAPE

Glackin Thomas Panzak, Inc

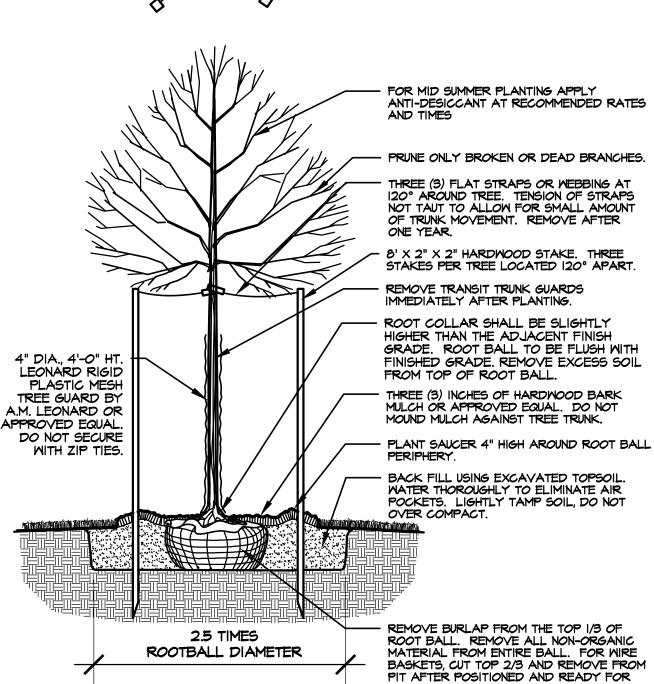
**ARCHITECTURE** 

Paoli Executive Green 1 41 Leopard Road Paoli, Pennsylvania 1930 610.408.901 Fax: 610.408.9477 E-mail: plans@glackinplan.com



amilton

DATE: 08/27/2020 (jr)



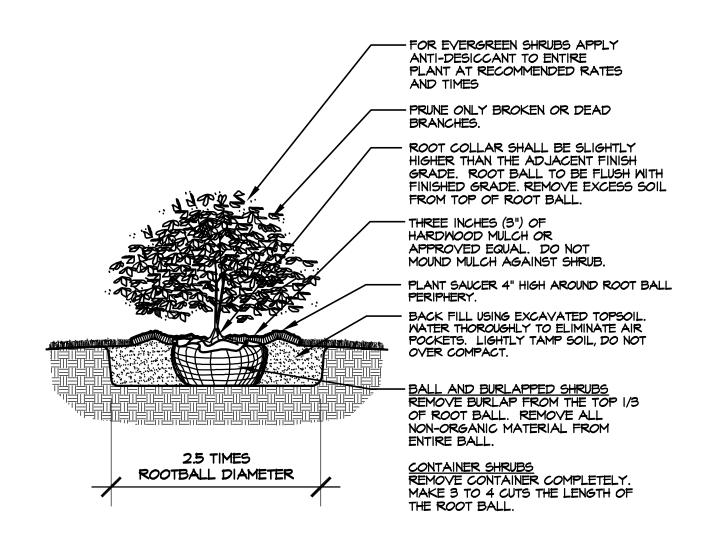
GUY SPACING DIAGRAM

1. Trees with poor quality root balls or root balls that have been cracked or damaged shall be rejected. 2. Trees with central leader broken or dead shall be rejected

3. Trees that do not display the typical characteristics for their species shall be rejected.

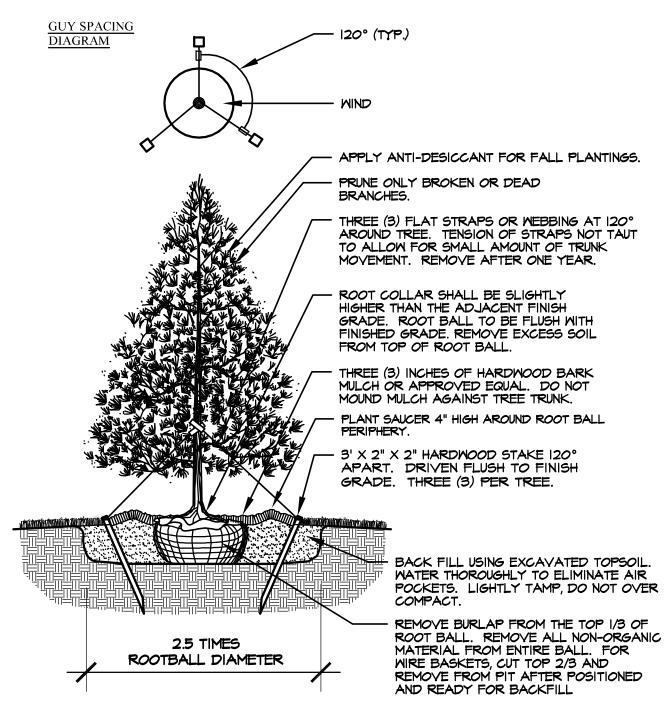
4. Flood planting pit with water twice within 24 hours of planting.

# DECIDUOUS TREE WITH STAKES DETAIL



NOTE: FLOOD PLANTING PIT WITH WATER TWICE WITHIN 24 HOURS OF PLANTING.

SHRUB DETAIL



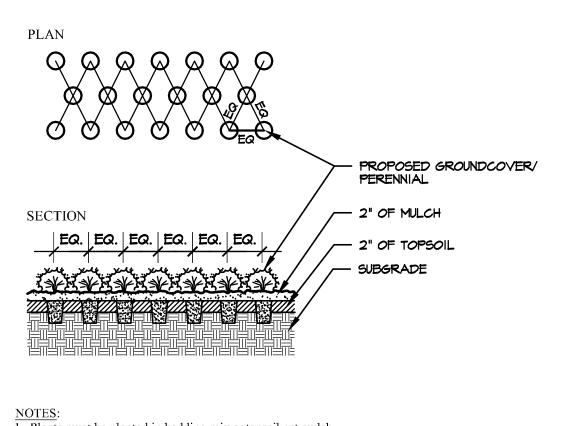
1. Trees with poor quality root balls or root balls that have been cracked or damaged shall be rejected.

2. Trees with central leader broken or dead shall be rejected 3. Trees that do not display the typical characteristics for their species shall be rejected.

4. The owner shall be responsible for contacting the contractor, who shall right and secure any tree that leans out

5. Flood planting pit with water twice within 24 hours of planting.

# EVERGREEN TREE WITH STAKES DETAIL



1. Plants must be planted in bedding mix or topsoil not mulch. 2. See planting list for groundcover species, size, and spacing dimension.

PLANTING DETAIL

# PLANTING NOTES:

1. The contractor shall furnish and install all plants shown on the drawings, as specified, and in quantities indicated on the plant list.

2. All plants shall be nursery grown and freshly dug.

- 3. All plants shall be in accordance with The American Standard for Nursery Stock, latest edition. 4. All plants shall be hardy under climatic conditions similar to those in the locality of the project.
- 5. Fall Digging Hazard: Any species listed below, if included on the plant list, must not be dug in the fall (October through December) because of risk to the tree's survival. Special exceptions may be granted if the owner is notified in writing and an extended warranty on these plants is agreed upon prior to digging. The following varieties should not be dug in fall: Betula, Carpinus, Celtis, Cercidiphyllum, Crataegus, Cryptomeria, Fagus, Halesia, Ilex (tree form varieties), Liquidambar, Liriodendron, Nyssa, Ostrya, Prunus, Pyrus, Quercus (except Quercus palustris), Salix weeping varieties, and Tilia tomentosa. Digging for Malus and Zelkova varieties should be avoided in fall only when in leaf.
- 6. All plants shall be typical of their species or variety and shall have a normal habit or growth. They shall be sound, healthy and vigorous, well branched and densely foliated when in leaf. They shall be certified by appropriate State and Federal authorities to be free of disease and insect pests, eggs or larvae. They shall have healthy, well-developed root systems.

7. Plants that do not display typical characteristics for their species shall be rejected.

substitutions may be made upon written request by the contractor, if approved by the owner and/or municipal authority. 9. Measurement: Dimensions of trees and shrubs shall conform to The American Standard for Nursery

8. Substitutions: When plants of a specified kind or size are not available within a reasonable distance,

Stock, latest edition. 10. Size: All plants shall conform to the measurement specified on the plant list, unless authorized in

11. Balled and burlapped plants shall be dug with firm natural balls of earth, of diameter and depth to include most of the fibrous roots. Container grown stock shall have been grown in a container long enough for the root system to have developed sufficiently to hold its soil together firm and whole. No plants shall be loose in the container.

12. Trees with central leader broken or dead shall be rejected

writing by the Township Arborist and the owner.

- 13. Plants with broken root balls or excessive damage to the crown shall be replaced prior to planting. 14. Plants with poor quality root balls or root balls that have been cracked or damaged shall be rejected. 15. Root balls of all plants shall be adequately protected at all times from sun and drying winds or frost.
- 16. Conditions detrimental to plants: The contractor shall notify the project representative in writing of all soil or drainage conditions which the contractor considers detrimental to the growth of plants. The contractor shall state the conditions and submit a proposal for correcting the conditions, including any change in cost, for review and acceptance by the project representative.
- 17. The owner or the owner's representative shall be notified prior to beginning planting operations. 18. All planting shall be at the locations indicated on the drawings. The contractor shall be responsible for planting at the correct grades, alignment, and to the indicated layout of the planting beds.
- 19. Layout of planting: The contractor shall lay out with identifiable stakes the location of all planting beds as indicated on drawing. The layout of planting shall be approved by the project representative prior to any excavation of plant pits or plant beds.
- 20. Minor adjustments to plant locations may be necessary due to field conditions and final grading. The contractor shall notify the owner if major adjustments are required.
- 21. Install plant materials after final grades are established and prior to to planting of lawns unless otherwise acceptable to the owner.

22. Do not install plants when ambient temperatures may drop below 35°F or above 90°F. 23. Do not install plants when wind velocity exceeds 30 mph.

- 24. Planting shall be done within the following dates trees, shrubs, groundcover, and perennials shall be planted September 1 through December 1, or April 15 to May 31, and only when local climatic and soil conditions favor satisfactory planting operations. Planting may be done beyond these limits only if requested in writing and approved by the Landscape Architect.
- 25. Planting operations to commence only when preceding work within the area has been completed. Proceed with and complete planting as rapidly as portions of the site become available, working within seasonal limitations.
- 26. Planting soil shall be excavated native soil from the planting pit. Planting soil shall be thoroughly mixed, with all rocks, clods and roots removed.
- 27. All trees shall be staked and guyed according to accepted industry practice, and as noted on the planting details.
- 28. Each tree and shrub shall be pruned in accordance with The American Nursery and Landscape Association Standards to preserve the natural character of the plant. All dead wood or suckers and
- all broken or badly bruised branches shall be removed. 29. Mulch: Immediately after planting operations are completed, all trees and shrub planting pits shall be covered with a 3" (three inch) layer of double shredded hardwood bark mulch, or other material approved by the owner or the owner's representative. A granular pre-emergent weed control shall be spread prior to mulching. The limit of this mulch for deciduous trees and single evergreen trees shall be the area of the pit. For shrub and perennial beds and for evergreen tree clusters, a continuous, mulched bed shall be created.

30. Trees in leaf, including evergreens, when planted shall be treated with anti-desiccant such as Wilt-Pruf® if planted during the months of June through September, November, and December.

- 31. Weed control: All planting areas shall be free from weeds prior to the beginning of planting operation. Contact herbicide sprays should only be used as required and all manufacturer's specifications followed.
- 32. Guarantee: All plant material shall be guaranteed by the contractor for twelve (12) months from the date of installation. The owner shall be responsible for maintenance unless otherwise agreed with contractor. It shall be the contractor's responsibility to monitor the project during the guarantee and notify the owner if problems develop with the plant material. Any material that is 25 % dead or more shall be considered dead and must be replaced at no charge. A tree shall be considered dead when the main leader has died back, or there is 25% of the crown dead.
- 33. All debris resulting from landscape contracting operations shall be cleaned up and removed from the site on a weekly basis.
- 34. Watering: Landscape contractor is responsible for watering sufficiently at the time of planting and until the job is completed, accepted and turned over to the owner.

Serial Number:

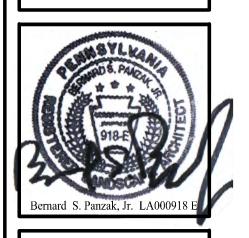
**CALL BEFORE YOU DIG!** PENNSYLVANIA LAW REQUIRES 3 WORKING DAYS NOTICE FOR CONSTRUCTION PHASE AND 10 WORKING DAYS IN DESIGN STAGE-STOP CALL Pennsylvania One Call System, Inc. 1-800-242-1776

**GLACKIN THOMAS PANZAK** 

**LAND PLANNING** LANDSCAPE **ARCHITECTURE** 

Glackin Thomas Panzak, Inc

Paoli Executive Green 1 41 Leopard Road Paoli, Pennsylvania 19301 610.408.9011 Fax: 610.408.9477 E-mail: plans@glackinplan.com



ORREST TY, PENNS **do** TRAFFORD ASHIP, DELAW am

PROJECT #: 19-033 DATE: 08/27/2020 (jr) REV.:

SHEET:

# GENERAL NOTES:

- 1. Base information including topography, grading, utilities, building and existing vegetation location prepared by Site Engineering Concepts, Inc. November 2019
- 2. This plan is for lighting purposes only.
- 3. The Landscape Architect's seal applies only to the lighting portion of this plan.

# LIGHTING NOTES:

- 1. Lighting to be installed to meet all appropriate national and local codes.
- 2. All light fixtures to be approved by Owner or Owner's representative. 3. All circuits to be installed by a licensed electrician.
- 4. All cable to be run in conduit unless approved in writing by the owner.
- 5. Contractor to provide sufficient nighttime adjustment to all lighting to satisfy Township requirements and Owner or Owner's representative. Fixture on/off control by circuit. 6. All lighting to be installed according to manufactures recommendations.
- 7. All electrical conduits shall be sch 40 pvc and all bends shall be "sweep" type for pulling wire. Conduit to extend beyond edge of hardscape element by 12" min. Conduit to be temporarily capped and location staked prior to backfilling.
- 8. While extending conduits under existing structures the contractor shall be responsible for damage to existing structures such as, but not limited to, paving, irrigation, pool piping, masonry piers and underground drainage pipe etc.
- 9. All lighting fixtures to be controlled by photo-cells.
- 10. All final light locations to be approved by Owner or Owner's representative. 11. All final light locations to be field adjusted, "aimed" as necessary so as to avoid shining of light into windows and doors.
- Contractor to arrange a pre-construction meeting with the owner and owners representatives including masons, and other individuals familiar with any underground facilities. As-built drawing should be requested of all such facilities.
- 12. Contractor shall spray paint trench layout prior to commencement. Owner or Owner's representative shall approve layout.
- 13. Electrical contractor to prepare a plan of all as-built underground electric runs. 14. Project electrical engineer to provide power plan, and ComCheck calculations as required.
- 15. In the event that the contractor proposes light fixture substitutions, requests must be received by the Landscape Architect for review (14) fourteen days prior to bid date. Failure to submit within that deadline renders all substitution requests void, and originally specified fixture(s) will be supplied. The contractor shall submit the following:
- B. Contractor's written certification that the proposed substitution(s) conforms to all requirements of the contract documents in every respect and is appropriate for the
- applications indicated in the documents.
- C. Contractor's written statement indicating the effect of the substitution(s) on the construction schedule compared to the schedule with the originally specified fixture(s).
- D. Contractor-net unit price for the originally specified fixture(s) and for the proposed substitute fixture(s).
- E. One sample of the proposed substitution fixture(s) with specified lamps and cord and plug connection for 277 volt operation.

A. Township required copies of all mandatory submittals for both the originally specified fixture(s) and the proposed substitution(s).

- F. Contractor's written certification that any alterations that may result from the proposed lighting fixture substitution(s) will be designed and constructed at the contractor's
- G. Reimbursement to the landscape Architect for all time associated with review of fixture substitutions(s). Payment shall be made in advance of the review, based on the Landscape Architect's hourly rates for the personal involved in the review.
- H. Contractor's written waiver of rights to additional payment and/or time that may become necessary should the proposed substitution(s) fail to perform in a manner that is equivalent to the originally specified fixture(s).
- 16. Post-approval alterations to lighting plans or intended substitutions for approved lighting equipment shall be submitted to the Township for review and approval.

# LIGHTING LEGEND:

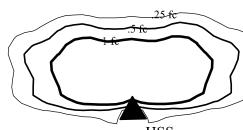
_	SYM.	LIGHT TYPE	QTY.	MFG.	MODEL#	REMARKS
		STREET LIGHT	9	AAL		AAL Providence Medium, LED, <u>Type III Optics</u> , 3K color temperature, photocell control by circuit, 14' height, mounted on 4" AAL POLE MODEL #DB6, Fixture and Pole Finish: Black



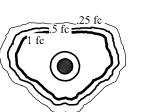
AAL Providence Bollard, LED, Type III Optics, 3K color temperature, photocell control by circuit, Round Shaft 5" Diameter, Finish: Black

# ISO FOOTCANDLE DIAGRAM:

SITE LIGHT:



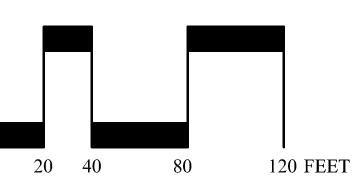
BOLLARD LIGHT:

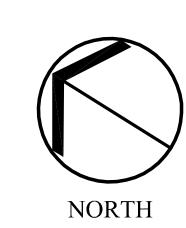


Typical footcandle distribution values are indicated in diagram. The same values, 0.25 fc, 0.50 fc

and 1.00 fc, are indicated on all proposed light fixtures.

Serial Number: **CALL BEFORE YOU DIG!** PENNSYLVANIA LAW REQUIRES 3 WORKING DAYS NOTICE FOR CONSTRUCTION PHASE AND 10 WORKING DAYS IN DESIGN STAGE-STOP CALL 20 Pennsylvania One Call System, Inc. 1-800-242-1776 SCALE: 1'' = 40'





GLACKIN **THOMAS PANZAK** LAND PLANNING

LANDSCAPE **ARCHITECTURE** 

Glackin Thomas Panzak, Inc.

Paoli Executive Green 1 41 Leopard Road Paoli, Pennsylvania 19301 610.408.9011 Fax: 610.408.9477 E-mail: plans@glackinplan.com



DATE: 08/27/2020 (jr)

POLE PER MANUFACTURER'S
SPECIFICATION

18" TO 24" DIA. CONCRETE BASE,
4500 PSI

BOLT CIRCLE PER
MANUFACTURER'S SPECIFICATION

SETBACK TO TERMINATE ALL GROUND VEHICULAR WIRES ON GROUND LUG. PAVING GROUT VOID SPACE SETBACK TO PEDESTRIAN (NOTE #8 BELOW) - ½" CHAMFER ALL AROUND 4,500 PSI CONCRETE FOOTING, FINAL DIMENSIONS OF SURFACE FOOTING AS PER STRUCTURAL ENGINEER. - FINISHED GRADE ANCHOR BOLTS AS PER MANUFACTURER'S SPECIFICATIONS REINFORCEMENT AS PER STRUCTURAL ENGINEER CONDUIT AND BRANCHING WIRE AS PER UTILITY PLAN. (BY OTHERS) I" SCH 40 PYC CONDUIT. STUB OUT APPROX. 6" AND PROVIDE SUITABLE - 34" X 8' COPPERWELD GROUND ROD. EXTEND #6 CU GROUND WIRE UP TO LUG ON POLE BASE. 18"-24" DIA.

NOTES:

I. THIS DETAIL IS FOR BID AND BUDGETARY PURPOSES ONLY.

2. A SONOTUBE FOOTING SHALL EXTEND 30" ABOVE FINISHED GRADE WHERE LIGHT POLES ARE TO BE DIRECTLY ADJACENT TO A PARKING SPACE.

3. CONTRACTOR SHALL ENSURE DESIGN IS PREPARED BY A QUALIFIED STRUCTURAL ENGINEER CONSIDERING LIGHTING MANUFACTURER REQUIREMENTS, LOCAL WIND LOADS AND SITE SPECIFIC SOIL PARAMETERS.

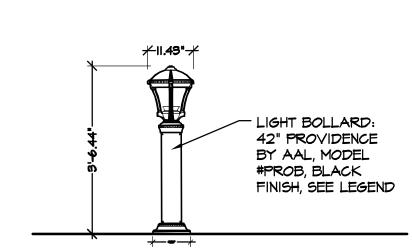
A. THE STRUCTURAL ENGINEER SHALL BE NOTIFIED OF THE INTENT TO MOUNT ANYTHING ASIDE FROM THE

- LIGHT FIXTURE INCLUDING (BUT NOT LIMITED TO) CAMERAS, BANNERS, FLAGS, SIGNAGE, ETC. AS IT WILL IMPACT THE POLE AND FOUNDATION DESIGN.

  B. SOME SITE LOCATIONS AND/OR CONDITIONS MAY REQUIRE VIBRATION DAMPENING MEASURES AS DETERMINED BY A STRUCTURAL ENGINEER.

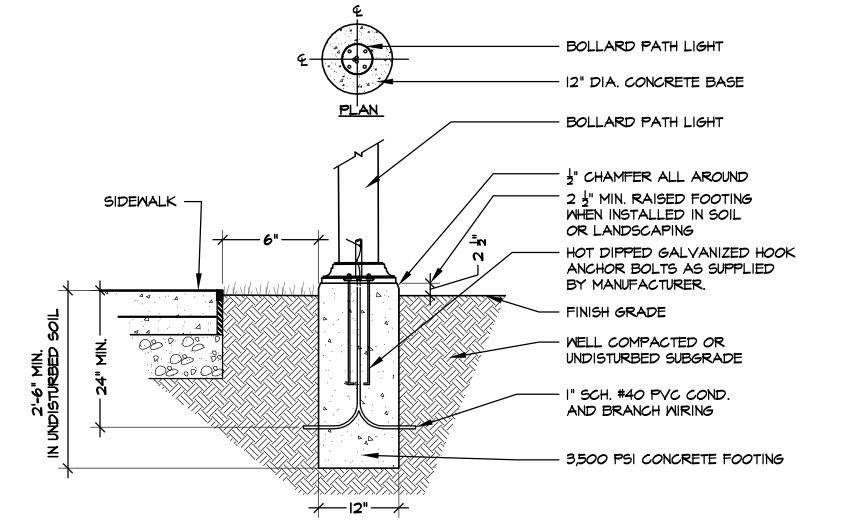
  4. ACTUAL DIAMETER AND DEPTH OF FOOTING SHALL BE DETERMINED BY THE STRUCTURAL ENGINEER.
- 5. ALL REBAR SHALL BE MAINTAINED AT A 3" MIN. CLEARANCE FROM SURFACES.
  6. FIXTURE MUST BE GROUNDED IN ACCORDANCE WITH LOCAL CODES OR THE NATIONAL ELECTRICAL CODE.
  FAILURE TO DO SO MAY RESULT IN SERIOUS PERSONAL INJURY.
  7. POLES SHOULD NEVER BE ERECTED WITHOUT THE LUMINAIRE INSTALLED. WARRANTY IS VOIDED IF THE POLE
- 1. POLES SHOULD NEVER BE ERECTED WITHOUT THE LUMINAIRE INSTALLED. WARRANTY IS VOIDED IF THE POLE IS ERECTED WITHOUT THE LUMINAIRE.

  8. THE WARRANTY IS VOIDED IF THE POLE IS NOT GROUTED UNDER THE ENTIRE BASE AFTER INSTALLATION.



BOLLARD LIGHT DETAIL

Scale: 1/2"=1'-0"



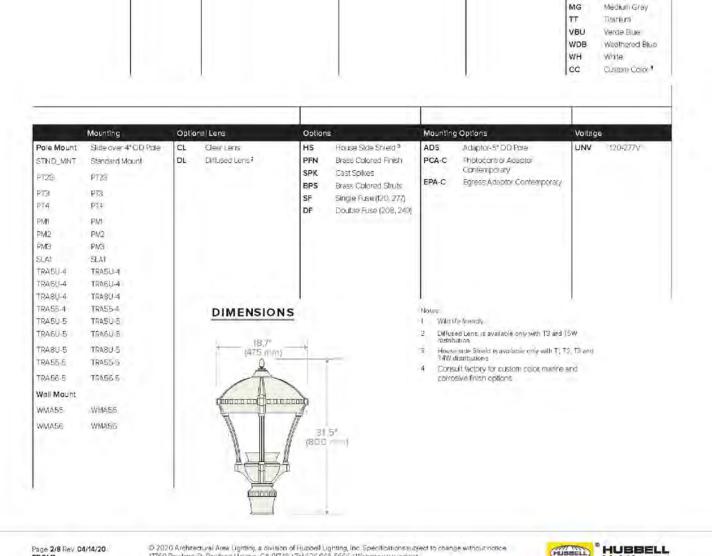
BOLLARD FOOTING DETAIL

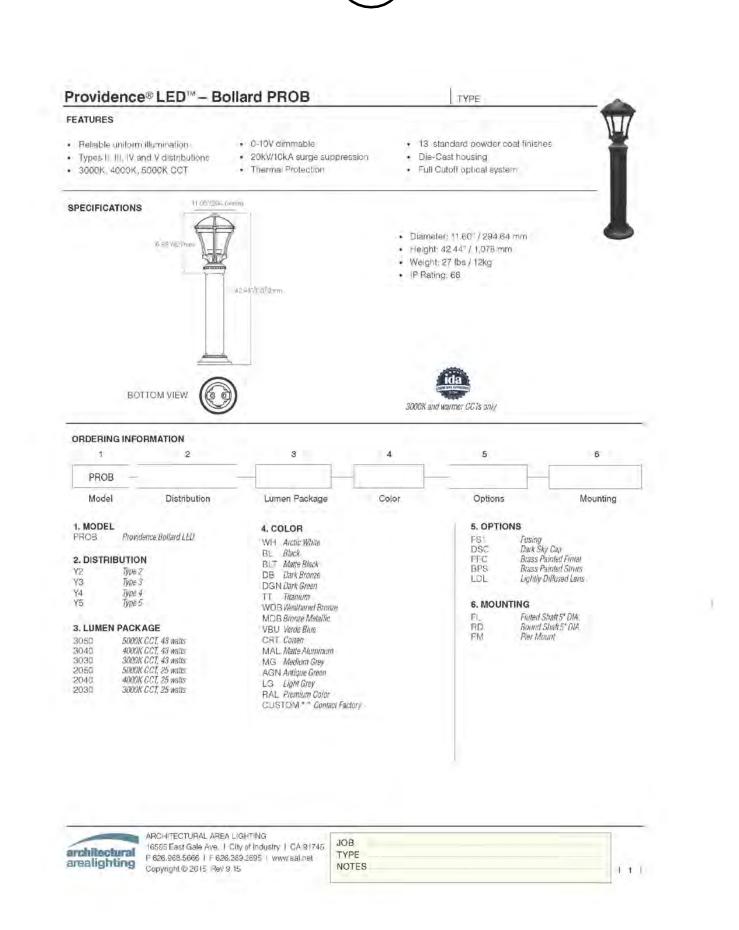
Scale: 1"-1' 0"

1 STREET LIGHT DETAIL
Scale: 1/2"=1'-0"

2 TYPICAL LIGHT POLE FOOTING DETAIL
Scale: 1/2"=1'-0"

architectural arealighting TYPE PROJECT: PROV2 CATALOG# ARCHITECTURAL AREA/SITE ORDERING GUIDE HOUSING 325 325m3, 4500 Lumens 4K7 4000K, 75 CRI 510 510m3, 6500 Lumens 3K7 3000K, 75 CRI 525 760m3-Microcore Crossover AMB Amber-595nm Peak 1 Type II Type III Matte Black 4W Type IV Wide Corton 5Q Type V Square 5W Type V Wide 615 615mA\_7500 Lumers Dark Bronze Graphte Light Gray MAL Matte Aluminum MDB Metallic Bronze MG Medium Gray Titarium Verde Blue WDB Weathered Blue
WH White Custom Color \*





6 BOLLARD LIGHT CUT SHEET

Serial Number:

CALL BEFORE YOU DIG!

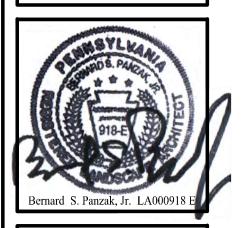
PENNSYLVANIA LAW REQUIRES
3 WORKING DAYS NOTICE FOR
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1-800-242-1776

GLACKIN THOMAS PANZAK

LAND PLANNING
LANDSCAPE
ARCHITECTURE

Glackin Thomas Panzak, Inc.

Paoli Executive Green 1 Suite 300 41 Leopard Road Paoli, Pennsylvania 19301 610.408.9011 Fax: 610.408.9477 E-mail: plans@glackinplan.com



PLAN
Property

LIGHTING PL,
Hamilton Pro
208 & 228 STRAFFORD AVE. & 1
RADNOR TOWNSHIP, DELAWARE COU

PROJECT #: 19-033 DATE: 08/27/2020 (jr) REV.:

SHEET:

LI-2

# DENSITY MODIFICATION REDEVELOPMENT OF 208 AND 228 STRAFFORD AVENUE AND 18 FORREST LANE DEVELOPMENT IMPACT STATEMENT

## FOR SUBMISSION TO: RADNOR TOWNSHIP, DELAWARE COUNTY, PA

#### PREPARED FOR:

Trustees of the Dorrance Hamilton 3/15/1996 Revocable Agreement of Trust c/o Mr. D. Charles Houder 551 W. Lancaster Avenue, Suite 307, Haverford, PA 19041

#### **COMPILED BY:**

George W. Broseman, Esquire Daniel P. Rowley, Esquire Kaplin Stewart Meloff Reiter & Stein 910 Harvest Drive P.O. Box 3037 Blue Bell, PA 19422

#### I. <u>INTRODUCTION</u>

This Development Impact Statement is being submitted in connection with the Conditional Use application for the redevelopment of the properties now known as 208 and 228 Strafford Avenue and 18 Forrest Lane (collectively "**Tract**"). The Tract includes Delaware County Folio Numbers: 36-01-00538-00; 36-01-00539-00; 36-01-00540-00; and 36-01-00236-02.

The Applicant has filed with the Township a conditional use application under Code § 280-29.B(1) for Density Modification Development to permit the construction of forty-one townhomes and related improvements (collectively "**Project**"). The Applicant has also filed with the Township a preliminary land development application for the Project.

The Tract and the Project are shown on the plan set entitled "208 & 228 Strafford Avenue, 18 Forrest Lane Conditional Use and Preliminary Land Development Plans," containing 15 sheets dated August 27, 2020 (sheets 1-11 prepared by SITE Engineering Concepts, LLC, sheets 12-15 prepared by Glackin Thomas Panzak) (collectively referred to as "Plans").

This Development Impact Statement addresses the applicable Development Impact Statement requirements for a conditional use application under Radnor Township Code §280-135.G as follows (responses to Code requirements in italics):

#### II. <u>CODE §280-135.G.(1) & (2) -DEVELOPMENT IMPACT STATEMENT</u>

#### Code §280-135.G.(1)

#### Comprehensive Plan

(A) An analysis of the consistency of the proposed use with the Radnor Township Comprehensive Plan, as amended. The analysis shall include, but not be limited to, the compatibility with environmental and natural resources; housing, demographics, and socioeconomics; business and economic development; transportation and circulation plan; open space and recreation; historical and archaeological resources; community services and facilities; and the land use plan sections of the Comprehensive Plan.

Under Pennsylvania law a township's Comprehensive Plan is not regulatory. "A comprehensive plan is not forever binding, nor does it actually regulate land use." <u>Michaels Dev. Co. v. Benzinger Twp. Bd. of Sup'rs</u>, 413 A.2d 743, 747 (1980). "A comprehensive plan is, by nature, abstract and recommendatory and does not forever control a zoning ordinance enacted to give it legal effect." <u>Appeal of Molnar</u>, 441 A.2d 487, 489 (1982). "The comprehensive plan is a general guideline to the legislative body of the municipality for its consideration of the municipality's program of land utilization and the needs and desires of the community." <u>Forks Twp. Bd. of Sup'rs v. George Calantoni & Sons, Inc.</u>, 297 A.2d 164, 166–67 (1972). Additionally, when a particular use is permitted by conditional use, there is a legislative acceptance that the use is consistent with the zoning plan and is thus consistent with the spirit, purposes, and intent of a Township's Comprehensive Plan. <u>In re Cutler Grp., Inc.</u>, 880 A.2d 39, 45 (Pa. Commw. Ct. 2005)

The Project is permitted by conditional use under Code § 280-29.B(1) as a Density Modification Development and therefore there exists a legislative determination that Density Modification Developments, such as this Project, are consistent with the spirit, purpose and intent of the Township's Comprehensive Plan.

The following is an analysis of the Project under the Radnor Township Comprehensive Plan for the sections enumerated in Code  $\S280-135.G(1)(a)$ :

#### i. Environmental and Natural Resources:

The Properties do not contain any water resources, wetlands, floodplains or riparian areas. The Comprehensive Plan advises to direct development to areas of minimum environmental sensitivity, such as the Tract. (Radnor Comprehensive Plan pgs ii, 2-1). Furthermore, the Tract currently has no stormwater management controls. The Project would provide stormwater management controls in furtherance of the Township's desire to promote the management of water resources. (Radnor Comprehensive Plan pgs ii, 2-1).

#### ii. Housing, Demographics, and Socioeconomics:

The Comprehensive Plan includes the Township's goals to "allow for increased housing density immediately surrounding commercial areas and particularly near transit centers" and to "promote conservation development strategies for new development on large parcels." (Radnor Comprehensive Plan pgs vi, 3-1). The proposed Project is located in close proximity to the commercial corridor along Lancaster Avenue and the Strafford Station along the SEPTA Paoli/Thorndale Regional Rail Line and promotes conservation development strategies by clustering development, providing common open space and providing for stormwater management.

#### iii. Business and Economic Development:

The zoning of the Tract does not allow for commercial use. The Project is located in close proximity to the commercial corridor along Lancaster Avenue and will serve as a transitional development from the commercial area along Lancaster Avenue and along Eagle Road to the residential areas northward from Lancaster Avenue. The Project will provide an expanded customer base for these commercial uses along Lancaster Avenue, such as the Lancaster Farmer's market in the Strafford Shopping Center and the various shops and restaurants in the adjoining Eagle Village Shops. The Tract is within walking distance of these businesses and will allow visits from the Project without adding traffic.

#### iv. Transportation and Circulation Plan:

The Project's close proximity to the Strafford Station SEPTA Regional Rail Station will encourage new residents to use this public transit station, which furthers the goals of the Comprehensive Plan. (Radnor Comprehensive Plan pgs x, 5-2). For additional information regarding traffic related to the Project, a Traffic Impact Study prepared by Frank Tavani, P.E., PTOE was submitted with the Conditional Use application. The Project will also improve pedestrian circulation by constructing sidewalks on the exterior and interior of the Project tract. (Radnor Comprehensive Plan pgs x, 5-2).

v. Open Space and Recreation:

The Project proposes common open space in a centralized green area and along the edges of the Tract for the use of the residents. These areas will be restricted from further development through a Planned Community Declaration.

vi. Historical and Archaeological Resources:

The Property contains no historical or archaeological resources.

vii. Community Services and Facilities:

An analysis of the Project's impact on community services and facilities can be found in the Fiscal Impact Study prepared by Erik Hetzel, AICP/PP, LEED AP that was submitted with the Conditional Use application and is attached to this report as **Appendix A**.

viii. Land Use Plan:

One of the stated goals of the Comprehensive Plan is to "accommodate reasonable growth using innovative growth management techniques such as transit oriented development, traditional neighborhood design, and other flexible design techniques that harmonize with and enhance the existing communities." (Radnor Comprehensive Plan pgs xxv, 10-1, 10-2). The Project utilizes the flexible design technique of density modification development as permitted in the R-4 District in Article XIX of the Zoning Ordinance. The use of this provision allows for a layout that includes common open space that would not be required under a conventional development under the R-4 District provisions.

Furthermore, the Project is located in close proximity to the commercial corridor along Lancaster Avenue and the Strafford Rail Station and will provide an expanded customer base for the commercial uses.

#### Natural Features

(B) The impact of the proposed use on floodplains, waterways, heavily wooded areas, steep slopes, and other sensitive natural features located upon and adjacent to the site, if any.

There are no floodplains, waterways, heavily wooded areas, or steep slopes on the site. The Project is located in a fully developed area of Radnor Township, surrounded by other residential and commercial development.

#### **Transportation**

(C) The proposed use's impact on the Township and regional transportation system(s) and the ability of adjacent streets and intersections to efficiently and safely handle the traffic generated by the proposed development. This analysis shall include all modes of transportation and shall be based on current Pennsylvania Department of Transportation (PennDOT) requirements within their Policies and Procedures for Transportation Impact Studies (PennDOT Strike-off Letter 470-

09-4, dated 2009, as amended). The applicant shall address any measures proposed to be implemented in order to mitigate any adverse impacts.

A Traffic Impact Study prepared by Frank Tavani, P.E., PTOE was submitted with the Conditional Use application and is attached to this report as **Appendix B**. This Traffic Impact Study addresses the Project's impact on the Township's transportation systems including the surrounding streets and intersections. Additionally, the Project's close proximity to the Strafford Rail Station and commercial uses within walkable distance will reduce traffic and will encourage new residents to use this public transit station.

It is also noted that traffic concerns are not a basis for a denial of a conditional use application. Even evidence of a significant increase in traffic does not warrant denial of a conditional use application unless it is shown that the potential for a substantial increase would "by a high degree of probability, pose a substantial threat to the health and safety of the community" that is not typically expected from a similar development. In re Brickstone Realty Corp., 789 A.2d 333, 341 (Pa. Commw. Ct. 2001).

#### School District

**(D)** The proposed use's impact on the Radnor School District, including an estimate of new pupils generated by the proposed development.

A Fiscal Impact Study prepared by Erik Hetzel, AICP/PP, LEED AP was submitted with the Conditional Use application and is attached to this report as **Appendix A**. This Fiscal Impact Study addresses the proposed use's impact on the Radnor School District. By way of further response, it is anticipated that an estimated 9 school-aged children will live in the proposed development, although all of them may not utilize the Radnor School District's public schools.

Because the conditional use is contemplated and permitted by the ordinance, there is a presumption that the governing body considered the effect of the use when enacting the ordinance and determined that the use is consistent with the health, safety, and welfare of the community so long as it meets the objective requirements of the ordinance. Marr Dev. Mifflinville, LLC v. Mifflin Twp. Zoning Hearing Bd., 166 A.3d 479, 483 (Pa. Commw. Ct. 2017). Whether or not a proposed development adds school children to the local school district is not a factor that can be considered in the review of a conditional use application or a land development application.

#### **Commercial Impact**

(E) The proposed use's impact on nearby commercial facilities within the Township and surrounding municipalities.

A Fiscal Impact Study prepared by Erik Hetzel, AICP/PP, LEED AP was submitted with the Conditional Use application and is attached to this report as **Appendix A**. This Fiscal Impact Study addresses the proposed use's economic impacts. By way of further response, the new residents of the proposed development will use goods and services in and around Radnor Township which will have a positive impact on nearby commercial facilities.

#### **Public Utilities**

**(F)** The proposed use's impact on public utilities, including but not limited to sewage disposal, water supply, storm drainage, and electrical utilities.

It should be noted that the Tract and the immediate area is currently served by public utilities and the proposed density is permitted under Article XIX of the Zoning Ordinance, therefore these impacts are already considered under the Zoning Ordinance. No adverse impacts on public utilities are anticipated.

#### **Police and Fire Protection**

(G) The proposed use's impact upon the provision of police and fire protection.

A Fiscal Impact Study prepared by Erik Hetzel, AICP/PP, LEED AP was submitted with the Conditional Use application and is attached to this report as **Appendix A**. This Fiscal Impact Study addresses the proposed use's impact on public safety services. It should be noted that the Tract is currently served by the police and fire department and the proposed density is permitted under Article XIX of the Zoning Ordinance, therefore these impacts are already considered under the Zoning Ordinance. The eventual homeowners of the Project will also pay taxes to Radnor Township which will increase Township revenues.

#### **Open Space and Recreation Facilities**

(H) The proposed use's impact on the Township's open space and recreation facilities.

The Conditional Use Plans propose to designate at least 15% of the Tact as common open space. This proposed common open space will be preserved through the recordation of the approved plan and through a Planned Community Declaration. The proposed density is permitted under Article XIX of the Zoning Ordinance, therefore these impacts are already considered under the Zoning Ordinance. The Radnor Township Subdivision and Land Development Ordinance also requires recreational land dedication or that the Applicant pay a fee-in-lieu to address impacts on the Township's open space and recreation facilities.

#### Neighborhood Impact

(I) The proposed use's impact upon the character of the surrounding neighborhood. The applicant must show the proposed development will not adversely affect the surrounding neighborhood and what measures are proposed to mitigate any potential impacts.

The site is located in the R-4 Residence District and is in close proximity to one of the Township's primary commercial corridors along Lancaster Avenue. The Tract serves as a transitional area from the commercial areas along Lancaster Avenue to the residential areas to the north and northeast of the site.

The Project as a residential use is consistent with the residential uses in the area. Significant landscaping is proposed along the street frontages will help maintain the visual character along the public ways. The Project also provides a buffer along the residential property lines.

#### Fiscal Impacts

(J) An analysis of the proposed use's fiscal impacts upon the county, Township, and school district.

A Fiscal Impact Study prepared by Erik Hetzel, AICP/PP, LEED AP was submitted with the Conditional Use application and is attached to this report as **Appendix** A. This Fiscal Impact Study addresses the proposed use's economic impacts. It should be noted that the proposed density is permitted under Article XIX of the Zoning Ordinance, therefore these impacts are already considered under the Zoning Ordinance.

# III. <u>Required Documentation under Code §280-135.G for a Development Impact Statement (as applicable)</u>

(A) A conditional use plan for the proposed development, identifying all proposed uses to be located on the site, and demonstrating compliance with the area, bulk and dimensional requirements for the proposed use. The conditional use plan shall be submitted in the form of a sketch plan containing the information required by § 255-19 of the Township Subdivision and Land Development Ordinance.

Conditional Use Plans for the proposed development were submitted with the Conditional Use application. The Conditional Use Plans for the proposed development exceeds the requirements of a sketch plan under Code §255-19.

(B) The applicant shall provide conceptual architectural renderings (perspectives and elevations) of the proposed development.

Conceptual architectural renderings, as they are envisioned at this time, were submitted with the Conditional Use application. It is noted that there are no objective standards in the Code for architecture, except as in Code §280-93. The Project complies with these standards.

(C) The location and size of the site, with evidence supporting the general adequacy for development.

Information regarding the size, location, and adequacy for development can be found in the Plans for the Project.

**(D)** The proposed residential density of the development and the percentage mix of the various dwelling types.

The proposed residential density is 5.5 dwelling units per acre and all of the proposed dwellings are townhouses. The proposed use and density are permitted in the R-4 District and are detailed on the Plans.

(E) The location, size, accessibility and proposed use of the open space, manner of ownership and maintenance, and a copy of the covenant to be incorporated in the individual deeds, if applicable.

The Conditional Use Plans propose to designate at least 15% of the Tract as common open space for use of the residents of the development. The ownership and maintenance of the open space will be through the homeowners association to be formed.

**(F)** Conceptual landscape plans showing locations of trees and shrubs and other landscape improvements (e.g., berms, fences) as necessary to mitigate the adverse visual impacts which the proposed actions will have on the property, adjoining properties and the Township in general. This shall include improvements to the streetscape adjacent to the property boundaries.

Conceptual landscape plans are included in the Conditional Use Plans, specifically on Sheets 12 and 13.

(G) Conceptual plans of proposed utility and drainage systems.

Conceptual plans of the proposed utility and drainage systems are included in the Conditional Use Plans, which were submitted with the Conditional Use application. The Post Construction Stormwater Details and Construction Details can be found on Sheets 7 and 8 of the Conditional Use Plans.

(H) A phasing plan describing how the proposed development will be implemented (if applicable).

The development is proposed to be constructed in one phase.

(I) Plans and renderings indicating the design, unity and aesthetic relationship of building and landscaping within the proposed development with that of the surrounding area.

The Plans show this information.

(i) The text of covenants, easements and existing restrictions or those to be imposed upon the land or structure, including provisions for public utilities, and trails for such activities as hiking or bicycling, if applicable.

A draft Planned Community Declaration was submitted with the Conditional Use application. This Declaration will be finalized at a later stage of the process.

# APPENDIX A FISCAL IMPACT STUDY

ERIK W. HETZEL, AICP/PP, LEED AP 16 MANOR ROAD PAOLI, PA 19301 610.322.7154 erik@erikhetzel.com

# Memorandum

To: D. Charles Houder, Haverford Properties Inc.

From: Erik Hetzel, AICP/PP, LEED AP

Date: September 17, 2020

Re: Fiscal Impact Analysis – Townhouse Residential Development, Radnor Township

On behalf of Trust U/A Dorrance H. Hamilton dated 3/15/96, Haverford Properties Inc. is proposing a residential development on the site of the Hamilton estate in Radnor Township, Delaware County. Forty-one (41) single-family attached townhome units are planned at the northwest corner of the intersection of Strafford Avenue and Eagle Road. This memorandum describes the anticipated future annual fiscal impacts related to the proposed development. A concise summary of fiscal impacts associated with the proposed development is presented in a table on the last page of this memorandum.

Fiscal impacts presented in this analysis were estimated using a methodology developed by the Rutgers University Center for Urban Policy Research, as originally described in <a href="The New Practitioner's Guild to Fiscal Analysis">The New Practitioner's Guild to Fiscal Analysis</a> and further developed in a later publication by the same authors entitled <a href="Development Impact Assessment Handbook">Development Impact Assessment Handbook</a> analysis</a>. It is projected that, in total, the proposed development will result in beneficial, net-positive annual fiscal impacts to both Radnor Township and the Radnor Township School District.

#### **Revenue Impacts**

Real Estate Property Tax - At full build-out under the proposed concept, the development will have a total market value of approximately \$34.85 million, which translates to an assessed value of approximately \$17.425 million. This assessment

<sup>&</sup>lt;sup>1</sup> Burchell, Robert W., David Listokin, et al. *The New Practitioner's Guide to Fiscal Impact Analysis,* New Brunswick, NJ: Rutgers, The State University of New Jersey, 1985.

<sup>&</sup>lt;sup>2</sup> Burchell, Robert W., David Listokin, et al. *Development Impact Assessment Handbook*. Washington, D.C.: ULI-the Urban Land Institute, 1994.

calculation is based on the current (2020-2021) Delaware County common-level ratio of 2.00, which estimates assessed value at approximately 56.00% of market value. The proposed development will generate ongoing real estate tax revenue to the local taxing authorities, with an estimated \$76,813 going to the Township and \$445,486 going to the School District annually.

Real Estate Transfer Tax – The Township levies the Real Estate Transfer Tax at the rate of 1.0% of sale price, and the School District levies the Real Estate Transfer Tax at 0.5%. This analysis anticipates that an average of approximately 5% of the residential units in the proposed development will transfer ownership (be sold) in any given year, which equates to annual Transfer Tax Revenues totaling \$15,421 to the Township and \$7,711 to the School District. In addition to the annual Transfer Tax revenues estimated here, it is also important to note that the Township and School District will also receive significant Transfer Tax revenues from the initial sales of every residential unit from the builder to the first homeowners. With a total market value of \$34,850,000 in 2020, this equates to an estimated \$348,500 in initial-sales transfer taxes to the Township (\$34,850,000 market value x 1.0% transfer tax rate = \$348,500). For the School District, initial home sales would generate transfer taxes totaling \$174,250 (\$34,850,000 market value x 0.5% transfer tax rate = \$174,250).

Other Revenues - In addition to the aforementioned tax revenues, the proposed development is expected to generate approximately \$26,811 in Township revenues from other sources, as reported in the Radnor Township 2020 General Fund Budget. These sources include licenses & permits, fines & costs, and departmental earnings. Similarly, the School District will realize additional revenues totaling approximately \$44,999 in revenues from non-tax sources (LEA activities and rentals) and intergovernmental sources (State and Federal funding) as identified in the Radnor Township School District 2020-2021 Final General Fund Budget.

#### **Population Impacts**

An estimated 91 residents will be living in the proposed development, including 9 school-aged children. These population estimates are based on demographic multipliers published by the Rutgers University Center for Urban Policy Research (2006)<sup>3</sup>. This source uses information from the U.S. Census to derive population

<sup>&</sup>lt;sup>3</sup> Burchell, Robert W., David Listokin, et al. *Residential Demographic Multipliers (Pennsylvania)*. New Brunswick, New Jersey: Center for Urban Policy Research, Edward J. Bloustein School of Planning and Public Policy - Rutgers, the State University of New Jersey (2006).

multipliers specific to the Commonwealth of Pennsylvania, categorized by household structure type (i.e., single-family detached, single-family attached, apartments, etc.), and by the number of bedrooms per household.

#### **Economic Impacts**

The proposed development will have a beneficial economic "ripple effect" in the local economy, as the new residents and employees use goods and services in and around Radnor Township. In addition, the development phase of the project will provide construction jobs and result in construction-related consumption expenditures in the local and regional economies. These impacts are not reflected in the summary table included in this memorandum but will provide economic benefits over and above the annual revenues described therein.

#### **Cost Analysis**

Annual Township and School District expenditures attributable to the proposed development were projected using the Per Capita Multiplier Method described in the <u>Development Impact Assessment Handbook</u>. In calculating the per capita expenditure value for the Township, the methodology uses information from the current (2020) Township budget and accounts for the fact that costs are divided differently among serving both residential and non-residential portions of the Township, based on the actual mix of land uses provided by the County Board of Assessment. Overall, it is estimated that the proposed development will result in Township costs totaling approximately \$84,604 annually, which is more than offset by revenues totaling \$119,045 annually from the tax and non-tax sources described previously.

A similar per capita cost calculation is used for the Radnor Township School District. Based on current (2020-2021) enrollment and budget information provided by the School District, the estimated annual total cost-per-student in the Radnor Township School District used in this analysis is approximately \$24,082. With 9 school-aged children projected to reside in the proposed development, this results in projected annual costs of \$216,741 to the School District, which is more than offset by revenues from taxes and other sources totaling \$498,196. This estimate assumes that all 9 school-aged children will be attending public schools; however, it is likely that some will attend private schools, which would result in lower costs to the Radnor Township School District than estimated here, and a higher net-positive fiscal impact.

#### **Impacts to Public Safety Services and Facilities**

Overall, impacts on public safety services and facilities from the proposed development are expected to be minimal. Demands for Police, Fire, and Emergency Management Services (EMS) personnel, vehicles, and facilities are presented in the following table, using planning standards published in the Development Impact Assessment Handbook. Personnel, vehicles, and facilities for Police and Fire are based on the noted planning standards per 1,000 population. EMS standards for personnel and vehicles are per 30,000 population, and EMS calls are per 1,000 population per year.

Public Safety Demand Factors and Projected Demand from Proposed Development

	Residential Planning Standard	Projected Demand
Police		
Personnel	1.50	0.14
Vehicles	0.45	0.04
Facilities (square feet)	150	14
Fire		
Personnel	1.24	0.11
Vehicles	0.15	0.01
Facilities (square feet)	187.5	17
<u>EMS</u>		Name of the second
Personnel	3.08	0.01
Vehicles	0.75	0.00
Calls per year per 1,000 population	27.4	2

#### **Conclusions and Summary**

In conclusion, the net positive fiscal impact in terms of projected revenues over costs for Radnor Township is expected to be over \$34,000 annually at project completion. The projected net positive fiscal impact to the Radnor Township School District is projected to be over \$280,000 per year. The combined net positive fiscal impact for both taxing authorities is estimated at over \$300,000 annually. Not included in these annual estimates are significant real estate transfer fees that will be paid on the initial sales of every unit from the builder to the first homeowners. The table on the next page summarizes the fiscal impacts to the Township and School District.

#### **Fiscal Impact Summary**

	To	wnhomes
Residential Units		41
Value per Unit		\$850,000
Market Value of Proposed Development	\$3	34,850,000
Assessed Value (approx. 50% of market value)	\$:	17,425,000
New Residential Population		91
New School-Aged Children		9
Radnor Township		
Real Estate Tax Revenue (4.4082 mills)	\$	76,813
Non-Property Tax Revenue	\$	26,811
Real Estate Transfer Tax (0.5%)	\$	15,421
Total Township Revenues	\$	119,045
Total Township Expenditures	\$	(84,604
Net Township Fiscal Impact	\$	34,441
Radnor Township School District		
Real Estate Tax Revenue (25.5659 mills)	\$	445,486
Non-Property Tax Revenue	\$	4,428
Intergovernmental Revenue	\$	40,571
Real Estate Transfer Tax (0.5%)	\$	7,711
Total School District Revenues	\$	498,196
Total School District Expenditures	\$	(216,741)
Net School District Fiscal Impact	\$	281,455
Total Development-Generated Revenues (Township + School District)	\$	617,241
Total Development-Generated Expenditures (Township + School District)	\$	(301,345)
Total Net Annual Fiscal Impact	\$	315,895

# APPENDIX B TRAFFIC IMPACT STUDY



## F. Tavani and Associates, Inc.

#### Traffic Engineering and Planning

248 Beech Hill Road • Wynnewood • PA • 19096 • (215) 625-3821 Phone • (484) 792-9495 Fax www.FTAVANIASSOCIATES.com

25 August 2020

Charlie Houder Haverford Properties 551 W. Lancaster Avenue, SU 307 Haverford, PA 19041

#### VIA EMAIL ONLY

RE: Traffic Engineering Investigations of Strafford Ave 41-unit Residential TH Site Radnor Township, Delaware County, PA

FTA Job #219-011

#### Dear Mr. Houder:

F. Tavani and Associates, Inc. (FTA) has conducted traffic engineering investigations for the above-referenced project in Strafford. This report has been prepared in accordance with Radnor code requirements and follows the recommended outline as identified in said ordinance.

#### GENERAL SITE DESCRIPTION

This study considers the traffic impact of a proposed townhouse community of 41 units. The housing is proposed to be for sale and will feature a mix of 3 and 4 bedrooms. The housing is proposed to be market-rate and not age-restricted. The process of entitlements, construction, and occupancy is expected to take 3-5 years. The site is immediately surrounded by other residential properties and the Eagle Village Shopping Center. Beyond them, there is a mix of office and retail buildings within a 1 mile radius of the site. Ample mass transit opportunities are also within a short distance of the site.

The site is located on the west side of Strafford Avenue, north of Eagle Road and is known as the Hamilton Estate. The site is presently developed with some existing housing, namely 6 total dwellings.

The site location and surrounding area are presented in figures which are attached to the end of this report, namely Figure 1 and Figure 2. A reduced version of recent site plans for the project is featured in Figure 3. There are no other known approved land development projects in the vicinity of the site.

Note that technical appendices are provided following the figures. Appendix A is reserved for future project correspondence. Photodocumentation of the study area / surrounding intersections is provided in Appendix B.

#### TRANSPORTATION FACILITIES DESCRIPTION

The site is surrounded on two sides by existing, two-way, one-lane-per-direction, public roadways, namely Strafford Avenue and Eagle Road. The roadways generally do not feature on-street public parking. Posted speed limit signs are present in the vicinity of the site along both Strafford Avenue and Eagle Road, where the posted speed limit is 25 mph. There are limited sidewalk facilities in the study

Charlie Houder 25 August 2020 Page 2 of 4

area. The major intersections closest to the site are all-way stop-controlled intersections with no painted crosswalks. There are existing SEPTA mass transit opportunities near the site including bus route 106 and a regional rail station (Strafford), each of which are within approximately one half mile of the site. No traffic signals (save for a flashing beacon at the all-way stop-controlled intersection of Strafford Avenue and Eagle Road) exist or are proposed in the immediate vicinity of the site. More site driveway and surrounding intersection details can be seen in photodocumentation log as provided in **Appendix B**.

The site has 41 units and is proposed to feature internal roadways, 2 site driveways (both on Strafford Avenue), garage/driveway parking, and visitor parking (approximately 11 defined spaces). Sidewalks are also proposed.

There are no known planned roadway improvements in the vicinity of the site. None of the streets surrounding the site are "SR"s (state roadways) – instead they are all local roadways. Eagle Road is a "G" roadway, meaning it is not an SR but is eligible for liquid fuels funding and PennDOT does maintain traffic count data along it, as seen in **Appendix C**.

#### **EXISTING TRAFFIC CONDITIONS**

FTA conducted traffic counts at the intersections of:

- Strafford Avenue and Eagle Road,
- Strafford Avenue and Grant Lane/Hedgerow Lane, and
- Eagle Road and N Wayne Avenue.

The counts were conducted on Thursday, 16 May 2019 from 7:00 AM to 9:00 AM and from 4:00 PM to 6:00 PM. The counts were conducted during the school year, in fair weather, and on a typical weekday. Existing peak hours of 8:00 AM to 9:00 AM and 4:30 PM to 5:30 PM were selected for study based on a system-wide peak hour investigation. The corresponding existing peak hour traffic volumes are plotted and seen in **Figure 4**. Raw traffic volumes are attached in **Appendix D**, as is a spreadsheet which describes the system peak investigation.

With existing peak hour volumes established, present-day "levels of service" can be assessed. Level of service (or LOS) is a descriptive mechanism which is employed by traffic engineers to relate quality of traffic flow to both a letter grade and estimate of delay in seconds per vehicle. LOS results are assessed for traffic which must stop or yield to other traffic. Free-flowing traffic theoretically has no delay, and therefore no LOS rating. Existing levels of service were determined using *Synchro version 10* software, with HCS2010-format outputs selecting for performance reporting purposes. A LOS Comparison Matrix was prepared and is attached to the end of this report. The matrix summarizes AM and PM peak hour performance for existing and future (see next section) conditions for all intersections. As shown, existing levels of service are all LOS A and B, with all calculated delays being very low (10 seconds or less in most cases – an acceptable condition). No congestion locations (LOS E/F) are noted.

#### TRANSPORTATION IMPACT OF THE DEVELOPMENT

Site traffic was estimated using the Institute of Transportation Engineers (ITE) publication, <u>Trip Generation</u>, 10<sup>th</sup> edition. ITE website trip generation outputs are attached and provided in **Appendix E**. Raw trip generation could have been modified to reflect how this site is located in a setting which is within walking distance of several businesses as well as SEPTA bus route 106 plus the Strafford train station, though no such multimodal credits were taken. Instead, all site traffic was assigned (trip distributed) to the surrounding roadway network in accordance with existing traffic patterns as well as an understanding of

Charlie Houder 25 August 2020 Page 3 of 4

existing road network connectivity, current traffic/congestion patterns, and relative locations of major highway interchanges (Interstates 476, 76, 202, and 422 as well as Business Route 30). The assignments are summarized as follows:

- 30% to/from Routes 202 & 422 via Strafford Ave to Old Eagle School Rd;
- 30% to/from Routes 476 & 76 via Eagle Rd to King of Prussia Rd;
- 15% to/from Business Rt 30 West via Eagle Rd and Strafford Ave;
- 15% to/from Business Rt 30 East via Eagle Rd and Strafford Ave, West Ave., and/or Banbury Way; &
- 10% to/from Conestoga Road via Eagle Road.

The trip distribution model for the community is shown in **Figure 5** and the resultant assignment of new, site-generated, vehicular peak hour traffic is shown in **Figure 6**. A site trip generation summary table follows below. Note that a credit for the previously-mentioned 6 existing dwelling units was applied to the trip generation (net new 35 townhomes)

TABLE 1
PROJECTED VEHICULAR TRIP GENERATION

A	AM PEAK HOUR			M PEAK HO	UR
<u>IN</u>	OUT	TOTAL	IN	OUT	TOTAL
4	14	18	14	9	23

Average daily site traffic was also calculated and determined to be approximately 250 trips for the proposed community. See Appendix E for more details.

#### ANALYSIS OF TRANSPORTATION IMPACT

Future traffic conditions are a function of three components: (1) existing traffic volumes, (2) additional traffic due to general background growth as well as other known approved developments in the immediate proximity of the site, and (3) site traffic.

As mentioned earlier, there are no other known approved land development projects in the vicinity of the site. Regarding background growth, the currently promulgated background growth rate for Delaware County is 0.00% per year as reported by PennDOT. This means that future 'no build' traffic volumes and levels of service are identical to existing traffic volumes and levels of service.

The projected future 'build' (no build plus site traffic) peak hour volumes are shown in **Figure 7**. The related projected levels of service are once shown in **LOS Comparison Matrix**. As shown projected 'build' levels of service once again remain essentially the same as they are today, and are all LOS B or better. The impact of site traffic is no added delay at all intersections/turning movements (i.e, the impact of site traffic never amounts to *any* added delay at *any* impacted turning movement), and this again is while taking no credits for multi-modalism. Even with this conservative approach, no congestion locations (LOS E/F) are noted.

No road improvements are necessary to offset the impact of added site traffic. No proposed site driveway will feature traffic volumes which warrant the installation of a traffic signal. The acceptable operation of each site driveway (LOS A and B) in unsignalized state underscores this conclusion. Level of service worksheets are provided in **Appendix F**.

Charlie Houder 25 August 2020 Page 4 of 4

#### **AUXILIARY LANE ANALYSIS**

The need for new auxiliary left- and right-turn lanes at the site driveways was investigated. Investigations were based on PennDOT Strike Off Letter 560-08-4 as well as PennDOT Publication 46 Chapter 11 page 11-46 ("Turn Lane Warrants") using PennDOT-provided worksheets, and focusing on the highest peak hour. Investigations conclude that new auxiliary left- and right-turn lanes are <u>not</u> warranted at the site driveways. More details are provided in **Appendix G**.

#### **CONCLUSIONS**

As mentioned earlier, a LOS Comparison Matrix is provided to afford a simple means to review and assess site traffic impact in the study area. In locations where levels of service are not forecasted to change from one scenario to the next (i.e., from Existing to No Build, or from No Build to Build), hyphens are used. As shown, there are many instances in which the impact of site traffic results in essentially no measurable change in traffic performance and the underlying traffic performance is already acceptable, and with very low delays.

Other key conclusions are as follows:

- The study area is presently well-served by transit opportunities.
- There are no streets or intersections operating below LOS C under existing or future conditions.
- Both site driveways are forecasted to operate at LOS A/B during both peak hours, and for all turning movements.
- No site driveway requires new left-turn or right-turn auxiliary lanes per investigations using standard PennDOT tools.
- The foregoing conclusions were reached taking no credits for walking or transit, even though at least some of either/both are likely.

I hope this has been helpful. Please let me know if I can answer any questions.

Thank you,

2/2

ANIAND ASSOCIATE

Principal

attachments

cc: George Broseman, Esq. Rob Lambert, P.E.

## LEVEL OF SERVICE AND EXPECTED DELAY FOR UNSIGNALIZED INTERSECTIONS\*

LEVEL OF SERVICE	CONTROL DELAY PER VEHICLE (SECONDS)
a	0 to 10.0
b	10.1 to 15.0
С	15.1 to 25.0
d	25.1 to 35.0
e	35.1 to 50.0
f	Over 50.0

<sup>\*</sup> Transportation Research Board's Highway Capacity Manual

## LEVEL OF SERVICE AND EXPECTED DELAY FOR SIGNALIZED INTERSECTIONS\*

LEVEL OF SERVICE	DESCRIPTION	CONTROL DELAY PER VEHICLE (IN SECONDS)
Α	Very short delay, good progression; most vehicles do not stop at intersection.	≤ 10.0
В	Generally good signal progression and/or short cycle length; more vehicles stop at intersection than Level of Service A.	10.1 to 20.0
С	Fair progression and/or longer cycle length; significant number of vehicles stop at intersection.	20.1 to 35.0
D	Congestion becomes noticeable; individual cycle failures; longer delays from unfavorable progression, long cycle length, or high volume/ capacity ratios; most vehicles stop at intersection.	35.1 to 55.0
E	Usually considered <u>limit of acceptable</u> <u>delay</u> indication of poor progression, long cycle length, or high volume/ capacity ratio; frequent individual cycle failures.	55.1 to 80.0
F	Could be considered excessive delay in some areas, frequently an indication of saturation (i.e., arrival flow exceeds capacity), or very long cycle lengths with minimal side street "green" time. Capacity is not necessarily exceeded under this level of service.	> 80.0

<sup>\*</sup> Transportation Research Board's Highway Capacity Manual

#### LEVEL OF SERVICE COMPARISON TABLES

Direction	Movement		AM Peak Hour			PM Peak Hour	
Strafford ,	Ave	Existing (2019)	No Build (2024)	Build (2024)	Existing (2019)	No Build (2024)	Build (2024
Eastbound	LTR	A 8			A 9		144
Westbound	LTR	A 9	-		A 8		A 9
Grant Ln / Hed	lgerow Ln						
Northbound	LTR	A 8			A 7	-	
Southbound	LTR	A 8			A 7		**
	OVERALL:	A 9		94	A 9		-

Control Type: AWSC

		2	2. Strafford Ave	& Eagle Ave			
Direction	Movement		AM Peak Hour			PM Peak Hour	
Strafford	Ave	Existing (2019)	No Build (2024)	Build (2024)	Existing (2019)	No Build (2024)	Build (2024
Eastbound	LTR	A 10			B 11		
Westbound	LTR	A 9			B 10		
Eagle A	ve						
Northbound	LTR	A 9	-		B 11		-
Southbound	LTR	A 10			B 11		ww
	OVERALL:	A 9		A 10	B 11	1	2

Control Type: AWSC

		;	3. N Wayne Ave	& Eagle Ave			
Direction	Movement		AM Peak Hour			PM Peak Hour	
N Wayne	Ave	Existing (2019)	No Build (2024)	Build (2024)	Existing (2019)	No Build (2024)	Build (2024
Eastbound	LTR	A 4			A 5		
Westbound	LTR	A 3	**	**	A 5	(44)	
Eagle A	ve						
Northbound	LTR	B 20			B 20		
Southbound	LTR	B 19			B 20	-	
	OVERALL:	A 8			B 10		

Control Type: Signal

		4. 5	Strafford Ave &	TH Site Drive	N		
Direction	Movement		AM Peak Hour			PM Peak Hour	
TH Site D	rive	Existing (2019)	No Build (2024)	Build (2024)	Existing (2019)	No Build (2024)	Build (2024)
Eastbound	LR		171	A 9			B 10
Eagle A	ve	74.10	19				
Northbound	L	THE .		A 9	A- PA		A 9
	OVERALL:		diam'r.	A 1		A Land Land	A 1

Control Type: TWSC

		5. St	rafford Ave & S	FDU Site Driv	e S		
Direction	Movement		AM Peak Hour			PM Peak Hour	
SFDU Site	Drive	Existing (2019)	No Build (2024)	Build (2024)	Existing (2019)	No Build (2024)	Build (2024)
Eastbound	LR			A 10			A 10
Eagle A	ve	William .	-3-4		FE 1 - VIII -	COLOR TO	
Northbound	L			A 9			A 9
	OVERALL:		1 12 4	A 1			A 1

Control Type: TWSC

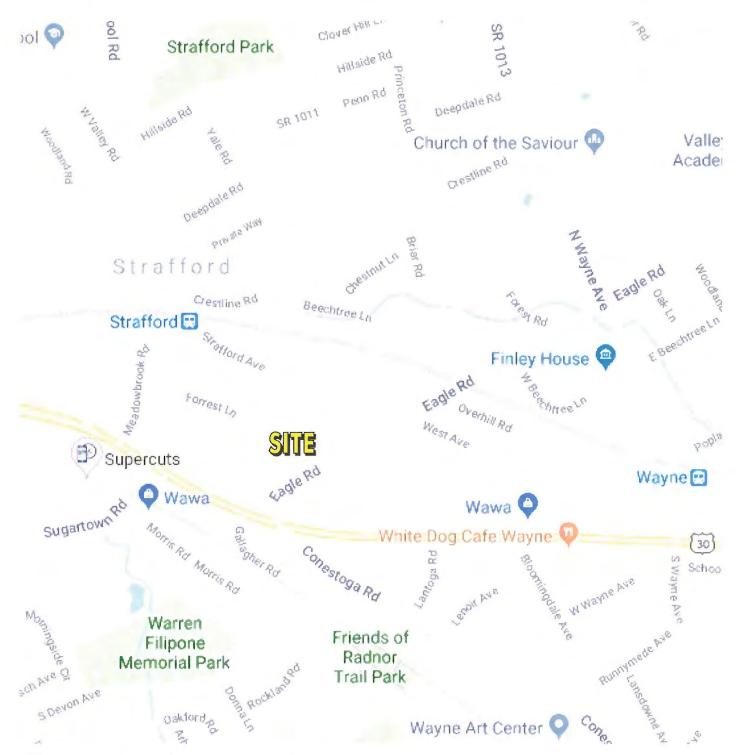


### Site and Surrounding Area - Map View

Strafford Avenue Residential - Townhouses Radnor Township, Delaware County, Pennsylvania



#### **April 2020\***

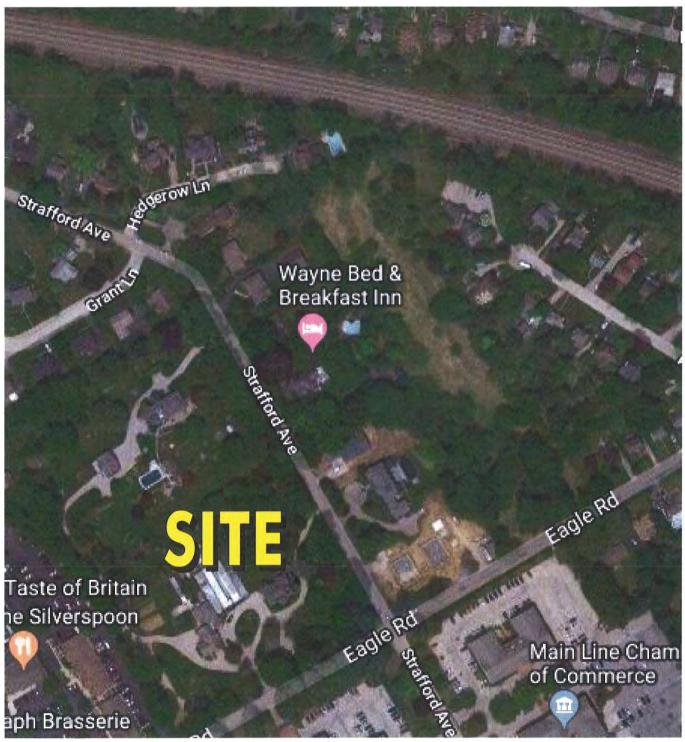


<sup>\*</sup> Figure preparation date. See report for data collection date(s).

## Site and Surrounding Area - Aerial View

Strafford Avenue Residential - Townhouses Radnor Township, Delaware County, Pennsylvania





## **Site Plan Excerpt**

Strafford Avenue Residential - Townhouses Radnor Township, Delaware County, Pennsylvania



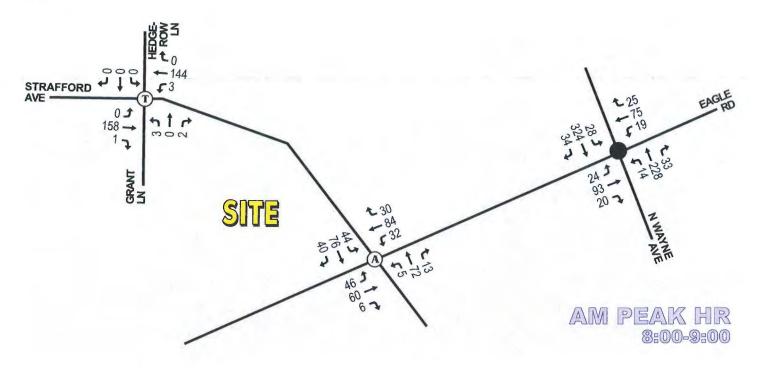


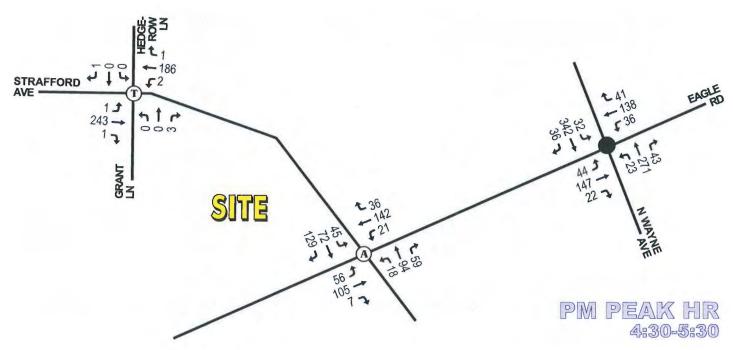


### **Existing (2019) Peak Hour Traffic Volumes**

Strafford Avenue Residential - Townhouses Radnor Township, Delaware County, Pennsylvania









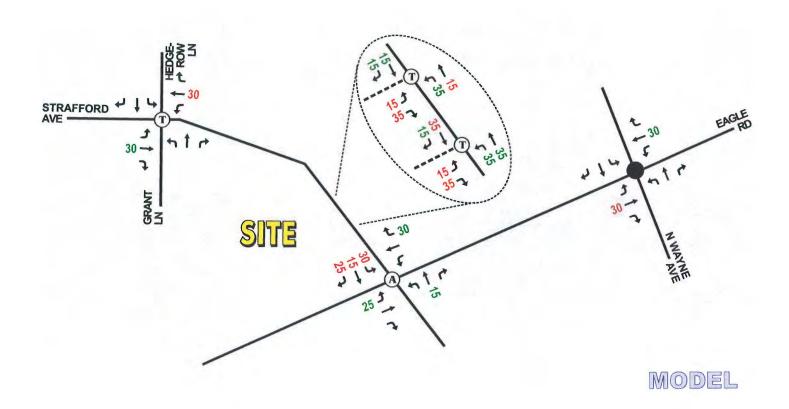
#### F. Tavani and Associates, Inc.

**Traffic Engineering and Planning** 

### Site Peak Hour Traffic - Model

Strafford Avenue Residential - Townhouses Radnor Township, Delaware County, Pennsylvania



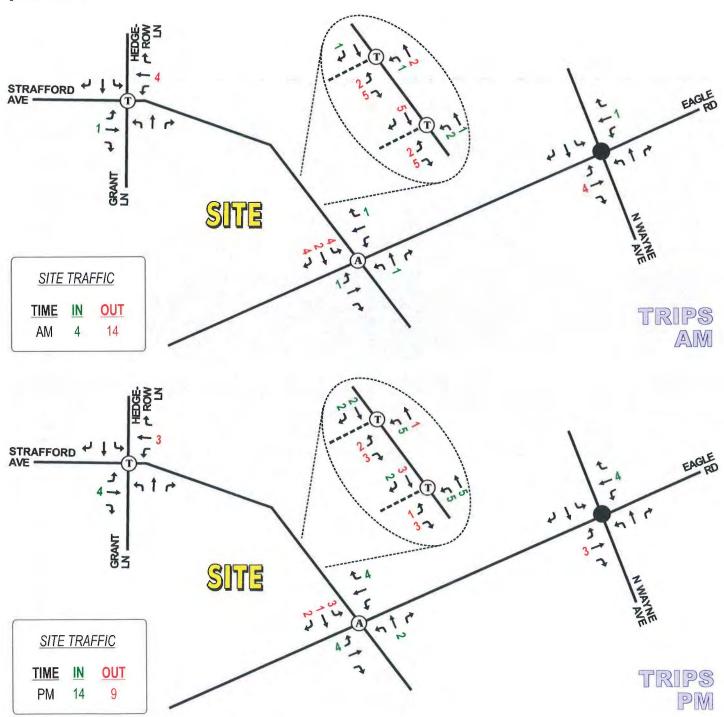




## Site Peak Hour Traffic - Volumes

Strafford Avenue Residential - Townhouses Radnor Township, Delaware County, Pennsylvania



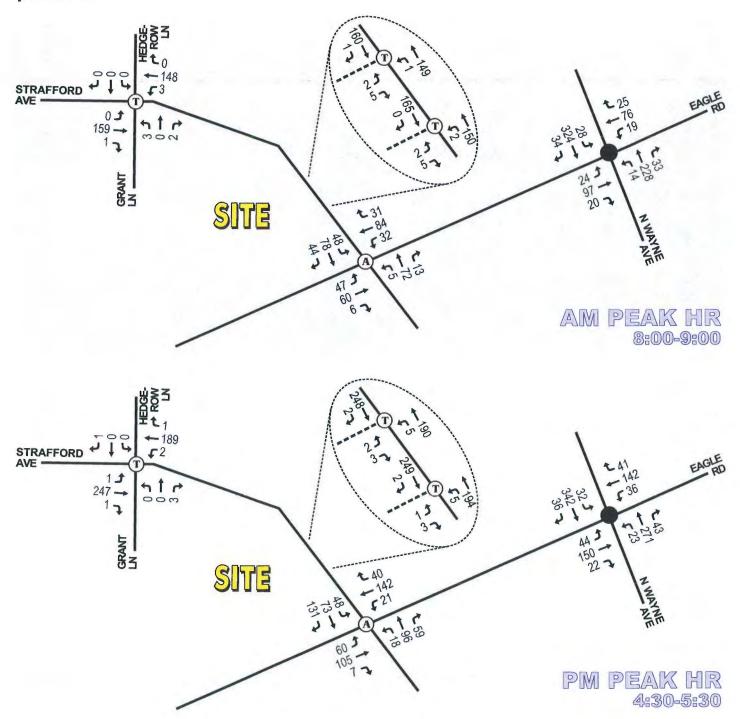




## Future (2024) Build Peak Hour Traffic Volumes

Strafford Avenue Residential - Townhouses Radnor Township, Delaware County, Pennsylvania





# APPENDIX A Correspondence

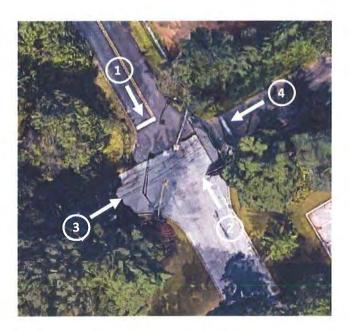
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# APPENDIX B Photodocumentation

#### Job #: 219-011

#### Road name (# of pages)

- 1. Eagle Road & Strafford Road (3)
- 2. Strafford Avenue & Grant Lane/Hedgerow Lane (3)
- 3. Eagle Road & Wayne Avenue (3)



Aerial image of intersection



Photo #1 - Description: Eastbound Strafford Road



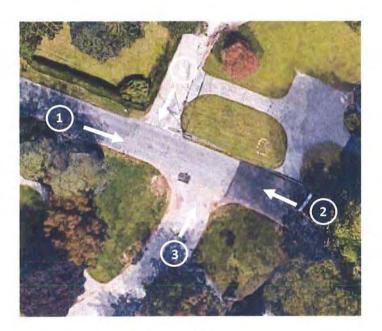
Photo # 2 - Description: Westbound Strafford Road



Photo #3 - Description: Northbound Eagle Road



Photo #4 - Description: Southbound Eagle Road



Aerial image of intersection



Photo #1 - Description: Eastbound Strafford Road



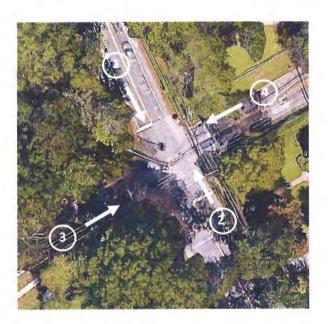
Photo # 2 - Description: Westbound Strafford Road



Photo #3 - Description: Northbound Grant Lane



Photo #4 - Description: Southbound Hedgerow Lane



Aerial image of intersection



Photo #1 - Description: Eastbound Wayne Avenue



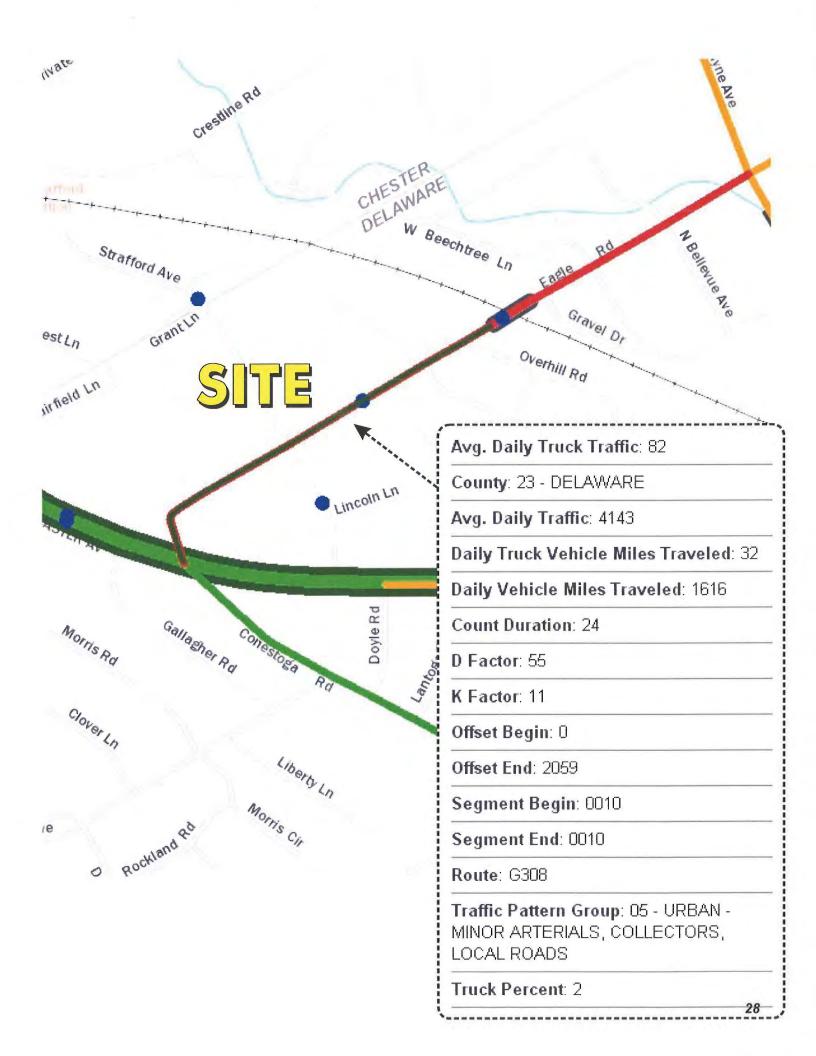
Photo #2 - Description: Westbound Wayne Avenue

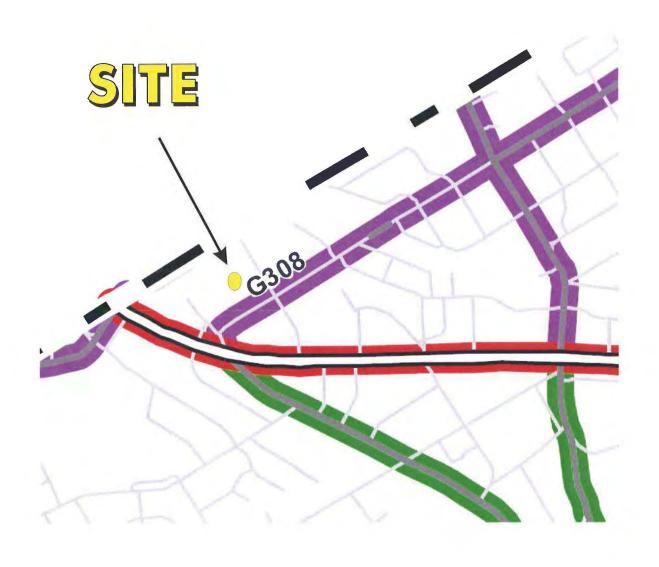


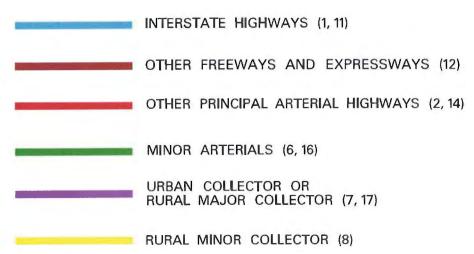
Photo #3 - Description: Northbound Eagle Road



Photo #4 - Description: Southbound Eagle Road







## **APPENDIX E**

Trip Generation, Background Growth & Other Developments

		actors for August 20		Decarl
County	Urban Interstate	Rural Interstate	Urban Non-Interstate	Rural Non-Interstate
ADAMS	,	*	0.98	0.75
ALLEGHENY	0.86	2.18	0.00	0.38
ARMSTRONG	0.85		0.00	0.38
BEAVER	0.80	1.96	0.00	0.35
BEDFORD	*	2.13	0.00	0.44
BERKS	1.16	2.43	0.26	0.58
	0.81	1.94	0.00	0.37
BLAIR		1.54	0.07	0.51
BRADFORD	1.14			
BUCKS	1.36	2.33	0.60	0.61
BUTLER	1.79	2.75	0.71	0.76
CAMBRIA	0.40	.90	0.00	0.19
CAMERON	•			0.16
CARBON	1.35	2.60	0.38	0.64
CENTRE	1.53	2.55	0.70	0.69
CHESTER	1.74	3.02	0.58	0.82
CLARION	0.96	2.02	0.00	0.41
CLEARFIELD	0.99	2.09	0.01	0.44
	0.95	2.26	0.00	0.47
CLINTON		2.29	0.35	0.56
COLUMBIA	1.19			0.56
CRAWFORD	0.95	2.00	0.09	
CUMBERLAND	1.58	2.56	0.80	0.70
DAUPHIN	1.37		0.47	0.64
DELAWARE	0.99		0.00	**
ELK			0.00	0.31
ERIE	1.00	2.16	0.00	0.45
FAYETTE	0.84		0.00	0.40
FOREST	*			0.67
FRANKLIN	1.36	2.57	0.53	0.66
	1.50	2.13	0.00	0.52
FULTON				0.52
GREENE	1.23	2.63	0.00	
HUNTINGDON		1.96	0.00	0.39
INDIANA	1.22		0.17	0.53
JEFFERSON	•	2.13	0.00	0.44
JUNIATA				0.57
LACKAWANNA	0.85	2.30	0.00	0.44
LANCASTER	1.79	2.67	1.14	0.80
LAWRENCE	0.80	2.09	0.00	0.37
LEBANON	*	2.48	0.45	0.62
	1.58	2.88	0.48	0.74
LEHIGH		2.17	0.00	0.40
LUZERNE	0.77			
LYCOMING	1.02	2.18	0.04	0.47
MCKEAN	0.66	*	0.00	0.34
MERCER	0.69	1.99	0.00	0.35
MIFFLIN	0.80	*	0.00	0.39
MONROE	1.44	2.49	0.73	0.68
MONTGOMERY	1.21		0.34	0.58
MONTOUR	1.53	2.64	0.34	0.67
NORTHAMPTON	1.33	2.56	0.47	0.65
NORTHUMBERLAND	0.83	2.09	0.00	0.41
	*	*	0.98	0.65
PERRY			0.00	0.00
PHILADELPHIA	0.75			0.98
PIKE	2.20	2.84	1.64	
POTTER	*		*	0.48
SCHUYLKILL.	0.64	1.92	0.00	0.35
SNYDER	1.21		0.40	0.57
SOMERSET	0.65	1.76	0.00	0.34
SULLIVAN				0.43
SUSQUEHANNA	1.16	2.26	0.33	0.54
TIOGA	1.10	2.20		0.50
		2.46	0.87	0.70
UNION	1.57			
VENANGO		1.71	0.00	0.29
WARREN	*		0.00	0.38
WASHINGTON	1.32	2.63	0.15	0.60
WAYNE	*	2.25	0.21	0.53
WESTMORELAND	0.96	2.09	0.00	0.42
WYOMING	*		0.00	0.44
***************************************	1.39	2.56	0.60	0.67

<sup>\* =</sup> Functional Class Doesn't Exist in County

Questions? Please contact Andrew O'Neill at the Bureau of Planning and Research, 717-346-3250 or andoneill@pa.gov

NOTE: The projected growth factors are derived using historical VMT (Vehicle Miles Traveled) data (1994 to 2017), as well as Woods and Poole demographic and economic data. The factors should be compounded when calculating future values. The factors should not be used to project traffic beyond a 20-year period. Please be aware that these factors are estimates, and unforeseen events (opening of shopping centers, fast food franchises, gas stations, etc) could cause growth to change over time.



		actors for August 20		Burnel
County	Urban Interstate	Rural Interstate	Urban Non-Interstate	Rural Non-Interstate
ADAMS	*		0.93	0.73
ALLEGHENY	0.81		0.00	0.37
ARMSTRONG	0.79		0.00	0.36
BEAVER	0.73	1.93	0.00	0.33
BEDFORD	*	2.10	0.00	0.42
BERKS	1.10	2.41	0.20	0.57
BLAIR	0.75	1.91	0.00	0.36
BRADFORD	1.08		0.01	0.49
BUCKS	1.31	2.31	0.54	0.59
BUTLER	1.75	2.74	0.65	0.75
CAMBRIA	0.34	*	0.00	0.18
CAMERON	•			0.14
CARBON	1.30	2.58	0.33	0.62
CENTRE	1.49	2.53	0.65	0.68
CHESTER	1.70	2.99	0.52	0.80
CLARION	0.90	2.00	0.00	0.40
CLEARFIELD	0.93	2.06	0.00	0.42
CLINTON	0.88	2.21	0.00	0.45
COLUMBIA	1.14	2.25	0.30	0.54
CRAWFORD	0.89	1.96	0.03	0.42
	1.53	2.55	0.74	0.69
CUMBERLAND	1.31	ž.55	0.41	0.63
DAUPHIN DELAWARE	0.93		0.00	0.03
	0.93		0.00	0.29
ELK	0.95	2.14	0.00	0.43
ERIE	0.95	2.14	0.00	0.38
FAYETTE	0.77		0.00	0.65
FOREST		2.54	0.47	0.65
FRANKLIN	1.31		*	0.50
FULTON		2.10	0.00	0.56
GREENE	1.19	2.62	0.00	0.37
HUNTINGDON		1.91		0.52
INDIANA	1.17		0.11	
JEFFERSON		2.11	0.00	0.42
JUNIATA				0.55
LACKAWANNA	0.78	2.27	0.00	0.42
LANCASTER	1.74	2.64	1.08	0.78
LAWRENCE	0.74	2.05	0.00	0.35
LEBANON		2.44	0.39	0.61
LEHIGH	1.54	2.86	0.43	0.73
LUZERNE	0.71	2.14	0.00	0.39
LYCOMING	0.96	2.16	0.00	0.45
MCKEAN	0.60	*-	0.00	0.33
MERCER	0.63	1.96	0.00	0.33
MIFFLIN	0.73		0.00	0.37
MONROE	1.40	2.46	0.68	0.67
MONTGOMERY	1.17		0.28	0.57
MONTOUR	1.48	2.61	0.28	0.65
NORTHAMPTON	1.28	2.53	0.41	0.63
NORTHUMBERLAND	0.75	2.04	0.00	0.39
PERRY		•	0.92	0.63
PHILADELPHIA	0.69	•	0.00	*
PIKE	2.14	2.79	1.59	0.96
POTTER			*-	0.46
SCHUYLKILL	0.58	1.89	0.00	0.33
SNYDER	1.15	*	0.35	0.55
SOMERSET	0.59	1.72	0.00	0.32
SULLIVAN	•		*	0.42
SUSQUEHANNA	1.11	2.23	0.27	0.53
TIOGA	•		*	0.48
UNION	1.52	2.42	0.82	0.69
VENANGO	•	1.67	0.00	0.28
WARREN			0.00	0.36
WASHINGTON	1.28	2.62	0.10	0.59
WASHINGTON	1.20	2.22	0.16	0.51
WESTMORELAND	0.89	2.05	0.00	0.40
WYOMING	0.05	*	0.00	0.43
AA LOIMING		2.53	0.54	0.66

<sup>\* =</sup> Functional Class Doesn't Exist in County

Questions? Please contact Andrew O'Neill at the Bureau of Planning and Research, 717-346-3250 or andoneill@pa.gov

NOTE: The projected growth factors are derived using historical VMT (Vehicle Miles Traveled) data (1994 to 2018), as well as Woods and Poole demographic and economic data. The factors should be compounded when calculating future values. The factors should not be used to project traffic beyond a 20-year period. Please be aware that these factors are estimates, and unforeseen events (opening of shopping centers, fast food franchises, gas stations, etc) could cause growth to change over time.



## Land Use: 220 Multifamily Housing (Low-Rise)

#### Description

Low-rise multifamily housing includes apartments, townhouses, and condominiums located within the same building with at least three other dwelling units and that have one or two levels (floors). Multifamily housing (mid-rise) (Land Use 221), multifamily housing (high-rise) (Land Use 222), and off-campus student apartment (Land Use 225) are related land uses.

#### **Additional Data**

In prior editions of *Trip Generation Manual*, the low-rise multifamily housing sites were further divided into rental and condominium categories. An investigation of vehicle trip data found no clear differences in trip making patterns between the rental and condominium sites within the ITE database. As more data are compiled for future editions, this land use classification can be reinvestigated.

For the three sites for which both the number of residents and the number of occupied dwelling units were available, there were an average of 2.72 residents per occupied dwelling unit.

For the two sites for which the numbers of both total dwelling units and occupied dwelling units were available, an average of 96.2 percent of the total dwelling units were occupied.

This land use included data from a wide variety of units with different sizes, price ranges, locations, and ages. Consequently, there was a wide variation in trips generated within this category. Other factors, such as geographic location and type of adjacent and nearby development, may also have had an effect on the site trip generation.

Time-of-day distribution data for this land use are presented in Appendix A. For the 10 general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:15 and 8:15 a.m. and 4:45 and 5:45 p.m., respectively. For the one site with Saturday data, the overall highest vehicle volume was counted between 9:45 and 10:45 a.m. For the one site with Sunday data, the overall highest vehicle volume was counted between 11:45 a.m. and 12:45 p.m.

For the one dense multi-use urban site with 24-hour count data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:00 and 8:00 a.m. and 6:15 and 7:15 p.m., respectively.

For the three sites for which data were provided for both occupied dwelling units and residents, there was an average of 2.72 residents per occupied dwelling unit.

The average numbers of person trips per vehicle trip at the five general urban/suburban sites at which both person trip and vehicle trip data were collected were as follows:

- 1.13 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.21 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.



The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in British Columbia (CAN), California, District of Columbia, Florida, Georgia, Illinois, Indiana, Maine, Maryland, Minnesota, New Jersey, New York, Ontario, Oregon, Pennsylvania, South Dakota, Tennessee, Texas, Utah, Virginia, and Washington.

It is expected that the number of bedrooms and number of residents are likely correlated to the number of trips generated by a residential site. Many of the studies included in this land use did not indicate the total number of bedrooms. To assist in the future analysis of this land use, it is important that this information be collected and included in trip generation data submissions.

#### **Source Numbers**

168, 187, 188, 204, 211, 300, 305, 306, 319, 320, 321, 357, 390, 412, 418, 525, 530, 571, 579, 583, 864, 868, 869, 870, 896, 903, 918, 946, 947, 948, 951



## Multifamily Housing (Low-Rise) (220)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday

Setting/Location: General Urban/Suburban

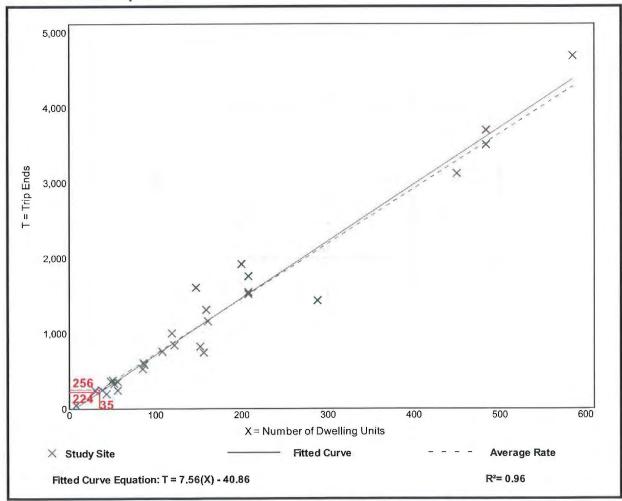
Number of Studies: Avg. Num. of Dwelling Units: 168

Directional Distribution: 50% entering, 50% exiting

#### Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
7.32	4.45 - 10.97	1.31

#### **Data Plot and Equation**



Trip Gen Manual, 10th Ed + Supplement • Institute of Transportation Engineers

## Multifamily Housing (Low-Rise)

(220)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic, One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

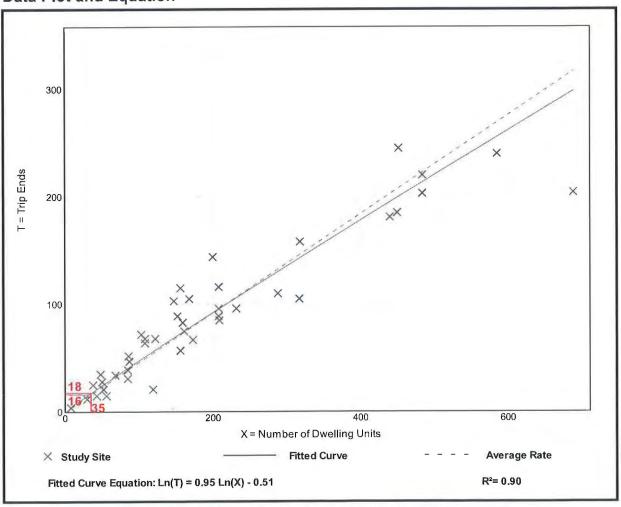
Number of Studies: 42 Avg. Num. of Dwelling Units: 199

Directional Distribution: 23% entering, 77% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.46	0.18 - 0.74	0.12

#### **Data Plot and Equation**



Trip Gen Manual, 10th Ed + Supplement • Institute of Transportation Engineers

## Multifamily Housing (Low-Rise)

(220)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

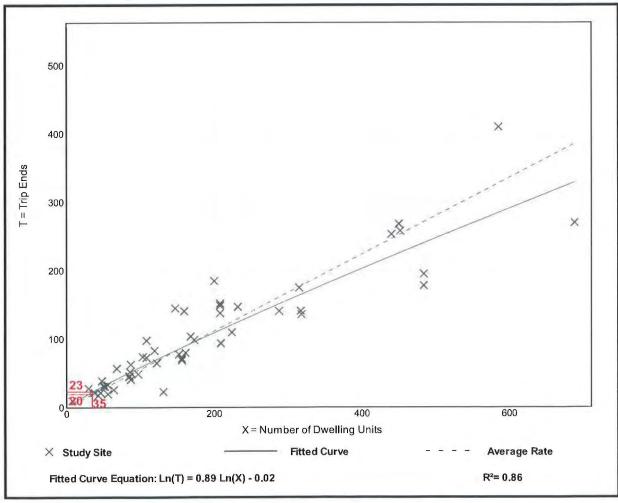
Number of Studies: 50 Avg. Num. of Dwelling Units: 187

Directional Distribution: 63% entering, 37% exiting

#### Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.56	0.18 - 1.25	0.16

#### **Data Plot and Equation**



Trip Gen Manual, 10th Ed + Supplement • Institute of Transportation Engineers

## APPENDIX F

Capacity Analyses

	*	-	7	1	+	*	1	1	-	1	+	1
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		4			4			4			4	
Traffic Volume (veh/h)	24	93	20	19	75	25	14	228	33	28	324	34
Future Volume (veh/h)	24	93	20	19	75	25	14	228	33	28	324	34
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1800	1761	1800	1872	1827	1872	1872	1806	1872	1800	1756	1800
Adj Flow Rate, veh/h	26	101	22	21	82	27	15	248	36	30	352	37
Adj No. of Lanes	0	1	0	0	1	0	0	1	0	0	1	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	0	0	0	0	0	0	4	4	4	2	2	2
Cap, veh/h	119	186	38	115	180	55	99	1026	143	117	1016	102
Arrive On Green	0.15	0.15	0.15	0.15	0.15	0.15	0.68	0.68	0.68	0.68	0.68	0.68
Sat Flow, veh/h	205	1212	245	188	1178	358	30	1505	210	55	1491	150
Grp Volume(v), veh/h	149	0	0	130	0	0	299	0	0	419	0	0
Grp Sat Flow(s), veh/h/ln	1662	0	0	1725	0	0	1745	0	0	1696	0	0
Q Serve(g_s), s	0.7	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	3.9	0.0	0.0	3.2	0.0	0.0	3.1	0.0	0.0	4.9	0.0	0.0
Cycle Q Clear(g_c), s	0.17	0.0	0.15	0.16	0.0	0.21	0.05	0.0	0.12	0.07	0.0	0.09
Prop In Lane	342	0	0.13	350	0	0.21	1268	0	0.12	1236	0	0.00
Lane Grp Cap(c), veh/h V/C Ratio(X)	0.44	0.00	0.00	0.37	0.00	0.00	0.24	0.00	0.00	0.34	0.00	0.00
The second secon	794	0.00	0.00	816	0.00	0.00	1268	0.00	0.00	1236	0.00	0.00
Avail Cap(c_a), veh/h HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00
Upstream Filter(I)	19.0	0.00	0.0	18.7	0.00	0.0	3.0	0.0	0.00	3.2	0.0	0.0
Uniform Delay (d), s/veh	0.9	0.0	0.0	0.7	0.0	0.0	0.4	0.0	0.0	0.7	0.0	0.0
Incr Delay (d2), s/veh			0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Initial Q Delay(d3),s/veh	0.0	0.0			0.0	0.0	3.0	0.0	0.0	4.7	0.0	0.0
%ile BackOfQ(95%),veh/ln	3.4	0.0	0.0	3.0	0.0	0.0	3.4	0.0	0.0	4.7	0.0	0.0
LnGrp Delay(d),s/veh	19.9	0.0	0.0	19.4	0.0	0.0	3.4 A	0.0	0.0		0.0	0.0
LnGrp LOS	В	440		В	400		A	000		A	440	
Approach Vol, veh/h		149			130			299			419	
Approach Delay, s/veh		19.9			19.4			3.4			4.0	
Approach LOS		В			В			Α			Α	
Timer	1	2	3	4	5	6	7	8	9	an artis		
Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		37.0		11.4		37.0		11.4				
Change Period (Y+Rc), s		5.0		5.0		5.0		5.0				
Max Green Setting (Gmax), s		32.0		20.0		32.0		20.0				
Max Q Clear Time (g_c+l1), s		5.1		5.9		6.9		5.2				
Green Ext Time (p_c), s		2.0		0.6		2.9		0.6				
Intersection Summary		-					1,00, 5	La Tar	Lorde.	W. Bi	A COL	Time.
HCM 2010 Ctrl Delay			8.2									
HCM 2010 LOS			Α									

EX am 06/17/2019

Synchro 10 Light Report Page 1

Intersection	
Intersection Delay, s/veh	9.4
Intersection LOS	Α

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations		4			4			4			4		
Traffic Vol, veh/h	46	60	6	32	84	30	5	72	13	44	76	40	
Future Vol, veh/h	46	60	6	32	84	30	5	72	13	44	76	40	
Peak Hour Factor	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	
Heavy Vehicles, %	2	7	0	6	1	3	0	3	0	5	0	0	
Mvmt Flow	58	76	8	41	106	38	6	91	16	56	96	51	
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0	
Approach	EB	W. L	TO A	WB	15 (4)	/43	NB	71	PE VIII	SB		77.	State State of the
Opposing Approach	WB			EB			SB			NB			
Opposing Lanes	1			1			1			1			
Conflicting Approach Le	eft SB			NB			EB			WB			
Conflicting Lanes Left	1			1			1			1			
Conflicting Approach Ri	ghNB			SB			WB			EB			
Conflicting Lanes Right	1			1			1			1			
HCM Control Delay	9.3			9.6			8.9			9.7			
HCM LOS	Α			Α			Α			Α			

Lane	NBLn1	EBLn1V	VBLn1	SBLn1
Vol Left, %	6%	41%	22%	28%
Vol Thru, %	80%	54%	58%	48%
Vol Right, %	14%	5%	21%	25%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	90	112	146	160
LT Vol	5	46	32	44
Through Vol	72	60	84	76
RT Vol	13	6	30	40
Lane Flow Rate	114	142	185	203
Geometry Grp	1	1	1	1
Degree of Util (X)	0.155	0.196	0.25	0.272
Departure Headway (Hd)	4.886	4.988	4.873	4.833
Convergence, Y/N	Yes	Yes	Yes	Yes
Сар	728	715	732	737
Service Time	2.956	3.058	2.938	2.897
HCM Lane V/C Ratio	0.157	0.199	0.253	0.275
HCM Control Delay	8.9	9.3	9.6	9.7
HCM Lane LOS	Α	Α	Α	Α
HCM 95th-tile Q	0.5	0.7	1	1.1

		e			

Intersection Delay, s/veh 8.8 Intersection LOS A

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations		4			4			4			4		
Traffic Vol, veh/h	0	158	1	3	144	0	3	0	2	1	0	0	
Future Vol, veh/h	0	158	1	3	144	0	3	0	2	1	0	0	
Peak Hour Factor	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	
Heavy Vehicles, %	0	1	0	33	3	0	33	0	0	0	0	0	
Mvmt Flow	0	208	1	4	189	0	4	0	3	1	0	0	
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0	
Approach		EB		WB	Tp =	11-11	NB		100	SB			
Opposing Approach		WB		EB			SB			NB			
Opposing Lanes		1		1			1			1			
Conflicting Approach Le	ft	SB		NB			EB			WB			
Conflicting Lanes Left		1		1			1			1			
Conflicting Approach Ri	ght	NB		SB			WB			EB			
Conflicting Lanes Right		1		1			1			1			
HCM Control Delay		8.4		9.2			8.3			8			
HCM LOS		Α		Α			Α			Α			

Lane	NBLn1	EBLn1V	VBLn1	SBLn1
Vol Left, %	60%	0%	2%	100%
Vol Thru, %	0%	99%	98%	0%
Vol Right, %	40%	1%	0%	0%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	5	159	147	1
LT Vol	3	0	3	1
Through Vol	0	158	144	0
RT Vol	2	1	0	0
Lane Flow Rate	7	209	193	1
Geometry Grp	1	1	1	1
Degree of Util (X)	0.01	0.237	0.249	0.002
Departure Headway (Hd)	5.259	4.071	4.635	5.023
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	685	872	772	717
Service Time	3.259	2.145	2.688	3.024
HCM Lane V/C Ratio	0.01	0.24	0.25	0.001
HCM Control Delay	8.3	8.4	9.2	8
HCM Lane LOS	Α	Α	Α	Α
HCM 95th-tile Q	0	0.9	1	0

	J	-	7	1	-	*	1	†	1	1	ţ	1
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		4			44			4			4	
Traffic Volume (veh/h)	44	147	22	36	138	41	23	271	43	32	342	36
Future Volume (veh/h)	44	147	22	36	138	41	23	271	43	32	342	36
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1800	1779	1800	1872	1872	1872	1872	1838	1872	1800	1770	1800
Adj Flow Rate, veh/h	46	153	23	38	144	43	24	282	45	33	356	38
Adj No. of Lanes	0	1	0	0	1	0	0	1	0	0	1	0
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	1	1	1	0	0	0	2	2	2	2	2	2
Cap, veh/h	135	246	34	123	240	66	108	960	146	117	963	98
Arrive On Green	0.20	0.20	0.20	0.20	0.20	0.20	0.65	0.65	0.65	0.65	0.65	0.65
Sat Flow, veh/h	253	1257	175	207	1227	339	51	1482	225	63	1487	151
Grp Volume(v), veh/h	222	0	0	225	0	0	351	0	0	427	0	0
Grp Sat Flow(s), veh/h/ln	1685	0	0	1773	0	0	1758	0	0	1701	0	0
Q Serve(g_s), s	0.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Cycle Q Clear(g_c), s	6.0	0.0	0.0	5.7	0.0	0.0	4.4	0.0	0.0	5.8	0.0	0.0
Prop In Lane	0.21	0.0	0.10	0.17	0.0	0.19	0.07	0.0	0.13	0.08	0.0	0.09
A STATE OF THE STA	414	0	0.10	429	0	0.19	1214	0	0.13	1178	0	0.00
Lane Grp Cap(c), veh/h	0.54	0.00	0.00	0.52	0.00	0.00	0.29	0.00	0.00	0.36	0.00	0.00
V/C Ratio(X)	759	0.00	0.00	794	0.00	0.00	1214	0.00	0.00	1178	0.00	0.00
Avail Cap(c_a), veh/h	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
HCM Platoon Ratio		0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00
Upstream Filter(I)	1.00		0.00	18.8	0.00	0.00	3.9	0.0	0.0	4.2	0.00	0.00
Uniform Delay (d), s/veh	18.9	0.0		1.0	0.0	0.0	0.6	0.0	0.0	0.9	0.0	0.0
Incr Delay (d2), s/veh	1.1	0.0	0.0				0.0	0.0	0.0	0.0	0.0	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0		0.0		3.0		0.0
%ile BackOfQ(50%),veh/ln	3.0	0.0	0.0	3.0	0.0	0.0	2.3		0.0		0.0	0.0
LnGrp Delay(d),s/veh	20.0	0.0	0.0	19.8	0.0	0.0	4.5	0.0	0.0	5.1	0.0	0.0
LnGrp LOS	В			В			A	0.51		A	407	
Approach Vol, veh/h		222			225			351			427	
Approach Delay, s/veh		20.0			19.8			4.5			5.1	
Approach LOS		В			В			Α			Α	
Timer	1	2	3	4	5	6	7	8	7		No. of the last	STANK K
Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		37.0		13.9		37.0		13.9				
Change Period (Y+Rc), s		5.0		5.0		5.0		5.0				
Max Green Setting (Gmax), s		32.0		20.0		32.0		20.0				
Max Q Clear Time (g_c+l1), s		6.4		8.0		7.8		7.7				
Green Ext Time (p_c), s		2.4		1.0		3.0		1.0				
Intersection Summary	A PART A			S. S. Barre	The Sale	18 一块	1.15		Talle 1			
HCM 2010 Ctrl Delay			10.3									
HCM 2010 LOS			В									

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Intersection		Section 1	Sell,
Intersection	Delay,	s/veh	10.7
Intersection	LOS		В

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	MATERIAL TELESCOPE
Lane Configurations		4			4			4			4		
Traffic Vol, veh/h	56	105	7	21	142	36	18	94	59	45	72	129	
Future Vol., veh/h	56	105	7	21	142	36	18	94	59	45	72	129	
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	
Heavy Vehicles, %	4	0	0	5	1	0	6	1	2	0	1	0	
Mvmt Flow	60	112	7	22	151	38	19	100	63	48	77	137	
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0	
Approach	EB		Q P	WB	101		NB		MAL.	SB	Miles III	- 077	
Opposing Approach	WB			EB			SB			NB			
Opposing Lanes	1			1			1			1			
Conflicting Approach Le	eft SB			NB			EB			WB			
Conflicting Lanes Left	1			1			1			1			
Conflicting Approach Ri	ghNB			SB			WB			EB			
Conflicting Lanes Right	1			1			1			1			
HCM Control Delay	10.6			10.9			10.3			10.9			
HCM LOS	В			В			В			В			

Lane	NBLn1	EBLn1V	VBLn1	SBLn1
Vol Left, %	11%	33%	11%	18%
Vol Thru, %	55%	62%	71%	29%
Vol Right, %	35%	4%	18%	52%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	171	168	199	246
LT Vol	18	56	21	45
Through Vol	94	105	142	72
RT Vol	59	7	36	129
Lane Flow Rate	182	179	212	262
Geometry Grp	1	1	1	1
Degree of Util (X)	0.268	0.274	0.315	0.363
Departure Headway (Hd)	5.295	5.518	5.357	4.99
Convergence, Y/N	Yes	Yes	Yes	Yes
Сар	677	651	671	720
Service Time	3.335	3.559	3.396	3.027
HCM Lane V/C Ratio	0.269	0.275	0.316	0.364
HCM Control Delay	10.3	10.6	10.9	10.9
HCM Lane LOS	В	В	В	В
HCM 95th-tile Q	1.1	1.1	1.3	1.7

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Intersection Delay, s/veh 8.7
Intersection LOS A

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	空区 使 理论 医牙髓
Lane Configurations		4			4			4			4		
Traffic Vol, veh/h	1	243	1	2	186	1	0	0	3	0	0	1	
Future Vol, veh/h	1	243	1	2	186	1	0	0	3	0	0	1	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	
Heavy Vehicles, %	0	0	0	0	2	0	0	0	0	0	0	0	
Mvmt Flow	1	264	1	2	202	1	0	0	3	0	0	1	
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0	
Approach	EB			WB				NB		Mary and	SB		
Opposing Approach	WB			EB				SB			NB		
Opposing Lanes	1			1				1			1		
Conflicting Approach Le	eft SB			NB				EB			WB		
Conflicting Lanes Left	1			1				1			1		
Conflicting Approach Ri	ghNB			SB				WB			EB		
Conflicting Lanes Right	1			1				1			1		
HCM Control Delay	8.9			8.4				7.3			7.3		
HCM LOS	Α			Α				Α			Α		

Lane	NBLn1	EBLn1	VBLn1	SBLn1
Vol Left, %	0%	0%	1%	0%
Vol Thru, %	0%	99%	98%	0%
Vol Right, %	100%	0%	1%	100%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	3	245	189	1
LT Vol	0	1	2	0
Through Vol	0	243	186	0
RT Vol	3	1	1	1
Lane Flow Rate	3	266	205	1
Geometry Grp	1	1	1	1
Degree of Util (X)	0.004	0.3		
Departure Headway (Hd)	4.294	4.059	4.104	4.297
Convergence, Y/N	Yes	Yes	Yes	Yes
Cap	838	881	869	838
Service Time	2.294	2.103	2.159	2.297
HCM Lane V/C Ratio	0.004	0.302	0.236	0.001
<b>HCM Control Delay</b>	7.3	8.9	8.4	7.3
HCM Lane LOS	Α	Α	Α	Α
HCM 95th-tile Q	0	1.3	0.9	0

Intersection					Y-int	1000	是一种类似的主义。2. 8 COM 由于各种的基本企业
Int Delay, s/veh	0.3						
Movement	EBL	EBR	NBL	NBT	SBT	SBR	<b>生力的基本主义性,抗发展的特殊的企业</b> 。
Lane Configurations	A			4	T»		
Traffic Vol, veh/h	2	5	2	150	165	0	
Future Vol, veh/h	2	5	2	150	165	0	
Conflicting Peds, #/hr	0	0	0	0	0	0	
Sign Control	Stop	Stop	Free	Free	Free	Free	
RT Channelized	-	None	-	None	-	None	
Storage Length	0	-	-	-	-	-	
Veh in Median Storage,	# 0	-	- 50	0	0	141	
Grade, %	0	+	-	0	0	-	
Peak Hour Factor	79	79	79	79	79	79	
Heavy Vehicles, %	0	0	0	0	0	0	
Mvmt Flow	3	6	3	190	209	0	
	linor2		Major1		Major2		THE STREET STREET, STR
Conflicting Flow All	405	209	209	0	-	0	
Stage 1	209	-	-	-			
Stage 2	196	-	-	-	-	-	
Critical Hdwy	6.4	6.2	4.3	7.0		-	
Critical Hdwy Stg 1	5.4	-	-	7.0	-	-	
Critical Hdwy Stg 2	5.4	-	-	-			
Follow-up Hdwy	3	3.1	3	-	-	-	
Pot Cap-1 Maneuver	688	885	1019	-		-	
Stage 1	956	-	-	-	-	-	
Stage 2	969	-	-	-	-	1 2	
Platoon blocked, %				-	-	-	
Mov Cap-1 Maneuver	686	885	1019	-	U	-	
Mov Cap-2 Maneuver	686	-	-	-	- 2	-	
Stage 1	953	1 -	-	-			
Stage 2	969	-	-	-	- 2	-	
¥.							
Approach	EB		NB	Total Var	SB	12.00	CONTRACTOR OF THE ACTUAL PROPERTY.
HCM Control Delay, s	9.5		0.1		0		
HCM LOS	Α						
Minor Lane/Major Mvmt	11.04	NBL	NBT	EBLn1	SBT	SBR	· · · · · · · · · · · · · · · · · · ·
Capacity (veh/h)		1019	-	0,,	-		
HCM Lane V/C Ratio		0.002	-	0.011		+	
HCM Control Delay (s)		8.5	0	9.5			
HCM Lane LOS		Α	Α			+	
HCM 95th %tile Q(veh)		0			-	-	

	*	-	1	1	+	*	1	†	~	1	+	1
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		4			4			4			4	
Traffic Volume (veh/h)	24	97	20	19	76	25	14	228	33	28	324	34
Future Volume (veh/h)	24	97	20	19	76	25	14	228	33	28	324	34
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1800	1762	1800	1872	1827	1872	1872	1806	1872	1800	1756	1800
Adj Flow Rate, veh/h	26	105	22	21	83	27	15	248	36	30	352	37
Adj No. of Lanes	0	1	0	0	1	0	0	1	0	0	1	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	0	0	0	0	0	0	4	4	4	2	2	2
Cap, veh/h	118	191	37	115	184	55	98	1023	143	117	1013	102
Arrive On Green	0.16	0.16	0.16	0.16	0.16	0.16	0.68	0.68	0.68	0.68	0.68	0.68
Sat Flow, veh/h	198	1227	239	185	1185	356	30	1505	210	55	1491	150
Grp Volume(v), veh/h	153	0	0	131	0	0	299	0	0	419	0	0
Grp Sat Flow(s), veh/h/ln	1665	Ö	0	1725	0	0	1745	0	0	1696	0	0
Q Serve(g_s), s	0.8	0.0	0:0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Cycle Q Clear(g_c), s	4.0	0.0	0.0	3.3	0.0	0.0	3.2	0.0	0.0	5.0	0.0	0.0
Prop In Lane	0.17	0.0	0.14	0.16	0.0	0.21	0.05		0.12	0.07		0.09
Lane Grp Cap(c), veh/h	346	0	0	355	0	0	1264	0	0	1232	0	0
V/C Ratio(X)	0.44	0.00	0.00	0.37	0.00	0.00	0.24	0.00	0.00	0.34	0.00	0.00
Avail Cap(c_a), veh/h	793	0.00	0.00	814	0.00	0.00	1264	0.00	0.00	1232	0	0
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00
	19.0	0.00	0.0	18.7	0.00	0.0	3.0	0.0	0.0	3.3	0.0	0.0
Uniform Delay (d), s/veh	0.9	0.0	0.0	0.6	0.0	0.0	0.4	0.0	0.0	0.8	0.0	0.0
Incr Delay (d2), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Initial Q Delay(d3),s/veh	3.5	0.0	0.0	3.0	0.0	0.0	3.0	0.0	0.0	4.7	0.0	0.0
%ile BackOfQ(95%),veh/ln		0.0	0.0	19.3	0.0	0.0	3.4	0.0	0.0	4.0	0.0	0.0
LnGrp Delay(d),s/veh	19.9	0.0	0.0		0.0	0.0	3.4 A	0.0	0.0	4.0 A	0.0	0.0
LnGrp LOS	В	450		В	404		A	200		A	419	
Approach Vol, veh/h		153			131			299				
Approach Delay, s/veh		19.9			19.3			3.4			4.0	
Approach LOS		В			В			Α			Α	
Timer	1	2	3	4	5	6	7	8	(Prof. 19) in			
Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		37.0		11.6		37.0		11.6				
Change Period (Y+Rc), s		5.0		5.0		5.0		5.0				
Max Green Setting (Gmax), s		32.0		20.0		32.0		20.0				
Max Q Clear Time (g_c+l1), s		5.2		6.0		7.0		5.3				
Green Ext Time (p_c), s		2.0		0.7		2.9		0.6				
Intersection Summary											11. 11.	ALT:
			0.0									
HCM 2010 Ctrl Delay			8.3									

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Intersection Delay shap	0.6	70			minut, sh	er de la			S. T.		14 E. E. II		A Pilling a		11.0
Intersection Delay, s/veh															
ntersection LOS	Α														
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	E 1836	hite (g	
Lane Configurations		4			4			4			4				
Traffic Vol, veh/h	47	60	6	32	84	31	5	72	13	48	78	44			
Future Vol, veh/h	47	60	6	32	84	31	5	72	13	48	78	44			
Peak Hour Factor	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79	0.79			
Heavy Vehicles, %	2	7	0	6	1	3	0	3	0	5	0	0			
Mvmt Flow	59	76	8	41	106	39	6	91	16	61	99	56			
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0			
Approach	EB	担与	- 4	WB			NB		(305)	SB				- 87	
Opposing Approach	WB			EB			SB			NB					
Opposing Lanes	1			1			1			1					
Conflicting Approach Lef	ft SB			NB			EB			WB					
Conflicting Lanes Left	1			1			1			1					
Conflicting Approach Rig	hNB			SB			WB			EB					
Conflicting Lanes Right	1			1			1			1					
HCM Control Delay	9.4			9.7			8.9			9.9					
HCM LOS	Α			Α			Α			Α					
	NAME AND POST OF THE PARTY.	um victory.					TO ME TO SE	W. 100		and the same	And Admin to 2	30 × 30 × 30	ota ilir ili	10 A 10 B	
Lane	1			VBLn1		190(2)299(0)	100							() 点	
Vol Left, %		6%	42%	22%	28%										
Vol Thru, %		80%	53%	57%	46%										
Vol Right, %		14%	5%	21%	26%										
Sign Control		Stop	Stop	Stop	Stop										
Traffic Vol by Lane		90	113	147	170										
LT Vol		5	47	32	48										
Through Vol		72	60	84	78										
RT Vol		13	6	31	44										
Lane Flow Rate		114	143	186	215										
Geometry Grp		1	1	1	1										
Degree of Util (X)		0.156			0.289										
Departure Headway (Hd	)	4.914	5.027	4.906	4.842										

Yes

726

9.7

A

Yes

723

8.9

Α

0.6

Yes

708

9.4

Α

0.7

2.99 3.099 2.974 2.909 0.158 0.202 0.256 0.292

Yes

736

9.9

A

1.2

Convergence, Y/N

HCM Lane V/C Ratio
HCM Control Delay

Service Time

HCM Lane LOS

HCM 95th-tile Q

Cap

	ni	Ω	re	Δ	$\sim$	п	0	r
1	133	v		U	v	u	v	

Intersection Delay, s/veh 8.8 Intersection LOS A

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations		4			4			4			4		
Traffic Vol, veh/h	0	159	1	3	148	0	3	0	2	1	0	0	
Future Vol, veh/h	0	159	1	3	148	0	3	0	2	1	0	0	
Peak Hour Factor	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	0.76	
Heavy Vehicles, %	0	1	0	33	3	0	33	0	0	0	0	0	
Mvmt Flow	0	209	1	4	195	0	4	0	3	1	0	0	
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0	
Approach	0101	EB	TIL	WB	""	- Are	NB			SB		The World	
Opposing Approach		WB		EB			SB			NB			
Opposing Lanes		1		1			1			1			
Conflicting Approach Le	eft	SB		NB			EB			WB			
Conflicting Lanes Left		1		1			1			1			
Conflicting Approach Ri	ght	NB		SB			WB			EB			
Conflicting Lanes Right		1		1			1			1			
HCM Control Delay		8.4		9.3			8.3			8			
HCM LOS		Α		Α			Α			Α			

Lane	NBLn1	EBLn1V	VBLn1	SBLn1
Vol Left, %	60%	0%	2%	100%
Vol Thru, %	0%	99%	98%	0%
Vol Right, %	40%	1%	0%	0%
Sign Control	Stop	Stop	Stop	Stop
Traffic Vol by Lane	5	160	151	1
LT Vol	3	0	3	1
Through Vol	0	159	148	0
RT Vol	2	1	0	0
Lane Flow Rate	7	211	199	1
Geometry Grp	1	1	1	1
Degree of Util (X)	0.01	0.238	0.256	0.002
Departure Headway (Hd)	5.273	4.075	4.636	5.038
Convergence, Y/N	Yes	Yes	Yes	Yes
Сар	683	869	771	714
Service Time	3.273	2.151	2.689	3.039
HCM Lane V/C Ratio	0.01	0.243	0.258	0.001
HCM Control Delay	8.3	8.4	9.3	8
HCM Lane LOS	Α	Α	Α	Α
HCM 95th-tile Q	0	0.9	1	0

NAMES OF TAXABLE PARTY.			artistica e e e e			THE THE PERSON NAMED IN
Intersection	0.0	ili ini.	THE STREET		g ht I	41.30
Int Delay, s/veh	0.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			ની	1	
Traffic Vol, veh/h	2	5	1	149	160	1
Future Vol, veh/h	2	5	1	149	160	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	_		-	
Veh in Median Storage	,# 0	-	_	0	0	
Grade, %	0	_	-	0	0	-
Peak Hour Factor	79	79	79	79	79	79
Heavy Vehicles, %	0	0	0	3	3	0
Mymt Flow	3	6	1	189	203	1
TATALLIE I 1044	J	Ū		100	200	•
	Minor2		Major1		Major2	
Conflicting Flow All	395	204	204	0	-	0
Stage 1	204	-	-	-	-	
Stage 2	191	-	-	-	-	-
Critical Hdwy	6.4	6.2	4.3	-	-	- 3
Critical Hdwy Stg 1	5.4	-	-	-	-	-
Critical Hdwy Stg 2	5.4	-	-	- 1	-	
Follow-up Hdwy	3	3.1	3	-	-	-
Pot Cap-1 Maneuver	698	891	1023		-	-
Stage 1	961	-	-		_	-
Stage 2	975				-	- 1
Platoon blocked, %	010				_	- 4
Mov Cap-1 Maneuver	697	891	1023		1,5-2	
Mov Cap-2 Maneuver	697	001	1025	- 4		
	960	-			-	-
Stage 1	975	-				
Stage 2	9/3	-	-	-	-	-
Approach	EB	3	NB	(e)(h)	SB	W. # W
HCM Control Delay, s	9.4		0.1	-	0	
HCM LOS	Α					
Minor Lane/Major Mvm	nt	NBL		EBLn1	SBT	SBR
Capacity (veh/h)		1023			-	
HCM Lane V/C Ratio		0.001	-			-
HCM Control Delay (s)		8.5	0	9.4	-	
HCM Lane LOS		Α	Α	Α		-
HCM 95th %tile Q(veh	)	0	*	0	-	-

Intersection	man de	1/3-14		<b>建</b> 处性	Take!	
Int Delay, s/veh	0.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			सी	ĵ.	
Traffic Vol, veh/h	1	3	5	194	249	2
Future Vol, veh/h	1	3	5	194	249	2
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	TAONE		-	_	110110
Veh in Median Storage,		7	(7)	0	0	
	# 0	-	-	0	0	
Grade, %		-	90	90	90	90
Peak Hour Factor	90	90				
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	1	3	6	216	277	2
Major/Minor M	linor2	The state of the s	Major1	N.	Major2	-1/3/1/
Conflicting Flow All	506	278	279	0	-	0
Stage 1	278		213	-		-
		-	-	-	-	_
Stage 2	228	6.0		-	-	-
Critical Hdwy	6.4	6.2	4.3		-	- 6
Critical Hdwy Stg 1	5.4	-	-	-	-	-
Critical Hdwy Stg 2	5.4	-	-	-	-	-
Follow-up Hdwy	3	3.1	3	-	¥	-
Pot Cap-1 Maneuver	598	809	964		-	-
Stage 1	886	-	_	-	-	-
Stage 2	936		-	-	+	-
Platoon blocked, %				- 2	2	-
Mov Cap-1 Maneuver	594	809	964	- 2	- 2	-
Mov Cap-2 Maneuver	594	-	-	-		-
Stage 1	880			-		-
	936	70.		-		140
Stage 2	930	-	-	-	-	-
Approach	EB		NB	atalah Ya	SB	
HCM Control Delay, s	9.9		0.2		0	
HCM LOS	Α					
110.11.200						
					000	000
Minor Lane/Major Mvmt	her me	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)		964	-			
HCM Lane V/C Ratio		0.006	-	0.006	-5	-
HCM Control Delay (s)		8.8	0	9.9	-	
HCM Lane LOS		Α	Α	Α	-	-
HCM 95th %tile Q(veh)		0	-	0	-	
				-		

	۶	-	*	1	<b>—</b>	1	1	<b>†</b>	-	1	<b>↓</b>	1
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		4			4			4			4	
Traffic Volume (veh/h)	44	150	22	36	142	41	23	271	43	32	342	36
Future Volume (veh/h)	44	150	22	36	142	41	23	271	43	32	342	36
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1800	1779	1800	1872	1872	1872	1872	1838	1872	1800	1770	1800
Adj Flow Rate, veh/h	46	156	23	38	148	43	24	282	45	33	356	38
Adj No. of Lanes	0	1	0	0	1	0	0	1	0	0	1	0
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	1	1	1	0	0	0	2	2	2	2	2	2
Cap, veh/h	134	249	34	122	244	66	108	958	146	117	961	98
Arrive On Green	0.20	0.20	0.20	0.20	0.20	0.20	0.65	0.65	0.65	0.65	0.65	0.65
Sat Flow, veh/h	249	1265	172	203	1238	333	51	1482	225	63	1486	151
Grp Volume(v), veh/h	225	0	0	229	0	0	351	0	0	427	0	0
Grp Sat Flow(s), veh/h/ln	1686	0	0	1775	0	0	1758	0	0	1701	0	0
Q Serve(g_s), s	0.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Cycle Q Clear(g_c), s	6.1	0.0	0.0	5.9	0.0	0.0	4.4	0.0	0.0	5.9	0.0	0.0
Prop In Lane	0.20	0.0	0.10	0.17	0.0	0.19	0.07	0.0	0.13	0.08		0.09
Lane Grp Cap(c), veh/h	417	0	0.10	432	0	0.10	1212	0	0	1175	0	0
V/C Ratio(X)	0.54	0.00	0.00	0.53	0.00	0.00	0.29	0.00	0.00	0.36	0.00	0.00
Avail Cap(c_a), veh/h	758	0.00	0.00	793	0.00	0.00	1212	0	0.00	1175	0	0
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	0.00
Uniform Delay (d), s/veh	18.9	0.0	0.0	18.8	0.0	0.0	4.0	0.0	0.0	4.2	0.0	0.0
	1.1	0.0	0.0	1.0	0.0	0.0	0.6	0.0	0.0	0.9	0.0	0.0
Incr Delay (d2), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Initial Q Delay(d3),s/veh	3.0	0.0	0.0	3.0	0.0	0.0	2.3	0.0	0.0	3.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	20.0	0.0	0.0	19.8	0.0	0.0	4.6	0.0	0.0	5.1	0.0	0.0
LnGrp Delay(d),s/veh		0.0	0.0	19.0 B	0.0	0.0	4.0 A	0.0	0.0	Α	0.0	0.0
LnGrp LOS	В	005		В	229			351			427	
Approach Vol, veh/h		225			19.8			4.6			5.1	
Approach Delay, s/veh		20.0						4.0 A			5.1 A	
Approach LOS		В			В			А			A	
Timer	1	2	3	4	5	6	7	8				
Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		37.0		14.1		37.0		14.1				
Change Period (Y+Rc), s		5.0		5.0		5.0		5.0				
Max Green Setting (Gmax), s		32.0		20.0		32.0		20.0				
Max Q Clear Time (g_c+l1), s		6.4		8.1		7.9		7.9				
Green Ext Time (p_c), s		2.4		1.0		3.0		1.0				
Intersection Summary				1.2				JIL 18	BILL		Tank.	
HCM 2010 Ctrl Delay			10.4									
HCM 2010 LOS			В									

FB pm 04/17/2020

Synchro 10 Light Report Page 2

Intersection			Admin	us in		H-dw		No. 1	No.51		11.31	A CONTRACTOR		Gal.
Intersection Delay, s/veh	110.9													
Intersection LOS	В													
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR		TE Non
Lane Configurations		4			4			4			4			
Traffic Vol, veh/h	60	105	7	21	142	40	18	96	59	48	73	131		
Future Vol, veh/h	60	105	7	21	142	40	18	96	59	48	73	131		
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94		
Heavy Vehicles, %	4	0	0	5	1	0	6	1	2	0	1	0		
Mvmt Flow	64	112	7	22	151	43	19	102	63	51	78	139		
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0		
Approach	EB		rigana)	WB			NB	10.00	TOTAL	SB	6	A de la	Mile-ma	
Opposing Approach	WB			EB			SB			NB				
Opposing Lanes	1			1			1			1				
Conflicting Approach Le	ft SB			NB			EB			WB				
Conflicting Lanes Left	1			1			1			1				
Conflicting Approach Rig	ghNB			SB			WB			EB				
Conflicting Lanes Right	1			1			1			1				
HCM Control Delay	10.8			11			10.4			11.1				
HCM LOS	В			В			В			В				
Lane	١		EBLn1V			116	300000		10/11		All of	L'Proje		
Vol Left, %		10%	35%	10%	19%									
Vol Thru, %		55%	61%	70%	29%									
Vol Right, %		34%	4%	20%	52%									
Sign Control		Stop	Stop	Stop	Stop									
Traffic Vol by Lane		173	172	203	252									
LT Vol		18	60	21	48									
Through Vol		96	105	142	73									
RT Vol		59	7	40	131									
Lane Flow Rate		184	183	216	268									
Geometry Grp		1	1	1	1									
Degree of Util (X)			12 C 2 C 3 (4)											
Departure Headway (Hd	1)	5.345	5.565	5.39	5.033									
Convergence, Y/N		Yes	Yes	Yes	Yes									
Сар		672	644	665	713									
Service Time		3.386	3.606	3.43	3.071									
HCM Lane V/C Ratio		0.274	0.284	0.325	0.376									
HCM Control Delay		10.4	10.8	11	11.1									
HCM Lane LOS		В	В	В	В									
HCM 95th-tile Q		1.1	1.2	1.4	1.7									

Intersection	100
Intersection Delay, s/veh	8.7
Intersection LOS	Α

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	the fire in	III IT	EDW-151
Lane Configurations	202	4		1100	4	TIDIC	1100	4	TO THE	001	4	ODIT			2000
Traffic Vol, veh/h	1	247	1	2	189	1	0	0	3	0	0	1			
Future Vol, veh/h	1	247	1	2	189	1	0	0	3	0	0	1			
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92			
Heavy Vehicles, %	0	0	0	0	2	0	0	0	0	0	0	0			
Mvmt Flow	1	268	1	2	205	1	0	- 0	3	0	0	1			
Number of Lanes	0	1	0	0	1	0	0	1	0	0	1	0			
Approach	EB			WB				NB	Jan-		SB			10.75	
Opposing Approach	WB			EB				SB			NB				
Opposing Lanes	1			1				1			1				
Conflicting Approach Le	eft SB			NB				EB			WB				
Conflicting Lanes Left	1			1				1			1				
Conflicting Approach Ri	ghNB			SB				WB			EB				
Conflicting Lanes Right	1			1				1			1				
HCM Control Delay	8.9			8.5				7.3			7.3				
HCM LOS	Α			Α				Α			Α				

Lane	NBLn1	EBLn1\	WBLn1	SBLn1		2"0" 27 3 Min (1)	St. Systems 19	10 mg	The spin and the second	Company of the Sales of the Sal
Vol Left, %	0%	0%	1%	0%						
Vol Thru, %	0%	99%	98%	0%						
Vol Right, %	100%	0%	1%	100%						
Sign Control	Stop	Stop	Stop	Stop						
Traffic Vol by Lane	3	249	192	1						
LT Vol	0	1	2	0						
Through Vol	0	247	189	0						
RT Vol	3	1	1	1						
Lane Flow Rate	3	271	209	1						
Geometry Grp	1	1	1	1						
Degree of Util (X)	0.004	0.305	0.238							
Departure Headway (Hd)	4.31	4.061	4.108	4.313						
Convergence, Y/N	Yes	Yes	Yes	Yes						
Сар	835	880	868	835						
Service Time	2.31	2.107	2.163							
HCM Lane V/C Ratio	0.004	0.308	0.241							
HCM Control Delay	7.3	8.9	8.5	7.3						
HCM Lane LOS	Α	Α	Α	Α						
HCM 95th-tile Q	0	1.3	0.9	0						

Intersection	168		E- , ]				THE SECTION OF THE SE
Int Delay, s/veh	0.2						
Movement	EBL	EBR	NBL	NBT	SBT	SBR	
Lane Configurations	A.A.			सी	1		
Traffic Vol, veh/h	2	3	5	190	248	3	
Future Vol, veh/h	2	3	5	190	248	3	
Conflicting Peds, #/hr	0	0	0	0	0	0	
	Stop	Stop	Free	Free	Free	Free	
RT Channelized	-	None		None	-	None	
Storage Length	0	-	-	77	(#3)	-	
Veh in Median Storage,	# 0	-	+	0	0		
Grade, %	0	(1 <del>.1.</del> 1)	-	0	0	-	
Peak Hour Factor	90	90	90	90	90	90	
Heavy Vehicles, %	0	0	0	2	3	0	
Mvmt Flow	2	3	6	211	276	3	
	inor2		/lajor1		/lajor2	Hay V	
Conflicting Flow All	501	278	279	0	-	0	
Stage 1	278	-	-	-		-	
Stage 2	223	-	-	-	-	-	
Critical Hdwy	6.4	6.2	4.3	-	-		
Critical Hdwy Stg 1	5.4	-	-	-	-	-	
Critical Hdwy Stg 2	5.4	-	-	-		-	
Follow-up Hdwy	3	3.1	3	-	-	-	
Pot Cap-1 Maneuver	602	809	964	-	-	-	
Stage 1	886	-	-	-	-	20	
Stage 2	941	-	-			-	
Platoon blocked, %				-	140	-	
Mov Cap-1 Maneuver	598	809	964	-		-	
Mov Cap-2 Maneuver	598	4	-		-	-	
Stage 1	880		-	-	-	-	
Stage 2	941	141	¥	. 2	4	2	
Approach	EB		NB		SB	n English	
HCM Control Delay, s	10.1		0.2		0		
HCM LOS	В						
Minor Lane/Major Mvmt	53	NBL	NBT	EBLn1	SBT	SBR	
Capacity (veh/h)		964	-		-		
HCM Lane V/C Ratio		0.006	-	0.008	-	-	
HCM Control Delay (s)		8.8	0	10.1	7.	-	
TION CONTION Delay (3)							
HCM Lane LOS		Α	Α	В	-	-	

## **APPENDIX G**

Auxiliary Turn Lane Warrants

### Turn Lane Warrant and Length Analysis Workbook

	Mui	nicipality:	Rad	lnor		Analysis	s Date:	4/15/2	020	
		County:	Delawar	e County		Conduct	ted By:	FT		
PennDOT	Engineerin	g District:	(	5			ced By:	NB Appr		
					Age	ency/Company	Name:	FTA		
ntersection & Ap	proach De	scription: Straf	ford Ave & S	Site Driveway (	assumes comb	ined volumes in	to one hypo	thetical drivewa	y)	
	Analys	is Period:	20	24	1	Number	r of Approac	h Lanes:	1	$\exists$
	Des	ign Hour:	PM Pea	ak Hour		Undivided	or Divided H	lighway:	Undivided	
	ntersection			nalized						
Posted	Speed Lim	it (MPH): f Terrain:		5 vel	4	Left or Right-	Turn Lama A.		e of Analys ft Turn Lane	
	Туре о	i Terraini.		VOLUME	CALCULAT		Turri Larie Ai	naiysis:	it ruin cane	_
				eft Turn Lane						
						Cuiations				
Movemen		Include?	Volume	% Trucks	PCEV		-	dummeter steet		200
Advancing	Left Through	Yes	10 194	0.0% 5.0%	10			dvancing Volun		209 260
Advancing	Right	No	0	0.0%	N/A			Opposing Volun Left Turn Volun		10
	Left	No	0	0.0%	N/A			Lare runn volun		20
Opposing	Through	-	249	5.0%	256					
	Right	Yes	4	0.0%	4	% Le	ft Turns in A	dvancing Volun	ne: 4	.78%
			Ri	ght Turn Lan	e Volume Ca	lculations				
Movement		Include?	Volume	% Trucks	PCEV					
	Left	No	0	0.0%	N/A					
Advancing	Through	· ·	0	0.0%	N/A N/A			dvancing Volun	_	N/A
	Right		U	0.0%	N/A		K	ight Turn Volun	ie:	N/A
			TUR	N LANE W	ARRANT	FINDINGS				
Let	t Turn La	ne Warrant Fi	ndings			Rig	ht Turn La	ne Warrant Fi	ndings	
Applicable V	Varrant Fi	gure: Fig	ure 1			Applicable V	Narrant Fig	ure: N/	Α	
	Warrant N	Met?:	Vo				Warrant M	et?: N/	Ά	
			TURN	LANE LEN	NGTH CAL	CULATIONS	5			
1	ntersection	Control	Unsignalize							
esign Hour Volu			10	u						
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# Turn Lane Warrant and Length Analysis Workbook

	Mur	nicipality:	Rad	dnor		Analysis Date:	4/15/2020	
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George W. Broseman Direct Dial: (610) 941-2459 Direct Fax: (610) 684-2005 Email: gbroseman@kaplaw.com

December 10, 2020

#### **VIA FIRST CLASS MAIL**

Stephen Norcini Township Engineer Township of Radnor 301 Iven Avenue Wayne, PA 19087

RE: Preliminary Land Development Application #2020-LD-08 and #2020-LD-09 –
Eagle Road Hamilton Estate; Conditional Use Application – 208 & 228 Strafford
Avenue and 18 Forrest Lane – Hamilton Estate

Our Reference: 15709-1

Dear Mr. Norcini:

In accordance with Radnor Township Code provisions, I am enclosing the following documents on behalf of the Dorrance H. Hamilton 3/15/1996 Trust, applicant in the above matter, in advance of the upcoming public meetings:

- 1. A copy of the notice sent by this office to property owners in the vicinity of the property utilizing the addresses provided to us by Radnor Township; and
- 2. Original United States Postal Service 3877 forms, (Blue Bell Post Office) date-stamped December 9, 2020.

Should you have any questions, please do not hesitate to contact me.

Sincerely,

George W Broseman

GWB:sl Enclosure



George W. Broseman Direct Dial: (610) 941-2459 Direct Fax: (610) 684-2005 Email: gbroseman@kaplaw.com www.kaplaw.com

December 10, 2020

### **VIA FIRST CLASS MAIL (PROOF OF MAILING)**

RE: Preliminary Land Development - Plan Land Development Application #2020-LD-08 and #2020-LD-09 - Eagle Road Hamilton Estate; Conditional Use Application - 208 & 228 Strafford Avenue and 18 Forrest Lane - Hamilton Estate

Dear Neighbor,

We have submitted an application to Radnor Township for the approval of a preliminary land development plan to subdivide the above property into 9 single family lots and construct 9 single family homes along Eagle Road.

We have also submitted a Conditional Use Application for 208 & 228 Strafford Avenue, and 18 Forrest Lane, to construct 41 Townhomes under the Density Modification provisions in §280.90 of the Radnor Township Zoning Code.

These plans are available for public viewing in the Engineering Department. The Land Development Plans for Eagle Road and the Conditional Use Application for Strafford Road will be reviewed by the Planning Commission at a scheduled meeting on **Monday December 21**, 2020.

This Planning Commission meeting will begin at 6:00 P.M. Currently, these meetings are being held via Zoom.

Sincerely,

George W. Broseman

GWB:sl

Kaplin Stewart Meloff Reiter & Stein, PC Union Meeting Corporate Center 910 Harvest Drive, P.O. Box 3037 Blue Bell, PA 19422-0765 610-260-6000 tel Offices in Pennsylvania

New Jersey

910 Harvest Drive Blue Bell, PA 19422

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PS Form 3877, January 2017 (Page 1 of 2)

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PS Form <b>3877</b> , January 2017 ( <i>Page 1 of 2</i> ) PSN 7530-02-000-9098	Total Number of Pieces Listed by Sender  Received at Post Office		8.	7.	6.	BLUE	4. PA 19422	3.	2.		USPS Tracking/Article Number	910 Harvest Drive Blue Bell, PA 19422	KAPLIN STEWART	Post Address of Sender
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PS Form <b>3877</b> , January 2017 ( <i>Page 1 of 2</i> )	Total Number of Pieces Listed by Sender  8 Total Number of Pieces Received at Post Office	8.	7.	6.	S. S	4.			2.	USPS Tracking/Article Number	KAPLIN STEWART 910 Harvest Drive Blue Bell, PA 19422
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PS Form <b>3877</b> , January 2017 ( <i>Page 1 of 2</i> ) PSN 7530-02-000-9098	Total Number of Pieces Listed by Sender  Received at Post Office	8.	7.	DE	\$ 2020 	4.	ω	2.		USPS Tracking/Article Number	KAPLIN STEWART 910 Harvest Drive Blue Bell, PA 19422	POSTAL SERVICE®  Name and Address of Sender
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PS Form **3877**, January 2017 (Page 1 of 2) PSN 7530-02-000-9098

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PS Form **3877**, January 2017 (Page 1 of 2) PSN 7530-02-000-9098

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WAYNE, PA 19087	GIARDINELLI CHAD & SAMANTHA L	WAYNE, PA 19087	RONGIONE NICHOLAS M & MALONE CONSTANCE A	DOUGHERTY GERARD F & KATHLEEN M 416 W BEECHTREE LA WAYNE, PA 19087	KATHARINE A  419 W BEECHTREE LA  WAYNE, PA 19087	WAYNE, PA 19087	YOSUA MICHAEL J & EMMA -	DUDA LISA A 6 MARLYN CIR WAYNE, PA 19087	THOROUGHGOOD PRISCILLA  116 SPRUCE LA  PAOLI, PA 19301	CHRISTIANSEN LAUREN 211 WEST AVE WAYNE, PA 19087	☐ Collect on Delivery (COD) ☐ Signature Confirmation ☐ Insured Mail ☐ Signature Confirmation ☐ Priority Mail Restricted Delivery	☐ Certified Mail ☐ Return Receipt for ☐ Certified Mail Restricted Delivery ☐ Merchandise	Adult Signature Restricted Delivery	Check type of mail or service  ☐ Adult Signature Required ☐ Priority Mail Express	
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PS Form **3877**, January 2017 (*Page 1 of 2*) PSN 7530-02-000-9098

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PS Form <b>3877</b> , January 2017 ( <i>Page 1 of 2</i> )	Total Number of Pieces Listed by Sender  Received at Post Office	8.	7. USPS	0202	15.40	BELL, A	3. SA CH	2. TAF 209 WA	1. BAK 201 ! WAY	USPS Tracking/Article Number 405 \	KAPLIN STEWART  910 Harvest Drive  Blue Bell, PA 19422	Name and Address of Sender Check
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PS Form <b>3877</b> , January 2017 ( <i>Page 1 of 2</i> ) PSN 7530-02-000-9098	Total Number of Pieces Total Number of Pieces Listed by Sender 8	8.	7. USPS	6. DEL GILL	5. PA 19522	4.	ω	2.		USPS Tracking/Article Number	KAPLIN STEWART 910 Härvest Drive Blue Bell, PA 19422	POSTAL SERVICE®  Name and Address of Sender
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	HOOPER WILLIAM J JR 324 WEST AVE WAYNE, PA 19087	BELLAIRE CHERYL	SHEEHAN JOHN T & COLLEEN J 409 WINDSOR AVE WAYNE, PA 19087	FOX HAT PROPERTIES LLC  1451 HELLTOPEL RD  CHESTER SPRINGS, PA 19425	BOWES SCOTT T & PATRICIA M — 336 WEST AVE — WAYNE, PA 19087 — —	POTEAU RAYMOND & VERA  338 OVERHILL RD  WAYNE, PA 19087		335 OVERHILL RD WAYNE, PA 19087	ROSIN CARL J & KATHRYN L	WAYNE, PA 19087	TIERNEY DAVID & KATHERINE 327 WEST AVE	☐ Collect on Delivery (COD) ☐ Signature Confirmation tion	Certified Mail     Certified Mail Restricted Delivery     Merchandise	☐ Adult Signature Required ☐ Priority Mail Express ☐ Adult Signature Restricted Delivery ☐ Registered Mail	vice	
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	CONN JEFFREY & LINDA E 348 EAGLE ROAD WAYNE, PA 19087	GRAVES ERIC J 340 WEST AVE WAYNE, PA 19087	337 WEST AVE WAYNE, PA 19087	WAYNE, PA 19087	KOO YUMMEE K 333 OVERHILL RD	PERTSCHUK MICHAEL & GRAVES JANET 15 CHESTNUT LANE WAYNE, PA 19087	HENRY LAWRENCE E 316 WEST AVE WAYNE, PA 19087	MASSARO CHRISTOPHER J & DEBORAH A 320 WEST AVE WAYNE, PA 19087	PARELLA JONATHAN M & DENISE 328 WEST AVE WAYNE, PA 19087	Collect on Delivery (COD)	Adult Signature Restricted Delivery  Certified Mail Certified Mail	Check type of mail or service  Adult Signature Required  Priority Mail Express	
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PS Form **3877**, January 2017 (Page 1 of 2) PSN 7530-02-000-9098

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PS Form <b>3877</b> , January 2017 ( <i>Page 1 of 2</i> ) PSN 7530-02-000-9098	Total Number of Pieces Listed by Sender  8 Total Number of Pieces Received at Post Office	8.	7.	6. 3. 5. 5. 5. 5. 5. 5. 5. 5. 5. 5. 5. 5. 5.	5. UE BILLY	4.	ယ	2.		USPS Tracking/Article Number	910 Harvest Drive Blue Bell, PA 19422	Name and Address of Sender	POSTAL SERVICE ®
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PSN 7530-02-000-9098 PS Form 3877, January 2017 (Page 1 of 2)

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Ы 4. ω Name and Address of Sender Ġ Ω 7 တ Total Number of Pieces Listed by Sender Blue Bell, PA 19422 910 Harvest Drive KAPLIN STEWART BLUE BELL PA 19 USPS Tracking/Article Number Received at Post Office Total Number of Pieces (SQS) ☐ Collect on Delivery (COD) ☐ Adult Signature Restricted Delivery ☐ Adult Signature Required ☐ Certified Mail Restricted Delivery ☐ Certified Mail Check type of mail or service 104A LANTOGA RD - A2 MUELLER JEANNETTE SEIGEL WAYNE, PA 19087 GARCIA JOSE M & ANA BRYN MAWR, PA 19010 KAUFMAN BARBARA M WAYNE, PA 19087 125 DRAKES DRUM DR 104A LANTOGA RD - A5 WAYNE, PA 19087 STIFF SPEAKER BERTRAM III & MARGARET 104A LANTOGA RD MOSER JEANNETTE C WAYNE, PA 19087 235 LENOIR AVE TAGUE JAMES A & MARY J WAYNE, PA 19087 231 LENOIR AVE WAYNE, PA 19087 215 LENOIR AVE SAMSON PETER & COUGHLIN LYNNE GARZIO JOSEPH M & MARY ANN M WAYNE, PA 19087 225 LENOIR AVE ☐ Priority Mail Express☐ Registered Mail □ Signature Confirmation ☐ Return Receipt for Merchandise , ---tion Postage 4.00 .50 .50 .50 .50 .50 50 50 <u>5</u> Affix Stamp Here (for additional copies of this receipt). Postmark with Date of Receipt. (Extra Service) Fee 3.44 . 43 . 43 .43 43 . 43 . & .<del>4</del>3 .43 Handling Charge Handling Charge - if Registered and over \$50,000 in value Actual Value if Registered Insured Value Sender if ASR Fee Adult Signature Required ASRD Fee Adult Signature Restricted Delivery Tee R Restricted Delivery FR R Return Receipt Fee SC Signature Confirmation SCRD Fee Signature Confirmation Restricted Delivery Fee SH Special Handling

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